

# **HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU CANAL, DETAIL DESIGN**

W.P. 4088-07-01

City of Ottawa

CLASS ENVIRONMENTAL ASSESSMENT FOR  
PROVINCIAL TRANSPORTATION FACILITIES (2000)  
GROUP 'B' PROJECT

MINISTRY OF TRANSPORTATION  
EASTERN REGION

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## **DESIGN AND CONSTRUCTION REPORT**

February 2016

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EASTERN REGION

Prepared for the Ontario Ministry of Transportation by:  
**MMM Group Limited**

Prepared by:



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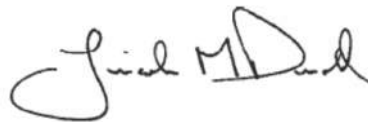
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## THE PUBLIC RECORD

Copies of this Design and Construction Report are available for review at the following locations:

**City of Ottawa**  
**Client Service Centre, City Hall**  
110 Laurier Avenue West  
Ottawa, ON K1P 1J1

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Friday: 10:00 a.m. to 6:00 p.m.

Saturday: 10:00 a.m. to 5:00 p.m.

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## GLOSSARY

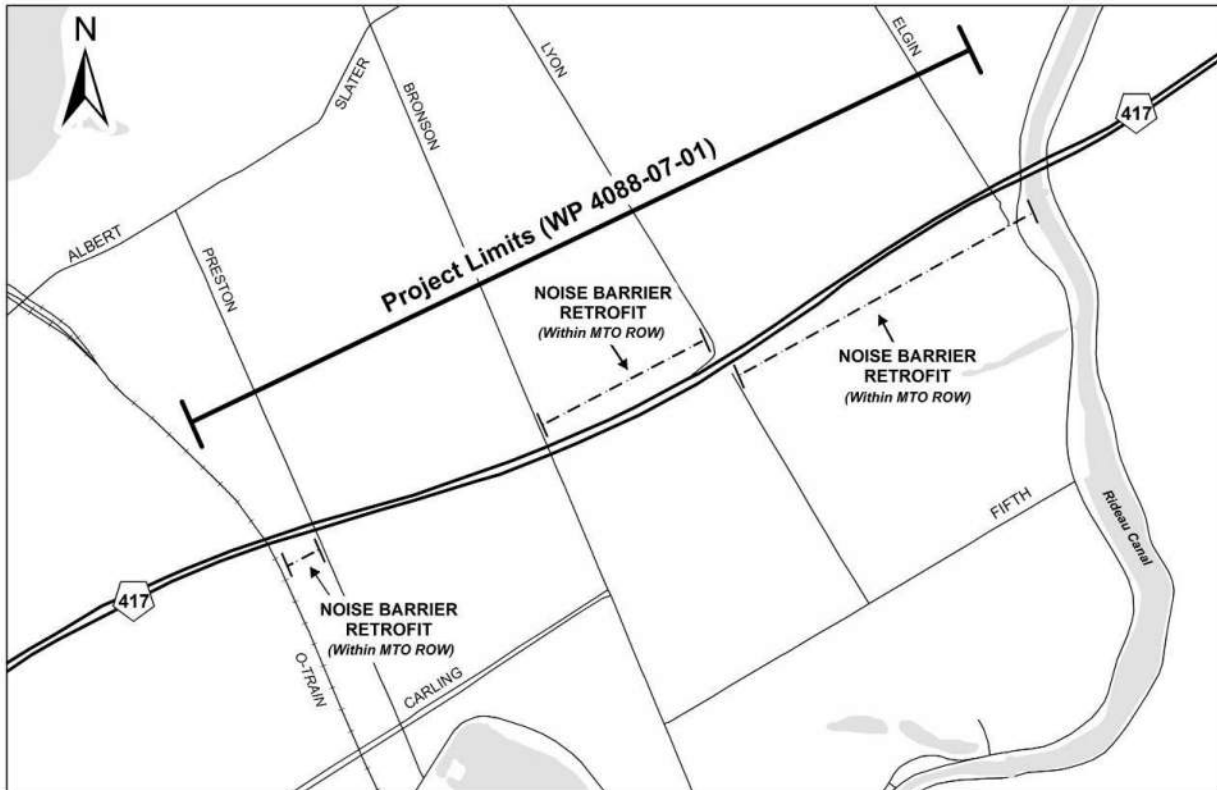
ATMS	ADVANCED TRAFFIC MANAGEMENT SYSTEMS
CA	CONTRACT ADMINISTRATOR
MTO CLASS EA	MINISTRY OF TRANSPORTATION'S CLASS ENVIRONMENTAL ASSESSMENT FOR PROVINCIAL TRANSPORTATION FACILITIES (1999, AS AMENDED 2000)
DCR	DESIGN AND CONSTRUCTION REPORT
EA	ENVIRONMENTAL ASSESSMENT
ERD	ENVIRONMENTAL REFERENCE FOR HIGHWAY DESIGN
GWP	GROUP WORK PROJECT
MNRF	ONTARIO MINISTRY OF NATURAL RESOURCES AND FORESTRY
MTO	ONTARIO MINISTRY OF TRANSPORTATION
NSP	NON-STANDARD SPECIAL PROVISION
OPSS	ONTARIO PROVINCIAL STANDARD SPECIFICATIONS
PIC	PUBLIC INFORMATION CENTRE
ROW	RIGHT-OF-WAY
SP	STANDARD PROVISION
TESR	TRANSPORTATION ENVIRONMENTAL STUDY REPORT
WP	WORK PROJECT

# 1 OVERVIEW OF THE UNDERTAKING

## 1.1 PROJECT SUMMARY

### 1.1.1 INTRODUCTION

In 2011, the Ministry of Transportation (MTO) retained MMM Group Limited to complete the detail design for the Highway 417 Noise Barrier Retrofits project (WP 4088-07-01), as part of the overall Queensway Expansion from Highway 416 to Anderson Road. The project limits are illustrated in **Exhibit 1-1**.



**Exhibit 1-1: Project Limits**

This Design and Construction Report (DCR) presents the results of the detail design component of the environmental assessment study which was completed in accordance with the approved environmental planning process for Group 'B' undertakings under the MTO's *Class Environmental Assessment* (Class EA) for *Provincial Transportation Facilities* (2000).

The purpose of this DCR is to document existing conditions, feedback received through consultation activities, the detail design (Recommended Plan), potential environmental impacts and proposed mitigation measures.

### 1.1.2 BACKGROUND – PRELIMINARY DESIGN / ENVIRONMENTAL ASSESSMENT STUDY

Highway 417 is the major east-west provincial transportation corridor in the Ottawa area that provides for inter-city and local travel. Construction of Highway 417 started in the 1950s to provide a controlled-access

highway to replace Highway 17. Over the years, Highway 417 has been expanded to serve growing travel demand and traffic volumes. In response to growing traffic volumes in the City of Ottawa, in summer 2002 the MTO initiated a comprehensive operational review of Highway 417 (Queensway) from west of Highway 416 easterly to Anderson Road (GWP 663-93-00). This study evaluated opportunities to optimize the use of the existing facility, while providing for the efficient movement of people and goods and reducing traffic congestion. It examined existing and future problems and opportunities and provided a Recommended Plan to guide the evolution of the Queensway over the next 20 years. The study examined the mainline, as well as all interchanges and crossing roadways within the study limits.

The Preliminary Design and Environmental Assessment Study was carried out in accordance with the approved environmental planning process for Group 'B' projects under the MTO Class EA. The preliminary design was documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in August 2008.

This study identified various concerns within the study area, including:

- Insufficient capacity to accommodate existing and projected travel demands for the study corridor;
- Locations on Highway 417 with higher than expected collision frequency (typically associated with recurring congestion);
- The standards used for the original construction and subsequent modifications to Highway 417 through the study area have resulted in a number of features that are considered atypical in the context of current design practice (including mainline horizontal and vertical alignment elements, ramp geometry and lane/shoulder widths); and
- The majority of the existing highway infrastructure is in need of significant rehabilitation and/or replacement over the planning horizon (the next 20 years) in order to maintain its functionality.

A Recommended Plan was developed to address these concerns, which included:

- Widening Highway 417 from three to four mainline lanes in each direction from Highway 416 to Carling Avenue and from Metcalfe Street to Ottawa-Road (OR) 174;
- Retaining the existing number of mainline lanes from Carling Avenue to Metcalfe Street;
- Widening Highway 417 from two to three lanes in each direction from OR 174 to east of Walkley Road;
- Retaining the basic horizontal and vertical geometry throughout;
- Modifying the interchanges at Richmond Road, Greenbank Road, Woodroffe Avenue, Carling Avenue, Parkdale Avenue, Bronson Avenue, Lyon Street, Nicholas Street and St. Laurent Boulevard to improve safety and traffic operations;
- Revising/enhancing the drainage system to accommodate the proposed widening;
- Rehabilitating pavement, bridges and the illumination system as required;
- Upgrading the existing Advance Traffic Management System including additional pavement loop detectors, cameras, changeable message signs and queue end warning devices;
- Upgrading noise walls to current standards; and
- Replacing/enhancing landscaping.

### 1.1.3 MTO CANDIDATE SITES FOR NOISE BARRIER RETROFIT LIST

In 2005, the Ministry conducted the Highway 417 Centretown Noise Study which evaluated areas along the Queensway to determine where the construction of retrofit noise barriers could achieve a 5 dBA reduction in noise levels at the first row of residential receivers. The areas of investigation included: the areas from Bronson Avenue to the Rideau Canal on the north side of Highway 417; and from Glendale Avenue to the Rideau Canal and in the vicinity of Preston Street on the south side of Highway 417.

The evaluations are documented in the 2007 TESR and concluded that the following locations should be added to the MTO Candidate Sites for Noise Barrier Retrofits List:

- On the north side of Highway 417 from Bronson Avenue to Lyon Street;
- On the south side of Highway 417 from east of Preston Street westerly towards the existing Parkdale Avenue noise barrier; and
- A series of barriers on the south side of Highway 417 from Elgin Street westerly towards Glendale Avenue.

### 1.1.4 DETAIL DESIGN STUDY PURPOSE & GENERAL DESCRIPTION OF THE UNDERTAKING (RECOMMENDED PLAN)

This study was undertaken to prepare the recommended noise barriers for implementation, including completion of detail design and preparation of contract drawings and tender documents.

#### Recommended Plan

This project includes the following components:

- New retrofit noise barriers at the following locations:
  - On the south side of Highway 417 from the O-Train to Preston Street;
  - On the north side of Highway 417 from Bronson Avenue to Lyon Street;
  - On the south side of Highway 417 and along ramps from Lyon Street to the Rideau Canal;
- Construction of related works, including modifications to existing structural barrier walls, roadside protection, and illumination modifications, as required; and
- Traffic management during construction in coordination with the City of Ottawa. It is anticipated that staged lane reductions/closures, ramp closures and night work will be required to accommodate installation of the noise barriers.

A more comprehensive description of the Recommended Plan is provided in Chapter 3.0 of this report.

### 1.1.5 CONSULTATION

Throughout the duration of the detail design study, local elected representatives, Aboriginal communities, external agencies, interest groups, and members of the general public were encouraged to participate through a proactive consultation plan that included notification letters, newspaper notices, a Public Information Centre (PIC), and a project website.

Stakeholder comments were taken into consideration during the refinement of the detail design. Specific concerns related to the timing of noise barrier construction and included requests to expand the detail design project limits to include the installation of noise barriers along sections of Highway 417 both east and west of the current project limits. A comprehensive discussion of comments received is included in Chapter 2.0 of this report.

### 1.1.6 ENVIRONMENTAL ASSESSMENT PROCESS

The Ministry of Transportation's *Class Environmental Assessment* (MTO Class EA) for *Provincial Transportation Facilities* was approved under the *Ontario Environmental Assessment Act* (OEAA) in fall 1999, and was amended in 2000. This planning document defines groups of projects and activities and the environmental assessment process that MTO has committed to follow for these undertakings. Provided that this process is followed, projects and activities included under the MTO Class EA do not require formal review or approval under the OEAA. There is an opportunity at any time during the MTO Class EA process for interested persons to provide comments and review outstanding issues.

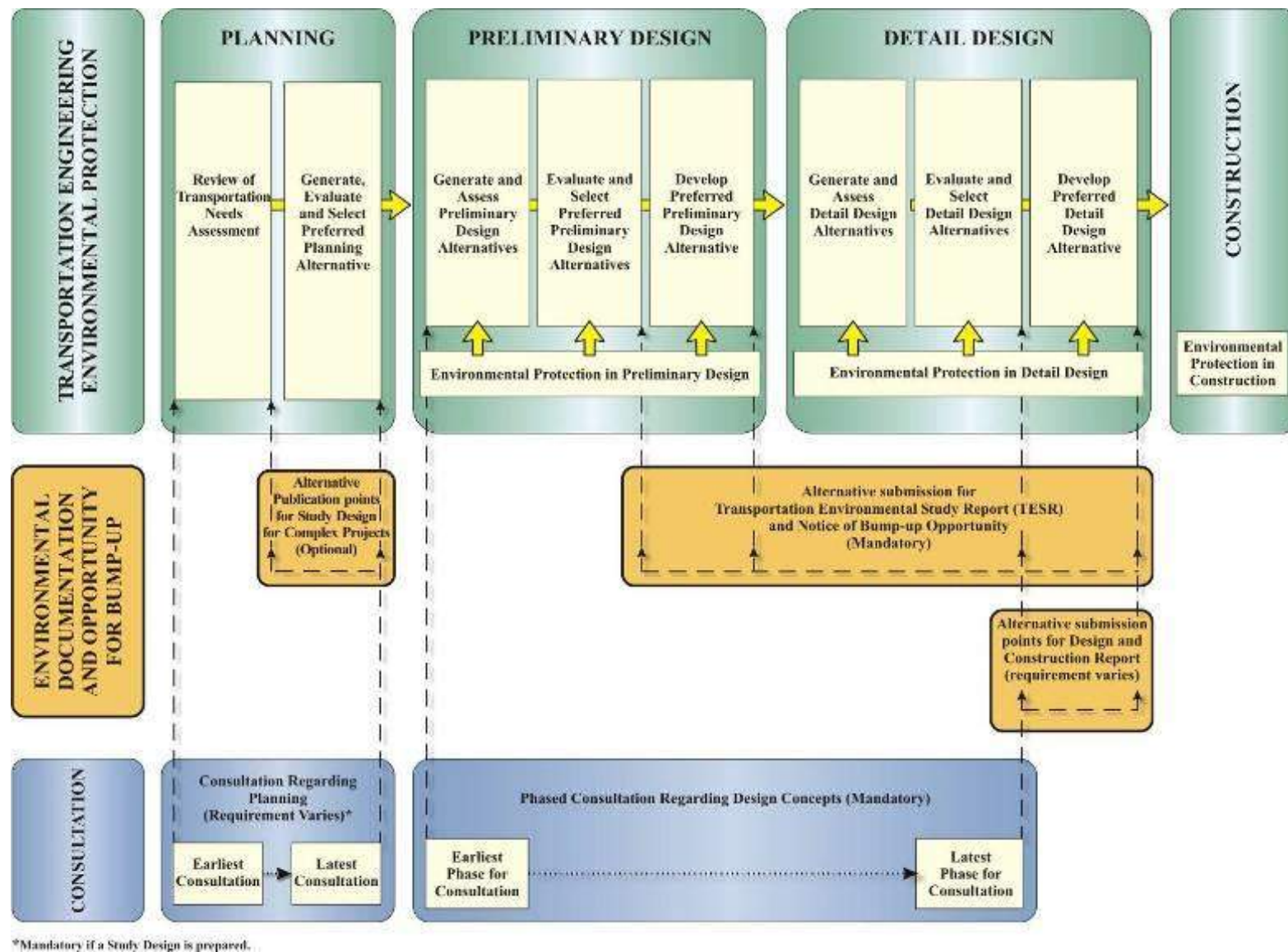
The MTO Class EA process is principle-based. Where appropriate, this DCR references the principles applied and how they were achieved during the environmental assessment process.

The following principles underlie the MTO Class EA process:

- Transportation engineering principles;
- Environmental protection principles;
- External consultation principles;
- Evaluation principles that are intended to achieve the best overall balance of these principles;
- Documentation principles;
- 'Bump-up' principles; and
- Environmental clearance principles to proceed.

As part of the Preliminary Design and Environmental Assessment Study, a Transportation Environmental Study Report (TESR), entitled *Highway 417 (Ottawa Queensway) from Highway 416 Easterly to Anderson Road Preliminary Design Study and Environmental Assessment*, G.W.P 663-93-00 was prepared and filed for public and agency review. The TESR described the recommended improvements to Highway 417, anticipated environmental effects and proposed mitigation measures, and was filed for a 30-day public review period to provide interested stakeholders with an opportunity to review and comment on the report. During the 30-day public review of the TESR, 9 'bump-up' requests (i.e. request for a Part II Order) were made for this project. MTO undertook additional consultation with bump-up requestors in order to address outstanding concerns. However, agreements with requestors were not achieved. The Minister of the Environment denied the bump-up requests through correspondence issued to each requestor on August 1, 2008.

This detail design study is being carried out in accordance with the approved planning process for Group 'B' projects (**Exhibit 1-2**). Following the 30-day public review of this DCR, the project will have met the requirements of the MTO Class EA.

**Exhibit 1-2: Overview of Class EA Process for Group 'B' Projects**(Excerpt from the *Class Environmental Assessment for Provincial Transportation Facilities* [2000])

### 1.1.7 PURPOSE OF THE DESIGN AND CONSTRUCTION REPORT

This DCR has been prepared in accordance with the requirements of the approved environmental planning process for Group 'B' undertakings, under the MTO Class EA. The DCR is intended to document the following:

- Transportation engineering and environmental issues and their impact on the EA process;
- Changes in existing environmental conditions from those documented in preliminary design;
- Environmental concerns and commitments;
- Anticipated environmental impacts and commitments to mitigation measures (to be included in the contract documents);
- The consultation program carried out during detail design;
- Identification of all project approvals, licenses and permits that have been or must be obtained prior to construction;
- Implementation of the commitments to further work contained in the Transportation Environmental Study Report (TESR), including any environmental effects monitoring that is required; and
- Construction documentation, as required.

As outlined above, approval of the DCR marks the final task in the detail design portion of the EA process. The "Notice of Submission" has been published concurrent with the filing of this DCR for a 30-day public review period, and identified the start and end dates for the DCR review period, as well as the addresses and hours of operation for the review locations.

Additional information is available in the contract drawings and specifications. The Project Team is available to discuss information provided within this report or project-related inquiries and can be contacted as follows:

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## 2 CONSULTATION PROCESS

Consistent with the requirements for Group 'B' projects under the MTO Class EA, consultation with federal, provincial and municipal agencies, Aboriginal communities, local elected representatives, interest groups, and members of the public was on-going throughout all stages of the project.

### 2.1 CONSULTATION DURING PRELIMINARY DESIGN

Consultation during the Preliminary Design and Environmental Assessment Study Phase (2002-2008) consisted of the following:

- Ontario Government Notices advertising study commencement and three rounds of Public Information Centres (PICs);
- Direct letter mailings;
- Preparation of a Study Design Report outlining how the study would be carried out;
- On-going maintenance of the external agency and public mailing list developed during Preliminary Design;
- Meetings with Municipal Technical Advisory and Public Advisory Committees (MTAC and PAC) to facilitate identification of concerns and evaluation of alternatives, with regular meetings held throughout the duration of the study;
- Presentations to the City of Ottawa Transportation Committee;
- Three PICs as follows:
  - Public Involvement Centre #1, held on January 21, 22, 23, and 30, 2003, to review preliminary drawings, present preliminary background inventory work, the proposed evaluation techniques, and to seek input on the Study Design Report and the overall perceived needs within the study area;
  - Public Involvement Centre #2, held on June 21, 22 and 23, 2004, to present and seek public comments on the alternatives and the technically preferred alternatives as identified by the study team;
  - Public Involvement Centre #3, held on June 20, 21 and 22, 2005, to present and seek public comments on the Recommended Plan as presented at a preliminary design level of detail;
- A notice announcing submission of the Transportation Environmental Study Report (TESR) for public review and comment.

Throughout the duration of the study, members of the public, interest groups, and external agencies were encouraged to participate. This proactive approach provided an opportunity for all interested stakeholders to comment both on the process and the study findings.

During the Preliminary Design and Environmental Assessment Study, Environment Canada provided advice on the investigation of environmental issues, and Indian and Northern Affairs Canada (presently Aboriginal and Northern Affairs Canada) provided contact information for Aboriginal communities.

Comments received during the Preliminary Design and Environmental Assessment Study from members of the public generally indicated:

- Support for prioritizing public transit;

- Support for the proposed widening;
- Concerns about traffic operations and safety; and
- Concerns about loss of green space, impacts to property, neighbourhoods, and air quality, and impacts from noise and vibration.

As previously noted, a TESR was prepared as part of the Preliminary Design and Environmental Assessment Study and described the recommended improvements on Highway 417, anticipated environmental effects and proposed mitigation measures. The TESR was filed for a 30-day public review period, during which 9 'bump-up' requests (i.e. request for a Part II Order) were made for this project. Following additional consultation between MTO and bump-up requestors to address outstanding concerns, agreements were not achieved and the Minister of the Environment denied the bump-up requests through correspondence issued to each requestor on August 1, 2008.

## 2.2 EXTERNAL AGENCY CONSULTATION DURING DETAIL DESIGN

A comprehensive Consultation Plan was developed for the detail design phase to address the requirements of the MTO Class EA process and to place emphasis on consultation with the stakeholders most directly affected by the project. As per the Consultation Plan, stakeholders were kept informed of the project and were asked for input through the use of conventional, effective consultation methods, including:

- Ontario Government Notices published in local newspapers;
- Notification letters/emails to local elected representatives, Aboriginal communities, external agency representatives, interest groups, and members of the public;
- Correspondence and meetings with a Technical Advisory Committee and other stakeholders;
- A Public Information Centre;
- Project website updates; and
- Filing of this Design and Construction Report (DCR) for a 30-day public review period.

An Aboriginal Consultation Plan was also developed for detail design. All consultation with Aboriginal communities was carried out in conformance with the Provincial Environmental Planning Office Info-bulletin: *Consultation with Aboriginal Peoples, Interim Direction* (December 2009).

### 2.2.1 CONSULTATION WITH LOCAL ELECTED REPRESENTATIVES

On March 8, 2013, letters were sent to local elected representatives to announce the commencement of the detail design study. These letters included summaries of the project components and an overview of the study process. A copy of the Study Commencement letter sent to local elected representatives is provided in **Appendix A**.

On January 4, 2016, letters were sent to local elected representatives inviting them to attend the Public Information Centre (PIC) held on January 20, 2016 (see **Section 2.3.4** for more details on the PIC). A copy of the Notice of Public Information Centre letter sent to local elected representatives is provided in **Appendix A**.

On February 4, 2016, letters were sent to local elected representatives announcing the submission of the DCR for a 30-day public review period, and identified the start and end dates for the DCR review period.

and the addresses and hours of operation for the review locations. A copy of the Notice of Submission letter sent to local elected representatives is provided in **Appendix A**.

The following local elected representatives were contacted:

- City of Ottawa Mayor
- City of Ottawa Chair of Transportation Committee of Council
- City of Ottawa Councillor – Somerset Ward (Ward 14)
- City of Ottawa Councillor – Capital Ward (Ward 17)
- MP – Ottawa Centre
- MPP – Ottawa Centre

## **2.2.2 CONSULTATION WITH ABORIGINAL COMMUNITIES**

On March 8, 2013, letters were sent to Aboriginal communities announcing the commencement of detail design. These letters included summaries of the project components, the study process, and information regarding potential cultural and environmental impacts. A copy of the Study Commencement letter sent to Aboriginal communities is provided in **Appendix A**.

On January 4, 2016, letters were sent to Aboriginal communities inviting them to attend the Public Information Centre (PIC), which was held on January 20, 2016. A copy of the Notice of Public Information Centre letter sent to Aboriginal communities is provided in **Appendix A**.

On February 4, 2016, letters were sent to Aboriginal communities announcing the submission of the DCR for a 30-day public review period, and identified the start and end dates for the DCR review period and the addresses and hours of operation for the review locations. A copy of the Notice of Submission letter sent to Aboriginal communities is provided in **Appendix A**.

The following Aboriginal communities were contacted:

- Alderville First Nation
- Algonquins First Nation Consultation Office
- Algonquins of Pikwàkanagàn
- Beausoleil First Nation
- Chippewas of Georgina Island
- Curve Lake First Nation
- Hiawatha First Nation
- Mississaugas of the New Credit First Nation
- Mississaugas of Scugog Island First Nation
- Moose Deer Point First Nation
- Ottawa Algonquin First Nation
- Métis Nation of Ontario Consultation Unit
- Ottawa Métis Council

## **2.2.3 CONSULTATION WITH EXTERNAL AGENCIES**

On March 8, 2013, letters were sent to external agency representatives from federal agencies, provincial ministries, the City of Ottawa, utilities, and interest groups to announce the commencement of the detail

design study. An Agency Comment Form was enclosed to give external agencies the opportunity to express any concerns or interests regarding the project and submit any additional comments including identification of required permits or approvals. Copies of the Study Commencement letter and Agency Comment Form sent to external agencies are provided in **Appendix A**.

On January 6, 2016, letters were sent to external agency representatives inviting them to attend the Public Information Centre (PIC), which was held on January 20, 2016. A copy of the Notice of Public Information Centre letter sent to external agencies is provided in **Appendix A**.

On February 4, 2016, letters were sent to external agency representatives to announce the submission of the DCR for a 30-day public review period, and identified the start and end dates for the DCR review period and the addresses and hours of operation for the review locations. A copy of the Notice of Submission letter sent to external agency representatives is provided in **Appendix A**.

The following external agencies were contacted:

#### Federal Agencies

- Aboriginal Affairs and Northern Development Canada
- National Capital Commission

#### Provincial Ministries/Agencies

- Ministry of Aboriginal Affairs
- Ministry of Community and Social Services
- Ministry of Economic Development and Trade
- Ministry of the Environment and Climate Change
- Ministry of Health and Long-Term Care
- Ministry of Natural Resources and Forestry
- Ministry of Tourism, Culture and Sport
- Rideau Valley Conservation Authority
- Ontario East Economic Development Commission

#### Municipalities and Local Agencies

- City of Ottawa – Distribution
- City of Ottawa – Environmental Engineering
- City of Ottawa – Infrastructure Projects
- City of Ottawa – Infrastructure Assessment – Water Resources Assets
- City of Ottawa – Public Works
- City of Ottawa – Road Renewal
- City of Ottawa – Right-of-Way Info and Approvals
- City of Ottawa – Signal Design and Installation
- City of Ottawa – Sustainable Transportation
- City of Ottawa – Traffic Engineering
- City of Ottawa – Traffic Operations
- City of Ottawa – Traffic Management
- City of Ottawa – Traffic Safety and Mobility
- City of Ottawa – Transportation Planning

- City of Ottawa – Water Distribution
- City of Ottawa – Watermain Renewal

#### Emergency Services

- Ottawa Central Ambulance Communications Centre
- Ottawa Fire Services
- Ottawa Police

#### Utilities

- Allstream
- Bell Canada
- Cogeco Inc.
- Enbridge Gas Distribution
- Hydro Ottawa Limited
- Rogers
- Telus Corporation

### **2.2.4 TECHNICAL ADVISORY COMMITTEE**

A Technical Advisory Committee (TAC) was formed to identify and resolve technical issues throughout the detail design study. To facilitate this coordination, a TAC contact list was developed and included representatives from City of Ottawa staff and the National Capital Commission. TAC members were kept informed through study notification letters and formal TAC meetings. In addition, a total of 4 TAC meetings were held for this project. The first TAC meeting was held on September 12, 2012 and the last meeting was held on January 23, 2014.

In general, technical aspects of the Queensway Expansion projects from Parkdale Avenue to Ottawa Road 174 were discussed during the TAC meetings, including: potential utility conflicts (including the need for protection and/or relocation of utilities within the corridor); construction, traffic and pedestrian staging during the projects; temporary/permanent signage; illumination requirements; and the need for coordination with other projects on-going within the study area.

The TAC meeting notes are on file with the Ministry of Transportation.

### **2.2.5 COMMENTS RECEIVED FROM LOCAL ELECTED REPRESENTATIVES, ABORIGINAL COMMUNITIES AND EXTERNAL AGENCIES**

**Table 2-1** provides a summary of comments received from local elected representatives, aboriginal communities and external agencies. Copies of comments received are provided in **Appendix C**.

Table 2-1: Summary of Comments Received from Local Elected Representatives, Aboriginal Communities and External Agencies

NO.	DATE / FORM OF CONTACT	AGENCY	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
<b>STUDY COMMENCEMENT</b>				
1	Mar. 28, 2013 / Email	Councillor, Somerset Ward	<ul style="list-style-type: none"> <li>Commented that the need for these noise barriers has been in discussion in for decades and that the Councillor's file includes letters between residents, community associations and the Province on this topic dating back to 1987.</li> <li>Requested that the noise barriers be installed as soon as possible and to be kept updated on decisions and the upcoming open house.</li> </ul>	<ul style="list-style-type: none"> <li>A response was sent by email by MMM on October 6, 2015 and was redirected to Councillor Catherine McKenney, who replaced Diane Holmes as the Councillor of Somerset Ward. The response included the following:</li> <li>Former Councillor Diane Holmes submitted a comment to the Ministry of Transportation (MTO) in March 2013 regarding the Highway 417 Noise Barrier Retrofits project, which includes noise barrier retrofits on the south side of Highway 417 between the O-Train corridor and Preston Street; on the north side of the highway between Bronson Avenue and Kent Street; and on the south side of the highway from Lyon Street to the Rideau Canal. MMM Group Limited has been retained by the MTO to undertake the detail design and environmental assessment study for this project.</li> <li>In response to the comment received from Ms. Holmes below, we would like to take this opportunity to advise you that the Highway 417 Noise Barrier Retrofits project is currently undergoing detail design. A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined. Please note that the timing of construction for this project is dependent on provincial priorities and the availability of funding. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174. Construction of the noise barrier retrofits may also be combined with other road improvements in the Highway 417 corridor to the extent possible, in order to minimize traffic impacts.</li> </ul>
2	Oct. 6, 2015 / Email	Councillor, Somerset Ward	<ul style="list-style-type: none"> <li>In response to a project update, looks forward to hearing about the timing of the PIC.</li> </ul>	<ul style="list-style-type: none"> <li>A response was not required.</li> </ul>
3	Oct. 7, 2015 /	Councillor, Somerset Ward	<ul style="list-style-type: none"> <li>The Councillor copied the Centretown Citizens' Community Association and the</li> </ul>	<ul style="list-style-type: none"> <li>A response was sent by email by MMM on October 7, 2015, confirming that local Community Associations would be notified in</li> </ul>

NO.	DATE / FORM OF CONTACT	AGENCY	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
	Email		Dalhousie Community Association to ensure they are aware of the project, and requested that they be notified of future consultation opportunities.	advance of the date, time and location of the PIC.
4	Mar. 11, 2013 / Email	Senior Project Manager – Sustainable Transportation, City of Ottawa	<ul style="list-style-type: none"> <li>Requested to be included on the project circulation list.</li> <li>Requested to see drawings once complete.</li> </ul>	<ul style="list-style-type: none"> <li>Contact was sent confirmation email on March 11, 2013.</li> </ul>
5	Mar. 11, 2013 / Email	Project Engineer – Infrastructure Projects, City of Ottawa	<ul style="list-style-type: none"> <li>Indicated that it will not be necessary to be a part of this project.</li> </ul>	<ul style="list-style-type: none"> <li>A response was not required.</li> <li>Contact was removed from project mailing list.</li> </ul>
6	Mar. 18, 2013 / Email	Ottawa Fire Service	<ul style="list-style-type: none"> <li>Organization does not wish to participate in the project, as indicated on submitted comment form.</li> </ul>	<ul style="list-style-type: none"> <li>A response was not required.</li> <li>Contact was removed from project mailing list.</li> </ul>
7	Mar. 20, 2013 / Email	Heritage Planner, Ministry of Tourism, Culture & Sport	<ul style="list-style-type: none"> <li>Requested that MTCS continue to receive project notices and are advised if the project area extends beyond areas previously subject to cultural heritage assessment.</li> <li>Reiterated MTCS' mandate to conserve Ontario's cultural heritage resources including archaeological resources, built heritage resources, and cultural heritage landscapes.</li> </ul>	<ul style="list-style-type: none"> <li>A response was sent by email by MMM on October 6, 2015 and was redirected to an updated contact at MTCS. The response included the following:</li> <li>The Noise Barrier Retrofits project currently undergoing detail design was recommended as a result of a Preliminary Design and Environmental Assessment (EA) Study for Highway 417 (from Highway 416 to Anderson Road), which was documented in a Transportation Environmental Study Report (TESR) and received environmental clearance in 2008. The purpose of the current detail design assignment is to prepare the approved plan for implementation.</li> <li>During preliminary design, Stage I &amp; II Archaeological Assessments were completed by C.R. Murphy Archaeological Consulting in August 2002 and October 2004, respectively. The Stage I archaeological assessment found that the Highway 417 right-of-way corridor lies within a zone of medium to high potential for discovery of significant prehistoric or historic archaeological sites, based on proximity to several provincially significant prehistoric and historic sites that have been recorded in the Ottawa Valley. Based on this general proximity, the Stage II</li> </ul>

NO.	DATE / FORM OF CONTACT	AGENCY	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
				<p>archaeological assessment examined the existing Highway 417 right-of-way over the project limits. These investigations found no evidence of archaeological resources within the existing right-of-way, which has been significantly disturbed.</p> <ul style="list-style-type: none"> <li>• No Cultural Heritage Evaluation Reports (CHERs) are planned as part of this project.</li> <li>• No impact on archaeological or built heritage resources is anticipated and no further assessments or investigations are planned.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date for the event has yet to be determined. Further details regarding the recommended plan for the noise barrier retrofits, construction staging, potential impacts and proposed mitigation will be presented at the PIC.</li> </ul>
8	Apr. 11, 2013 / Email	Mark-Up Administrator/Permits Coordinator, Distribution Planning, Enbridge Gas Distribution	<ul style="list-style-type: none"> <li>• Provided a plot confirming that there are no natural gas plants on Highway 417 but identified natural gas in surroundings areas.</li> <li>• Provided a PDF markup of existing or proposed gas plants, based on the general location of the project.</li> <li>• Detailed drawings must be resubmitted for sign off by Enbridge Gas Distribution.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was not required.</li> <li>• The contact was included in formal utilities circulation as part of the detail design process.</li> </ul>
9	Apr. 18, 2013 / Letter	Manager, Consultation Unit – Aboriginal Relations and Ministry Partnerships Division, Ministry of Aboriginal Affairs	<ul style="list-style-type: none"> <li>• Letter provided First Nations contact information for the Algonquins of Ontario, the Ottawa Region Métis Council, and the Métis Nation of Ontario as the Aboriginal communities identified as having existing or asserted rights or claims in Ontario's land claims process or litigation, and that could be impacted by the Highway 417 project. All contacts provided by MAA have already been included in all project consultation to date and have received all project notifications.</li> <li>• MAA requested to be removed from the mailing list.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was not required.</li> <li>• Contact was removed from project mailing list.</li> </ul>



NO.	DATE / FORM OF CONTACT	AGENCY	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
10	Apr. 2, 2014 / Email	Senior Land Use Planner – National Capital Commission	<ul style="list-style-type: none"><li>The NCC confirmed a continued interest in the project and provided additional contacts to include in project consultation.</li></ul>	<ul style="list-style-type: none"><li>A response was not required.</li><li>Additional contacts were added to the project mailing list.</li></ul>

## 2.3 PUBLIC INVOLVEMENT DURING DETAIL DESIGN

### 2.3.1 PROJECT WEBSITE

A project website was created to provide project information, updates and documents to interested stakeholders. The project website is available at **[www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)**.

The website includes information on the project background, study area, study process, public involvement, and the project schedule.

The newspaper notices announcing Study Commencement and the Public Information Centre are provided on the website, as well as relevant reports and the PIC display boards. The newspaper notice announcing submission of the DCR for a 30-day review period will also be provided on the website.

To date, the overall Queensway Expansion East project website has had approximately 500 unique visitors to date in 2016, 4,900 unique visitors in 2015, 7,500 unique visitors in 2014, and 5,700 unique visitors in 2013.

### 2.3.2 LETTER NOTIFICATIONS

A study contact list was developed based on a review of members of the public, including local community associations, interest groups, and businesses who previously expressed an interest in the project during preliminary design, and who live/work in the general vicinity of the project.

On March 8, 2013, letters/emails were sent to members of the public to announce the commencement of the detail design study. These letters included summaries of the project components and the study process. A copy of the Study Commencement letter sent to members of the public is provided in **Appendix A**.

On January 6, 2016, letters/emails were sent to members of the public inviting them to attend the PIC. These letters included information on the date, location and timing of the PIC. A copy of the PIC invitation letter to members of the public is provided in **Appendix A**.

On February 4, 2016, letters were sent to members of the public to announce the submission of the DCR for a 30-day public review period, and identified the start and end dates for the DCR review period and the addresses and hours of operation for the review locations. A copy of the Notice of Submission letter sent to members of the public is provided in **Appendix A**.

### 2.3.3 NEWSPAPER NOTIFICATIONS

In order to ensure public awareness of the project, a notice of Study Commencement was published in the following local newspapers:

- Ottawa Citizen (English version) Friday, March 8, 2013
- Ottawa Le Droit (French version) Friday, March 8, 2013

In order to ensure general public awareness and invite anyone with an interest in the project to attend the PIC held on January 20, 2016, a notice was published in the following local newspapers:

- Ottawa Citizen (English version) Wednesday, January 6, 2016
- Ottawa Le Droit (French version) Wednesday, January 6, 2016

A final notice announcing that this DCR was available for public review was published in the following local newspapers:

- Ottawa Citizen (English version) Friday, February 5, 2016
- Ottawa Le Droit (French version) Friday, February 5, 2016

Copies of the French and English notices advertising Study Commencement, the PIC, and filing of the DCR are provided in **Appendix A**.

### 2.3.4 PUBLIC INFORMATION CENTRE

A Public Information Centre (PIC) was held on January 20, 2016 to provide external agencies, interest groups, business representatives and members of the general public with an opportunity to review and comment on the project's proposed design, potential impacts and proposed mitigation plan. Members of the Project Team, including bilingual staff, were available at the PIC to discuss the project and answer any questions.

20 individuals signed the register at the PIC and were encouraged to complete comment sheets. Nine (9) comment sheets were received. An additional seven (7) comments were received by email, the project website, by letter, or by phone in advance of or following the PIC. The majority of comments and questions received pertained to the following:

- Anticipated construction timing for the installation of noise barrier retrofits;
- Effects of concrete vs. acrylic noise barrier materials on noise attenuation;
- Construction staging and associated traffic impacts and detours, including proposed nightly pedestrian detours;
- Concerns with noise levels related to decelerating vehicles along the Highway 417 eastbound off-ramp to Kent Street;
- Clarification regarding the location of noise barrier retrofits as part of this project and other ongoing or upcoming projects in the Highway 417 corridor;
- Inquiries as to when noise barrier replacements and/or retrofits can be expected along Highway 417 in areas outside of the current detail design project limits; and
- Availability of the PIC materials on the project website.

A copy of the Public Information Centre display materials is provided in **Appendix B**. Copies of comments received are provided in **Appendix D**.

### 2.3.5 COMMENTS RECEIVED FROM MEMBERS OF THE PUBLIC

**Table 2-2** provides a summary of public consultation. All comments requiring a response received a letter or email reply. Copies of comments received from members of the public are included in **Appendix D**.

Table 2-2: Summary of Comments Received from Members of the Public

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
<b>STUDY COMMENCEMENT</b>			
1	Mar. 8, 2013 / Email	<ul style="list-style-type: none"> <li>Commented that having noise barriers installed along the Queensway in the downtown area should be a top priority.</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 2, 2013.</li> <li>A response was sent by email by MMM on October 6, 2015 and included the following:</li> <li>Please be advised that the Highway 417 Noise Barrier Retrofits project is currently undergoing detail design. Through the downtown area, noise barriers being constructed as part of this project will include the north side of Highway 417 from Bronson Avenue to Kent Street and the south side of Highway 417 from Lyon Street to the Rideau Canal.</li> <li>Under the Ministry's Queensway improvements plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction.</li> <li>Please note that the timing of construction for the noise barrier retrofits as part of the current detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
2	Mar. 11, 2013 / Email	<ul style="list-style-type: none"> <li>Requested that the design team take into consideration the increasing number of residents and high rise buildings proposed in the new Carling-Preston CDP when designing the height and location of barriers.</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 2, 2013.</li> <li>A response was sent by email by MMM on October 6, 2015 and included the following:</li> <li>Please be advised that within the Carling-Preston CDP study area, the noise barrier retrofits proposed as part of the current detail design project are limited to the south side of Highway 417 between Preston Street and the O-Train corridor.</li> <li>Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of noise barriers along other sections of the Highway 417 corridor has not been determined at this time. Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for</li> </ul>

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
			<p>construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</p> <ul style="list-style-type: none"> <li>• Under the Noise Barrier Retrofits Program, MTO may consider noise mitigation on a retrofit basis, for subdivisions which received approval prior to 1977. Candidate sites under this program are identified and construction occurs on a priority basis. Subsequent to 1977, the issue of noise relating to proposed new developments is dealt with by the municipality through the development approvals process.</li> <li>• The noise barrier retrofits constructed as part of the current detail design project and future noise barrier replacements within the Highway 417 corridor will generally consist of 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection. The noise attenuation achieved through the installation of noise barriers is highest for the first row of residential receivers. The effectiveness of noise barriers is reduced for residences that are located several blocks from the highway.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
3	Mar. 13, 2013 / Email	<ul style="list-style-type: none"> <li>• Pleased that the noise barrier project is commencing.</li> <li>• Suggested using a concrete plank type wall.</li> <li>• Concerned about the noise caused by the increased use of engine breaks on the 417.</li> <li>• Suggested extending the barrier wall beyond the line-of-sight for improved noise attenuation.</li> <li>• Inquired when the PIC will be held.</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to project contact list on Apr. 2, 2013.</li> <li>• A response was sent by email by MMM on October 6, 2015 and included the following:</li> <li>• An Environmental Noise Assessment (May 2014) was completed to assess and confirm the noise barrier requirements in the areas where retrofit barriers were originally identified as part of a Preliminary Design and Environmental Assessment (EA) study for Highway 417 (from Highway 417 (from Highway 416 to Anderson Road), which received environmental clearance in 2008. The Noise Assessment found that an average attenuation of 6 dB will be achieved for the first row of receivers following the installation of 5 m high retrofit noise barriers from Lyon Street to just west of the Rideau Canal, measured from the outdoor living areas associated with the first row of residences south of Highway 417.</li> <li>• Please be advised that the noise barrier retrofits constructed as part of the current detail design project and future noise barrier replacements within the Highway 417 corridor will generally consist of 5 m high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection. Five metre high noise barriers significantly exceed the line of sight from a vehicle on Highway 417, which is generally measured at 1.05 m above pavement level.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined. Please note that the timing of construction for this project is dependent on provincial priorities and the availability of funding, and will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits,</li> </ul>

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
			the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.
4	Mar. 14, 2013 / Email	<ul style="list-style-type: none"> <li>Confirmed that the Orchard Estates Community Association has no comment regarding the design or its review.</li> </ul>	<ul style="list-style-type: none"> <li>A response was not required.</li> </ul>
5	Mar. 16, 2013 / Email	<ul style="list-style-type: none"> <li>Suggested that the current plan be extended to include the section of highway between Holland Avenue and Preston Street.</li> <li>Noted that noise levels on the south side of Highway 417 have increased since the improvement of the north side barriers.</li> <li>A noise survey recently conducted by a neighbour recorded the noise levels over 60 dBA. Suggested that retrofitting sections of the existing noise barrier is inadequate and that it makes sense to do a complete retrofit.</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 2, 2013.</li> <li>A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>The existing noise barrier on the north side of Highway 417 between Melrose Avenue and Loretta Avenue consists of Durisol precast noise barriers which are designed to absorb noise rather than reflect it. As a result, these noise barriers are much more effective in minimizing reflected noise than the metal barriers they replaced.</li> <li>Please be advised that the current noise barrier retrofits detail design project includes the construction of a noise barrier on the south side of Highway 417 between Preston Street and the O-Train corridor. However, the area on the south side of Highway 417 between Preston and Holland is outside of the limits of the current project.</li> <li>Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 west of the O-Train corridor has not been determined at this time.</li> <li>Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>Please also be advised that the noise barriers constructed as part of the current detail design project and future noise barrier replacements within the Highway 417 corridor will generally consist of 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection.</li> <li>A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015,</li> </ul>

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
			although a date has yet to be determined.
6	Mar. 16, 2013 / Email	<ul style="list-style-type: none"> <li>Concerned that noise barriers proposed on the north side of the highway will be reflective rather than absorptive and will reflect noise to the south side of the Queensway.</li> <li>If reflective, concerned that noise barrier will reflect noise towards them and increase noise on their street.</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 2, 2013.</li> <li>A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>Please be advised that the noise barriers constructed as part of the current noise barrier retrofits detail design project will be 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection. Furthermore, &lt;&lt; redacted &gt;&gt; is separated from Highway 417 by Chamberlain Avenue. As such, the buildings along Chamberlain contribute to noise attenuation for adjacent streets. As part of the current detail design project, a noise barrier retrofit is also planned on the south side of Highway 417 from Lyon Street to the Rideau Canal.</li> <li>Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
7	Mar. 17, 2013 / Email	<ul style="list-style-type: none"> <li>Expressed concern that since the noise barriers on the north side of Queensway were retrofitted, there is more noise on the south side.</li> <li>Requested that the study boundaries are extended to include area near Fairmont and Young.</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 2, 2013.</li> <li>A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>The existing noise barrier on the north side of Highway 417 between Melrose Avenue and Loretta Avenue consists of Durisol precast noise barriers which are designed to absorb noise rather than reflect it. As a result, these noise barriers are much more effective in minimizing reflected noise than the metal barriers they replaced.</li> <li>Please be advised that the area on the south side of Highway 417 at Fairmont and Young is outside of the limits of the current Noise Barrier Retrofits detail design project. Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 west of the O-Train corridor has not been determined at this time.</li> </ul>

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
			<ul style="list-style-type: none"> <li>• Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
8	Mar. 17, 2013 / Email	<ul style="list-style-type: none"> <li>• Improved (higher, more absorbent, better deflection) noise barriers are required on the south side of the 417 between Preston and Parkdale.</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to project contact list on Apr. 2, 2013.</li> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that the current noise barrier retrofits detail design project includes the construction of a noise barrier on the south side of Highway 417 between Preston Street and the O-Train corridor. However, the remaining area on the south side of Highway 417 between Preston and Parkdale is outside the limits of the current project.</li> <li>• Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the south side Highway 417 noise barriers west of the O-Train corridor to Parkdale has not been determined at this time.</li> <li>• Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>• Please also be advised that the noise barriers constructed as part of the current detail design project and future noise barrier replacements within the Highway 417 corridor will generally consist of 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
9	Mar. 18, 2013 / Email	<ul style="list-style-type: none"> <li>• Disappointed that noise barriers along Young Street</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to project contact list on Apr. 2, 2013.</li> </ul>



NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
		<p>are not being included in this project.</p> <ul style="list-style-type: none"> <li>• Feels that they are not being given the same consideration as other sections of the Queensway.</li> <li>• Suggests extending the barrier retrofits to Westgate Shopping Centre.</li> <li>• Requested a detailed rationale as to why noise barriers are not being considered along Young Street.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that the area on the south side of Highway 417 along Young Street and up to the Westgate Shopping Centre is outside of the limits of the current noise barrier retrofits detail design project. Under the Ministry's Queensway improvement plan, all existing metal noise barriers in the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 west of the O-Train corridor has not been determined at this time.</li> <li>• Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
10	Mar. 19, 2013 / Email	<ul style="list-style-type: none"> <li>• Requested that the project team review correspondence from Minister Naqvi.</li> <li>• Inquired if it is possible to accommodate a wider study area to the north of the Queensway between the Canal and the Nicholas exchange.</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to project contact list on Apr. 2, 2013.</li> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that the area on the north side of the Queensway between the Rideau Canal and the Nicholas Street interchange is outside of the limits of the current Noise Barrier Retrofits detail design project. Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if the noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the north side Highway 417 noise barriers between the Rideau Canal and the Nicholas Street interchange has not been determined at this time.</li> <li>• Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015,</li> </ul>

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			although a date has yet to be determined.
11	Mar. 19, 2013 / Email	<ul style="list-style-type: none"> <li>Expressed concern regarding the noise levels south of Highway 417 on Fairmount Avenue.</li> <li>Noted that the barrier on the north side of Highway 417 is higher and more efficient.</li> <li>Expressed concerns that the Fairmount section of the sound barrier is not scheduled to be upgraded and suggests that this should be done.</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 2, 2013.</li> <li>A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>Please be advised that the current Noise Barrier Retrofits detail design project includes the construction of a noise barrier on the south side of Highway 417 between Preston Street and the O-Train corridor. However, the remaining area on the south side of Highway 417 west of the O-Train corridor is outside the limits of the current detail design project.</li> <li>Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of noise barriers on south side of Highway 417 at Fairmont Avenue has not been determined at this time. Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>The existing noise barrier on the north side of Highway 417 between Melrose Avenue and Loretta Avenue consists of Durisol precast noise barriers which are designed to absorb noise rather than reflect it. The noise barriers constructed as part of the current detail design project and future noise barrier replacements within the Highway 417 corridor will generally consist of 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection. The noise attenuation achieved through the installation of noise barriers is highest for the first row of residential receivers. The effectiveness of noise barriers is reduced for residences that are located several blocks from the highway. Furthermore, the level of noise attenuation achieved is measured from the outdoor living area (OLA) at a residence considered to be a Noise Sensitive Area, and not from upper floors.</li> <li>A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
12	Mar. 19, 2013 / Email	<ul style="list-style-type: none"> <li>Requested additional information about the project including the process that will be initiated to expand this project to the new infill</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 2, 2013.</li> <li>See combined response with Comment #22.</li> </ul>

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		<p>development at Young and Champagne, which backs onto Highway 417.</p> <ul style="list-style-type: none"> <li>• Suggested that the study area be expanded to include the requirements of the infill housing projects.</li> </ul>	
13	Mar. 20, 2013 / Email	<ul style="list-style-type: none"> <li>• Concerned that new noise barriers on the north side of the highway reflect noise back towards their street.</li> <li>• Noise and debris from the Queensway prevent the opening of windows and produce noise and light pollution.</li> <li>• Strongly suggests considering a western extension of the proposed retrofits to include Young Street up to Reid Ave and eventually Reid Park.</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to project contact list on Apr. 2, 2013.</li> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• The existing noise barrier on the north side of Highway 417 between Melrose Avenue and Loretta Avenue consists of Durisol precast noise barriers which are designed to absorb noise rather than reflect it. As a result, these noise barriers are much more effective in minimizing reflected noise than the metal barriers they replaced.</li> <li>• Please be advised that the current Noise Barrier Retrofits detail design project includes the construction of a noise barrier on the south side of Highway 417 between Preston Street and the O-Train corridor. However, the area on the south side of Highway 417 west of the O-Train corridor to Reid Park is outside of the limits of the current project.</li> <li>• Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 west of the O-Train corridor has not been determined at this time. Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>• The noise barriers constructed as part of the current detail design project and future noise barriers within the Highway 417 corridor will generally consist of 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection.</li> <li>• Please be advised that construction of the "infill" section of noise barrier adjacent to Reid Park is still part of the Ministry's plans. However, the installation of a noise barrier in this area involves a number of complex design issues. A 1220 mm diameter City of Ottawa watermain lies in very close proximity to the edge of the rock outcrop. It is not feasible to extend a conventional noise</li> </ul>

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			<p>barrier along the top of the rock outcrop, as the installation would require drilled footings in very close proximity to the watermain, with the potential to damage it. Furthermore, it is not feasible to have an excessively high noise wall installed at the base of the rock outcrop (i.e. at the level of Highway 417).</p> <ul style="list-style-type: none"> <li>• The Ministry has examined alternative solutions for noise mitigation in this area, such as an earth berm and interlocking prefabricated blocks, constructed on the top of the rock outcrop. However, these potential solutions also risk destabilizing the edge of the rock outcrop and potentially causing rock debris to fall onto the highway. They also carry the risk of overloading the area in proximity of the watermain and subjecting it to damage, potentially causing a rupture of the pipe.</li> <li>• Ultimately, a solution to noise mitigation in this area is dependent on the eventual relocation of the watermain. Due to the significant construction costs associated with the relocation of the watermain, the most logical time for the City to consider its relocation would be at the time of its replacement. If other potential solutions are identified to address noise mitigation in this area in the interim, they will be considered by the Ministry.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
14	Mar. 22, 2013 / Email	<ul style="list-style-type: none"> <li>• Wants the study to include noise barriers west of the O-Train to Reid Park and beyond.</li> <li>• Noise barriers constructed on the north side of the Queensway reflect noise and have resulted in increased noise on their street south of the highway which has no barriers.</li> <li>• Noise, dust and light pollution significantly impact their quality of life.</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to project contact list on Apr. 2, 2013.</li> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• The existing noise barrier on the north side of Highway 417 between Melrose Avenue and Loretta Avenue consists of Durisol precast noise barriers which are designed to absorb noise rather than reflect it. As a result, these noise barriers are much more effective in minimizing reflected noise than the metal barriers they replaced.</li> <li>• Please be advised that the current Noise Barrier Retrofits detail design project includes the construction of a noise barrier on the south side of Highway 417 between Preston Street and the O-Train corridor. However, the remaining area on the south side of Highway 417 west of the O-Train corridor is outside of the limits of the current project.</li> <li>• Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 west of the O-Train corridor has not been determined at this time. Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas</li> </ul>

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			<p>Street interchange to Ottawa Road 174.</p> <ul style="list-style-type: none"> <li>The noise barriers constructed as part of the current detail design project and future noise barriers within the Highway 417 corridor will generally consist of 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection.</li> <li>Please be advised that construction of the "infill" section of noise barrier adjacent to Reid Park is still part of the Ministry's plans. However, the installation of a noise barrier in this area involves a number of complex design issues. A 1220 mm diameter City of Ottawa watermain lies in very close proximity to the edge of the rock outcrop. It is not feasible to extend a conventional noise barrier along the top of the rock outcrop, as the installation would require drilled footings in very close proximity to the watermain, with the potential to damage it. Furthermore, it is not feasible to have an excessively high noise wall installed at the base of the rock outcrop (i.e. at the level of Highway 417).</li> <li>The Ministry has examined alternative solutions for noise mitigation in this area, such as an earth berm and interlocking prefabricated blocks, constructed on the top of the rock outcrop. However, these potential solutions also risk destabilizing the edge of the rock outcrop and potentially causing rock debris to fall onto the highway. They also carry the risk of overloading the area in proximity of the watermain and subjecting it to damage, potentially causing a rupture of the pipe.</li> <li>Ultimately, a solution to noise mitigation in this area is dependent on the eventual relocation of the watermain. Due to the significant construction costs associated with the relocation of the watermain, the most logical time for the City to consider relocation of the watermain would be at the time of its replacement. If other potential solutions are identified to address noise mitigation in this area in the interim, they will be considered by the Ministry.</li> <li>A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
15	Mar. 23, 2013 / Email	<ul style="list-style-type: none"> <li>Inquired about the status of a response to a previous email and wanted to confirm that it was received by project team.</li> </ul>	<ul style="list-style-type: none"> <li>A response was sent by email on March 25, 2013 indicating the comment had been received and that a response was in the process of being formulated.</li> </ul>
16	Mar. 24, 2013 / Email	<ul style="list-style-type: none"> <li>Noted that a family member was informed by the MPP that noise barriers were being installed in certain sections along Highway 417.</li> <li>Inquired why noise barriers were not being installed in other areas.</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 2, 2013.</li> <li>A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>Please be advised that as part of the current noise barrier retrofits detail design project, noise barrier retrofits are proposed on the south side of the Highway 417 between the O-Train corridor and Preston Street, on the north side of the highway between Bronson Avenue and Kent Street, and on the south side of the highway between Lyon Street and the Rideau Canal. The south side of Highway 417 west of the O-Train corridor is outside of the limits of the current detail design project.</li> </ul>

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		<ul style="list-style-type: none"> <li>• Believes existing metal noise barriers should have been installed at the level of houses, not the roadway.</li> <li>• Noted that noise and air pollution have increased in the last 10 years.</li> </ul>	<ul style="list-style-type: none"> <li>• Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 between Parkdale Avenue and Island Park Drive has not been determined at this time. Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
17	Mar. 28, 2013 / Email	<ul style="list-style-type: none"> <li>• Expressed support for the construction of the noise barriers on the Queensway.</li> <li>• Suggested the use of concrete barriers because of their lifespan and that they can be artistically adorned.</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to project contact list on Apr. 2, 2013.</li> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that the noise barriers constructed as part of the current detail design project will generally consist of 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection. Facing the highway, the noise barriers will consist of a random, alternating pattern of warm grey ribbed and tan smooth panels, consistent with the recommendations of the Context Sensitive Design Concepts for the Queensway-Highway 417 report (MTO, 2011) and the new noise barrier at Lees Avenue constructed as part of the overall Highway 417 Expansion project. Facing residences, the noise barriers will be a uniform grey treatment.</li> <li>• Construction timing for the noise barrier retrofits is dependent on provincial priorities and the availability of funding, and will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
18	Apr. 4, 2013 / Email	<ul style="list-style-type: none"> <li>• The Glebe Community Association (GCA) expressed strong support for the construction of these noise barriers.</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to project contact list on Apr. 4, 2013.</li> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that construction of the noise barrier retrofits is dependent on provincial priorities and the availability of funding, and will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures</li> </ul>

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		<ul style="list-style-type: none"> <li>The GCA is committed to supporting the Ministry of Transportation through community feedback and consultation.</li> <li>Requested that the chair of the Traffic Committee be added to the project contact list and that the GCA continue to receive notification regarding this project.</li> </ul>	<p>will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</p> <ul style="list-style-type: none"> <li>A Public Information Centre (PIC) is being scheduled to take place in Fall 2015, although a date for the event has not yet been determined.</li> <li>Please also be advised that the Chair of the GCA Traffic Committee has been included on the project contact list and will continue to receive all future project notification.</li> </ul>
19	Apr. 8, 2013 / Email	<ul style="list-style-type: none"> <li>Inquired if there is any consideration for a noise barrier on the south side of Highway 417 between Bronson and Lyon.</li> <li>Commented that there is a section of new barrier proposed as part of the Bronson Avenue interchange improvements project.</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 17, 2013.</li> <li>A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 between Lyon and Bronson has not been determined at this time. Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>Please be advised that the noise barrier proposed as part of the Highway 417 Bronson Avenue Interchange Operational Improvements project will be constructed to replace the noise attenuation previously provided by the Ottawa Board of Education building, which has been demolished to allow for the realignment of the Bronson Avenue eastbound off-ramp / Chamberlain Avenue.</li> <li>A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
20	Apr. 10, 2013 / Email	<ul style="list-style-type: none"> <li>Concerned that Young Street was not included in the noise barrier retrofits and requested extension of the project limits</li> </ul>	<ul style="list-style-type: none"> <li>Contact was added to project contact list on Apr. 17, 2013.</li> <li>A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>Please be advised that the south side of Highway 417 west of the O-Train corridor is outside of the limits of the current Noise Barrier Retrofits detail design project.</li> </ul>

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		<p>based on proximity to the Queensway and associated traffic volumes, noise pollution caused by traffic, and traffic induced vibrations caused by truck traffic..</p> <ul style="list-style-type: none"> <li>Believes a new noise barrier would reduce noise pollution and disruptive vibrations in this area.</li> </ul>	<ul style="list-style-type: none"> <li>The objective of the MTO Environmental Guide for Noise / Noise Policy is to reduce noise levels as much as is technically and economically practicable towards the provincial noise level objective of 55 dBA. Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 west of the O-Train corridor has not been determined at this time.</li> <li>Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>Noise barrier retrofits are not anticipated to contribute to a decrease in the perception of ground-based vibrations.</li> <li>A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
21	April 24, 2013 / Phone	<ul style="list-style-type: none"> <li>Calling on behalf of homeowners in a townhome subdivision on Young Street, regarding low 4-foot metal barrier/snow guard east of Loretta to rail corridor.</li> <li>Major safety concern, especially due to blowing ice from trucks hitting townhomes and nearly hitting construction workers on site and homeowners.</li> <li>Would like to submit information, pictures, and suggested on-site inspection.</li> </ul>	<ul style="list-style-type: none"> <li>A response was given by phone on April 26, 2013 and detailed that the concerns would be brought to the Ministry's attention and considered along with other comments received regarding noise barrier retrofits.</li> <li>Commenter will be submitting more information regarding the safety concerns over the insufficient height of the metal barriers between Loretta and the rail corridor.</li> <li>MTO provided a direct response. The need for care in the plowing operations to minimize snow thrown onto private property has been reviewed with the winter maintenance service provider.</li> </ul>
22	April 24, 2013 / Email	<ul style="list-style-type: none"> <li>Forwarded email previously submitted to Glen Murrar, Minister of Transportation</li> </ul>	<ul style="list-style-type: none"> <li>A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>Please be advised that the current Noise Barrier Retrofits detail design project includes the construction of a noise barrier on the south side of Highway 417 between Preston Street and the</li> </ul>



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		<p>regarding noise concerns on Young Street.</p> <ul style="list-style-type: none"> <li>Concerned that the detail design project limits are in relatively unpopulated areas as compared to their neighbourhood and that they did not consider recent population and housing increases in the area.</li> <li>Expressed concerns regarding snow plowing operations and snow and ice from trucks on the highway falling onto the residential areas below.</li> </ul>	<p>O-Train corridor. However, the south side of Highway 417 adjacent to 53 Young Street is outside of the limits of the current detail design project. Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the retrofit of noise barriers along the south side of Highway 417 west of the O-Train corridor has not been determined at this time.</p> <ul style="list-style-type: none"> <li>Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>As the land use at the time of barrier construction was not residential, the existing section of barrier immediately west of the O-Train corridor was constructed as a snow barrier, designed to prevent snow from the highway being plowed onto the railway tracks below. The existing noise barrier wall between the west limit at Loretta Avenue and the O-Train is stepped down to become a snow barrier.</li> <li>Regarding the provision of noise mitigation for new development projects adjacent to Highway 417, please be advised that as part of the Noise Barrier Retrofits Program, MTO may consider noise mitigation on a retrofit basis for subdivisions which received approval prior to 1977. Candidate sites under this program are identified and construction occurs on a priority basis. Subsequent to 1977, the consideration of noise mitigation for proposed new developments is dealt with by the municipality through the site plan approvals process, in which the Ministry is not involved.</li> <li>The recently redeveloped area of Young Street just west of the O-Train corridor does not meet the above criteria for installation of noise barriers. The Ministry's Building and Land Use Permit issued for the residential condominium development on the north side of Young Street between Loretta Avenue and the O-Train corridor explicitly stated that responsibility for noise control and all associated costs would be the sole responsibility of the developer/property owner.</li> <li>Please be advised that a preliminary design study is currently underway for bridge rehabilitations in Central Ottawa, including the bridge carrying Highway 417 over the O-Train corridor. A detailed noise analysis will be conducted at the time of detail design for the replacement of the existing Highway 417 noise and snow barriers, and will determine the required height of the replacement wall and the appropriate limits.</li> <li>Your comments regarding the potential safety issue of falling ice and snow from fast-moving</li> </ul>

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			<p>vehicles breaching the existing snow barrier on the south side of Highway 417 have been forwarded to the Ministry for consideration. The Ministry has reviewed the need for care in the plowing operations to minimize snow thrown onto private property with the winter maintenance service provider.</p> <ul style="list-style-type: none"> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
23	June 10, 2013 / Phone	<ul style="list-style-type: none"> <li>• Requested to be added to the project contact list.</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to project contact list on June 10, 2013.</li> </ul>
24	Nov. 20, 2013 / Email	<ul style="list-style-type: none"> <li>• Requested to be added to study contact list.</li> <li>• Requested clarification that noise barriers will be established along the North side of the Queensway to protect Centretown residents from air and noise pollution.</li> <li>• Commented that the information regarding the realignment of Chamberlain Avenue does not provide a clear picture of what the revised route may look like.</li> </ul>	<ul style="list-style-type: none"> <li>• Contact was added to the project contact list on Nov. 20, 2013.</li> <li>• A response was sent by email on Jan. 14, 2013 and included the following: <ul style="list-style-type: none"> <li>○ On behalf of the Ministry of Transportation, thank you for submitting your comments regarding proposed improvements to Highway 417. The detail design for planned improvements to Highway 417 has been separated into several separate assignments, including: <ul style="list-style-type: none"> <li>▪ Parkdale Avenue Interchange Improvements;</li> <li>▪ Bronson Avenue Interchange Improvements;</li> <li>▪ Noise Barrier Retrofits; and</li> <li>▪ Kent Street Overpass Replacement.</li> </ul> </li> <li>○ Specifically, you asked for clarification regarding the implementation of noise barriers as part of the above-noted projects and regarding the proposed realignment of Chamberlain Avenue.</li> <li>○ <u>Noise Barriers</u></li> <li>○ The Highway 417 Kent Street Overpass Replacement project (WP 4033-08-01) includes the construction of new noise walls on the south barrier walls and replacement of the existing snow fence on the north barrier walls west of the bridge. As part of this project, work will only be completed where bridge work is also taking place (i.e. on the bridge and wing walls).</li> <li>○ The Highway 417 Noise Barrier Retrofits project (WP 4088-07-01) includes the construction of noise walls on the north side of Highway 417 from Bronson Avenue to Lyon Street, and on the south side of the highway from the O-Train corridor to Preston Street and from Lyon Street to the Rideau Canal.</li> <li>○ Any noise barrier retrofits in other locations along Highway 417 will be subject to future study in the highway corridor, and will be planned for implementation in coordination with other future road work to minimize traffic impacts.</li> <li>○ <u>Chamberlain Avenue Realignment</u></li> <li>○ As part of the Highway 417 Bronson Avenue Interchange Improvements project (WP 4089-07-01), Chamberlain Avenue will be realigned and extended to be directly in line with the Highway 417 eastbound off-ramp at Bronson Avenue, following the removal of the former</li> </ul> </li> </ul>

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			<p>Ottawa School Board building. Traffic exiting the highway will be able to travel directly east through the intersection. As such, the volume of traffic using the intersection of Chamberlain / Imperial Avenue adjacent to Drummond's gas station will be significantly reduced. In addition, a noise barrier is being constructed along the south side of the eastbound off-ramp from Booth Street to Cambridge Street South, and to replace the noise attenuation previously provided by the former Ottawa School Board building.</p> <ul style="list-style-type: none"> <li>○ The above works were recommended as a result of a Preliminary Design and Environmental Assessment (EA) study for Highway 417 (from Highway 416 to Anderson Road), which received environmental clearance in 2008. The study examined needs and opportunities with respect to mobility, operations, and safety, evaluated preliminary design alternatives, and arrived at a series of recommended highway modifications to address identified issues. The purpose of the current detail design assignments is to prepare the approved EA plan for implementation.</li> </ul>
25	Jan. 27, 2014 / Email	<ul style="list-style-type: none"> <li>• Inquired about the proposed timing of the noise barrier installation from Lyon to Bronson on the north side of Highway 417.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that construction timing of the noise barrier retrofits is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
26	May 14, 2014 / Email & Phone	<ul style="list-style-type: none"> <li>• Commented that the metal noise barrier on Highway 417 between Parkdale and Island Park Drive is in desperate need of being replaced and inquired when this will happen.</li> <li>• Concerned about the possibility of small animals getting onto the highway through the holes in the existing noise barrier.</li> <li>• Expressed concern that the barrier appears to have shifted and looks as though it might be in danger of falling</li> </ul>	<ul style="list-style-type: none"> <li>• The individual was added to the study contact list on August 12, 2014.</li> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that the Highway 417 noise barriers between Parkdale Avenue and Island Park Drive are outside of the limits of the current Noise Barrier Retrofits detail design project.</li> <li>• Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers between Parkdale and Island Park has not been determined at this time.</li> <li>• Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the</li> </ul>

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		<p>onto the highway.</p> <ul style="list-style-type: none"> <li>The deterioration of the 25+ year old noise barrier has also increased the noise levels in the residential streets in the area.</li> </ul>	<p>Nicholas Street interchange to Ottawa Road 174.</p> <ul style="list-style-type: none"> <li>A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
27	June 6, 2014 / Email	<ul style="list-style-type: none"> <li>Submitted a comment regarding the Queensway Mid-town Bridges study regarding the installation of noise barrier walls as far as Elgin Street on both sides of the highway.</li> </ul>	<ul style="list-style-type: none"> <li>The individual was added to the study contact list on June 10, 2014.</li> <li>MTO provided a response on June 5, 2014 explaining that the Noise Barrier Retrofit project will include the construction of noise barriers on the south side of Highway 417 from Lyon Street easterly to the Rideau Canal and confirmed that the contact would be added to the contact list for this project to receive future notices of consultation opportunities.</li> </ul>
28	June 18, 2014 / Email (French)	<ul style="list-style-type: none"> <li>Requested additional information on the location and size of the proposed noise wall on the south side of Highway 417 from Lyon Street to the Rideau Canal.</li> </ul>	<ul style="list-style-type: none"> <li>The individual was added to the study contact list on June 26, 2014.</li> <li>MMM provided a response on June 23, 2014 which indicated that the design for the noise walls is underway and confirmed the extent of the noise wall installation from Lyon Street up to the Canal on the south side of Highway 417.</li> </ul>
29	July 21, 2014 / Email	<ul style="list-style-type: none"> <li>Inquired when the south side of Highway 417 between Bayswater and Fairmont will be receiving concrete noise barriers.</li> </ul>	<ul style="list-style-type: none"> <li>The individual was added to the study contact list on August 12, 2014.</li> <li>A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>Please be advised that the area on the south side of the Queensway between Bayswater and Fairmont is outside of the limits of the current Noise Barrier Retrofits detail design project. Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 west of the O-Train corridor has not been determined at this time.</li> <li>Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>

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30	August 12, 2014 / Phone	<ul style="list-style-type: none"> <li>Phone call regarding the process for products to be used on MTO noise barrier projects.</li> </ul>	<ul style="list-style-type: none"> <li>A follow-up email was sent after the phone call on August 12, 2014 and included the following:               <ul style="list-style-type: none"> <li>MTO completed an EA Transportation Study Environmental Report in 2007 from Hwy 416 to Anderson Road which included noise barrier work along the whole corridor.</li> <li>MMM is doing the detail design with work likely happening after 2015. A PIC still needs to be held.</li> <li>To have products considered for use on MTO projects you need to get on the MTO approved sources list.</li> <li>Contact Number for MTO Noise specialist is 1-416-235-5561.</li> <li>As per you request we will add your name to our project participation list.</li> </ul> </li> </ul>
31	August 20, 2014 / Email (French)	<ul style="list-style-type: none"> <li>Inquired if there are answers to his previous questions regarding noise barriers west of Nicholas.</li> </ul>	<ul style="list-style-type: none"> <li>MMM provided a response on August 20, 2014 indicating that the individual's questions and the technical response were forwarded to MTO for review, as per the communication protocol in place, and that a response would be forthcoming in the next few weeks.</li> <li>See combined response from MTO for Comment #36.</li> </ul>
32	Nov. 23, 2014 / Email	<ul style="list-style-type: none"> <li>Requested information regarding the timing of the next public meeting and construction of the Highway 417 Bronson Avenue Interchange Operational Improvements and the Queensway Noise Barrier Retrofit project between Lyon and the Rideau Canal.</li> </ul>	<ul style="list-style-type: none"> <li>A response was sent by email on April 14, 2015 and included the following:</li> <li>On behalf of the Ministry of Transportation (MTO), thank you for submitting a comment regarding the Highway 417 Bronson Avenue Interchange improvements, Highway 417 Noise Barrier Retrofits, and Highway 417 Kent Street Overpass projects. Specifically, you requested an update on the projects and approximate timing of any future information sessions or construction of the project works.</li> <li>The Highway 417 Bronson Avenue Interchange improvements project is currently undergoing detail design. A Public Information Centre (PIC) for this project was held on April 3, 2013. No other PICs are planned for this project. The City of Ottawa is developing plans for the construction of an underground combined sewer tunnel which will use the former Ottawa Board of Education building site / MTO property as a staging / launching area for the tunnel boring machine following the demolition of the existing building. Consequently, the construction of the proposed works for this project cannot begin until the tunnel project is completed, likely in 2017.</li> <li>The Highway 417 Noise Barrier Retrofits project (WP 4088-07-01) is currently undergoing detail design. A Public Information Centre (PIC) for this project will be taking place, however a specific date for the event has not been determined at this time. Construction timing for the noise barrier retrofits is dependent on provincial priorities and the availability of funding. As various lane and ramp closures will be required to complete the installation of noise barriers, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to OR 174. The construction of noise barrier retrofits may also be combined with other road improvements in the Highway 417 corridor to the extent possible, in order to minimize traffic impacts.</li> <li>The detail design and environmental assessment study for the Highway 417 Kent Street Overpass Replacement project (WP 4033-08-01) was completed in December 2014, and the Design and Construction Report (DCR) was made available for a 30-day public review between</li> </ul>

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			<p>November 24, 2014 and December 23, 2014. The contract for this project is being tendered by MTO and construction is scheduled to begin in Spring 2015. The rapid bridge replacement is scheduled to take place over one weekend between July and September 2015.</p> <ul style="list-style-type: none"> <li>Please be advised that you will be notified directly of any upcoming consultation opportunities for the above-noted projects, including the PIC for the Highway 417 Noise Barrier Retrofits and the 30-day public review of the DCRs for the Highway 417 Noise Barrier Retrofits and Bronson Avenue Interchange Improvements projects. Should you have any further questions, please feel free to contact the undersigned.</li> </ul>
33	Jan. 22, 2015 / Email	<ul style="list-style-type: none"> <li>Requested information regarding a project update and approximate construction timing of the Retrofit Sound Barriers</li> <li>Inquired if the Kent Street overpass replacement will be taking place this year.</li> </ul>	<ul style="list-style-type: none"> <li>See combined response to Comment #32.</li> </ul>
34	March 15, 2015 / Email	<ul style="list-style-type: none"> <li>Requested to be added to the project contact list.</li> </ul>	<ul style="list-style-type: none"> <li>The individual was added to the study contact list on March 16, 2015.</li> </ul>
35	April 14, 2015 / Email	<ul style="list-style-type: none"> <li>Expressed thanks for the project update but disappointment that the noise barrier construction could not be combined with the summer 2015 bridge replacement work at Kent Street which also requires lane closure.</li> <li>Noted that Bank Street and O'Connor overpasses are only slated for rehabilitation and do not require lane closures.</li> </ul>	<ul style="list-style-type: none"> <li>A response was not required.</li> </ul>
36	May 27, 2015 / Email	<ul style="list-style-type: none"> <li>Comment sent to lightrail@ottawa.ca</li> <li>Inquired when the noise barriers will be replaced on Highway 417 between Lyon</li> </ul>	<ul style="list-style-type: none"> <li>A response was sent by MTO on June 1, 2015 and included the following:</li> <li>The Ministry's construction efforts on Highway 417 in the near term are centered on completing the expansion of Highway 417 between Nicholas Street and the Vanier Parkway.</li> <li>The detailed design for these noise barriers is ongoing. I would anticipate a Public Information Centre for this project will be held in Autumn 2015. This project is part of the Ministry's Southern</li> </ul>

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		Street and the Rideau Canal, and noted that the project website is outdated.	<p>Highway Program, with a tentative construction date of 2017. Please note that project timing is subject to change based on funding, planning, design, environmental approval, property acquisition and construction requirements.</p> <ul style="list-style-type: none"> <li>• Thank you also for pointing out that the schedule on the website is outdated. We will make sure that it is revised.</li> <li>• The project website was updated accordingly.</li> </ul>
37	June 28, 2015 / Email	<ul style="list-style-type: none"> <li>• Requested information regarding project updates and notices of public meetings regarding noise barriers between Lyon Street and the Rideau Canal.</li> <li>• Requested to be added to the study contact list.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was sent by MMM on June 29, 2015 and included the following:</li> <li>• Please be advised that this project is currently undergoing detail design. A Public Information Centre (PIC) for this project will be taking place and is tentatively planned for Fall 2015; however, a specific date for the event has yet to be determined.</li> </ul>
38	July 8, 2015 / Email	<ul style="list-style-type: none"> <li>• Requested to be added to the study contact list.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was provided by MTO on July 9, 2015 confirming the individual will be added to the study contact list.</li> </ul>
39	July 8, 2015 / Email	<ul style="list-style-type: none"> <li>• Requested to be added to the study contact list.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was provided by MTO on July 9, 2015 confirming the individual will be added to the study contact list.</li> </ul>
40	Aug 5, 2015 / Email	<ul style="list-style-type: none"> <li>• Would like to confirm that the noise barriers will be installed on the North side of the Queensway between Kent and Lyon.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that the noise barrier retrofits proposed as part of the current detail design project were approved as part of the Preliminary Design and Environmental Assessment (EA) Study for the Highway 417 Expansion from Highway 416 easterly to Anderson Road, which was documented in a Transportation Environmental Study Report (TESR) and received environmental clearance in 2008. The purpose of the current detail design project is to prepare the approved plan for implementation.</li> <li>• As part of the current detail design project, a Noise Assessment was conducted in May 2014 and recommended that noise barrier retrofits be constructed in the following locations: <ul style="list-style-type: none"> <li>• On the south side of Highway 417 between the O-Train corridor and Preston Street;</li> <li>• On the north side of Highway 417 between Bronson Avenue and Kent Street; and</li> <li>• On the south side of Highway 417 between Lyon Street and the Rideau Canal.</li> </ul> </li> <li>• In 2005, the Ministry conducted the Highway 417 Centretown Noise Study which did not recommend noise barrier retrofits on the north side of the highway covering the entire area between Bronson Avenue and the Rideau River for the following reasons: <ul style="list-style-type: none"> <li>• Installation of noise barriers would not achieve a reduction in noise levels of 5 dBA, which represents the minimum requirement for noise barrier retrofits to be considered under the MTO Retrofit Noise Barrier Program;</li> </ul> </li> </ul>

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			<ul style="list-style-type: none"> <li>• A potential loss of highway visibility would result for businesses in the area;</li> <li>• A tunnel-like effect would be created in the central area of Ottawa; and</li> <li>• Noise barrier retrofits in this area represent significant cost of construction for modest benefits.</li> <li>• Please also be advised that the noise attenuation received through the installation of noise barriers is highest for the first row of receivers. The effectiveness of noise barriers is reduced for residences that are located several blocks from the highway.</li> <li>• A Public Information Centre (PIC) is being scheduled to be held in Fall 2015, although a date has not yet been determined.</li> </ul>
41	Sept. 18, 2015 / Email	<ul style="list-style-type: none"> <li>• Inquired about the project start date.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that the project is currently undergoing detail design and is following the approved environmental planning process for Group 'B' undertakings under the MTO's Class Environmental Assessment for Provincial Transportation Facilities (2000). The timing of construction for the noise barrier retrofits is dependent on provincial priorities and the availability of funding.</li> <li>• Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.</li> <li>• A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined.</li> </ul>
42	Sept. 30, 2015 / Email	<ul style="list-style-type: none"> <li>• Inquired as to when the retrofit sound barriers approved in 2005 for the downtown and Glebe sections of the Queensway will be built.</li> <li>• Noted that MPP Naqvi's office reported to a resident that the deferred open house will take place this autumn.</li> <li>• Requested confirmation of the open house date as no updates have been made to the website.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was sent by email by MMM on Oct. 6, 2015 and included the following:</li> <li>• Please be advised that a Public Information Centre (PIC) for this project is being scheduled to take place this Fall 2015, although a date has yet to be determined. You will be notified directly of the date, time and location of the PIC. The website will also be updated with the PIC information.</li> </ul>



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43	Oct. 6, 2015 / Email	<ul style="list-style-type: none"> <li>Requested an explanation as to why the response to the comment submitted in March 2013 was so delayed in being sent out, and why residents on one side of Highway 417 will receive new sound barriers while those on the south side will not.</li> </ul>	<ul style="list-style-type: none"> <li>A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>On behalf of the Ministry of Transportation, we apologize for the delay in providing a response to your comment. Please be advised that the detail design project was on hold between 2013 and 2015 pending other ongoing project priorities within the Highway 417 corridor.</li> <li>Please also be advised that the area on the south side of Highway 417 between Preston and Holland is outside of the limits of the current project. However, the replacement of noise barriers in this area will be considered under an upcoming MTO detail design project for the replacement of noise barriers from west of Island Park Drive to west of the O-Train. Specific details regarding the commencement of this project are not available at this time.</li> </ul>
44	Oct. 6, 2015 / Email	<ul style="list-style-type: none"> <li>Reiterated a request made to MTO three years ago for the review area to be extended.</li> <li>Clarified that the area for which noise barriers are being requested is a residential area left uncovered and never corrected.</li> <li>Both the City Councillor and MPP were involved at that time and promised to support the request.</li> <li>Concerned that retrofits are underway elsewhere in the region and across the Province and many areas, like those around Brockville, are not directly impacted by construction noise and dust.</li> <li>Concerned that since the City and Province have decided not to enforce no-engine-breaking areas or to enforce current noise limits on motorcycles, speeding cars and trucks, there is a</li> </ul>	<ul style="list-style-type: none"> <li>The individual attended the PIC and discussed concerns regarding the lack of noise barriers on the north side of the Nicholas Street interchange with the MTO Project Manager.</li> <li>A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>As discussed at the PIC, at the time of the replacement of existing noise walls on the north side of Highway 417 in the vicinity of the Nicholas Street interchange, a new noise study will be conducted to confirm noise levels within the area and to examine whether the existing noise barriers on the north side of Highway 417 at the interchange warrant extension.</li> </ul>

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		<p>barrage of noise 24/7.</p> <ul style="list-style-type: none"> <li>• Further, more construction noise will result from the LRT as it climbs out of the tunnel and heads off to the south and east.</li> <li>• The area is no longer protected by a security fence so the neighbourhood is now also subject to increased vandalism and vagrancy, with associated increases in petty thefts and attempted break-ins.</li> <li>• Requested information on the route to follow to get the priority of this request advanced.</li> </ul>	
45	Oct. 6, 2015 / Email	<ul style="list-style-type: none"> <li>• Expressed disappointment that the noise barrier would not be extended to the south side of Highway 417 at Fairmont Avenue.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was not required.</li> </ul>
46	Oct. 6, 2015 / Email	<ul style="list-style-type: none"> <li>• Expressed that the noise barriers proposed on the north side of the highway between Bronson and Kent may address noise concerns on McLeod Street between Kent and Lyon Streets.</li> <li>• The individual will endeavor to attend the PIC.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was not required.</li> </ul>
47	Oct. 7, 2015 / Email	<ul style="list-style-type: none"> <li>• Expressed thanks for the update on the timing of the PIC.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was not required.</li> </ul>
48	Oct. 29, 2015 / Email	<ul style="list-style-type: none"> <li>• Informed by the onsite Contractor for the MTO Highway 417 Kent Street</li> </ul>	<ul style="list-style-type: none"> <li>• A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>• Please be advised that extension of the proposed noise barrier along the Kent Street eastbound</li> </ul>

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		<p>bridge replacement project that footings for the noise barrier were installed.</p> <ul style="list-style-type: none"> <li>Concerned that footings were not installed along the Highway 417 egress at Kent Street, as the continuity of noise barriers along the egress towards ground level at Chamberlain is critical in order to minimize the sound of traffic during deceleration as it achieves the 50 km/hour limit at Kent and Catherine.</li> </ul>	<p>off-ramp is not considered to be warranted, as the majority of the first row of receivers south of the ramp and along Chamberlain Avenue are not residential properties with Outdoor Living Areas and as such do not qualify as Noise Sensitive Areas under the Ministry's Noise Policy.</p>
49	Oct. 30, 2015 / Email	<ul style="list-style-type: none"> <li>Inquired if a date has been set for the Fall 2015 PIC, in order to be able to provide an update at a community Traffic Committee meeting.</li> <li>Presumes there will be at least 30 days' notice of the PIC.</li> </ul>	<ul style="list-style-type: none"> <li>A response was provided by email by MMM on Nov. 2, 2015 which informed that the PIC would be held on November 24, 2015.</li> </ul>
50	Nov. 14, 2015 / Email	<ul style="list-style-type: none"> <li>Inquired if the PIC was still planned to take place on Nov. 24, 2014. The Glebe Community Association would like to give the community at least one week's notice about the open house.</li> </ul>	<ul style="list-style-type: none"> <li>A response was provided by email by MTO on Nov. 17, 2015 and included the following:</li> <li>We have been working through some construction staging issues and need to have these sorted out prior to the PIC.</li> <li>MTO avoids scheduling PICs in December as this is a very busy time of year for most people and the participation in the PIC and input received suffers as a result. The PIC will be scheduled for late January 2016. This will allow us to place the ads in the newspapers and contact those on the study mailing list in early January, once the Christmas rush has passed.</li> </ul>
51	Nov. 17, 2015 / Email	<ul style="list-style-type: none"> <li>Asked to be advised of the PIC date once it is known.</li> <li>Inquired if any timing decisions have been made on the actual construction of the barriers.</li> </ul>	<ul style="list-style-type: none"> <li>The individual received notification of the PIC as part of the mailout to members of the study contact list two weeks in advance of the PIC.</li> <li>A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>Please be advised that the timing of construction varies for the individual noise barrier walls. Select walls may be bundled with an upcoming paving project on Highway 417 from Island Park Drive to Nicholas Street, scheduled for construction in Spring / Summer 2016. The proposed noise barrier wall from Bronson Avenue to west of Percy Street on the north side of Highway</li> </ul>

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			417 will be constructed in conjunction with the rehabilitation of the Bronson Avenue overpass which has undergone preliminary design as part of the Highway 417 Midtown Bridges project. The remaining noise barriers will be constructed in coordination with other work in the Highway 417 corridor.
52	Dec. 1, 2015 / Email	<ul style="list-style-type: none"> <li>Expressed that the area around Reid Park requires further review for noise attenuation due to the Reid Park Redevelopment project slated to be finalized shortly. The City of Ottawa has been holding public consultations since 2013 regarding the project, which involves reclaiming an old municipal yard and extending the boundaries of Reid Park.</li> <li>The former municipal yard backs on to the highway beyond Reid Avenue where there are no noise barriers, but a 5 foot high chain link fence.</li> <li>Expressed concerns regarding the pleasantness of the park as well as safety for park users and highway users.</li> <li>Both the City of Ottawa and members of the public attending consultations believe the MTO should be considering the installation of noise barriers or an alternative solution to enhance noise attenuation and public safety in this area.</li> </ul>	<ul style="list-style-type: none"> <li>A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>Please be advised that the area on the south side of Highway 417 west of the O-Train corridor to Reid Park is outside of the limits of the current detail design project. The Ministry has been made aware of the City's redevelopment of Reid Park. However, parks and open space do not qualify as Noise Sensitive Areas under the Ministry's Noise Policy.</li> <li>Please be advised that the design for the replacement of existing noise barriers from west of Island Park Drive to west of the O-Train will be undertaken as part of an upcoming MTO detail design project, although details regarding the commencement of this project are not available at this time. The issue of the "infill" section of noise barrier adjacent to Reid Park may be considered at that time.</li> <li>As noted in the previous response, a solution to noise mitigation in the "infill" area adjacent to Reid Park involves complex design issues, including a 1220 mm City of Ottawa watermain which lies in very close proximity to the edge of the rock outcrop and prevents the installation and drilling of footings for a conventional noise barrier along the top of rock outcrop due to potential risks of damaging the watermain. A solution to noise mitigation in this area is dependent on the eventual relocation of the watermain. The Ministry will coordinate with the City at the time of the design for the replacement of the existing noise barriers in this location to determine the City's plans for replacement of the watermain. Other potential solutions to address noise mitigation in this area may be considered by the Ministry during the upcoming detail design process for the replacement of existing noise barriers from west of Island Park Drive to west of the O-Train.</li> </ul>

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
		<ul style="list-style-type: none"> <li>• Provided photos of the site where the metal noise barriers end and chain link fence begins.</li> </ul>	
<b>PUBLIC INFORMATION CENTRE</b>			
53	Jan. 6, 2016 / Email	<ul style="list-style-type: none"> <li>• Requested to be removed from the study contact list.</li> </ul>	<ul style="list-style-type: none"> <li>• The contact was removed from the study contact list.</li> </ul>
54	Jan. 6, 2016 / Email	<ul style="list-style-type: none"> <li>• Requested to be removed from the study contact list.</li> </ul>	<ul style="list-style-type: none"> <li>• The contact was removed from the study contact list.</li> </ul>
55	Jan. 6, 2016 / Email	<ul style="list-style-type: none"> <li>• Intends to attend on behalf of the &lt;&lt;redacted&gt;&gt; on the north side of the Queensway between Lyon and the Canal.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was not required.</li> </ul>
56	Jan. 6, 2016 / Email	<ul style="list-style-type: none"> <li>• In response to the PIC notification, confirmed that he would be away from Ottawa the evening of the PIC.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was not required.</li> </ul>
57	Jan. 12, 2016 / Email	<ul style="list-style-type: none"> <li>• Suggested putting the highway underground or building a land bridge over it, with trees or grass growing above.</li> <li>• Inquired how well trees block noise and whether it is possible to plant trees to hide the wall on both sides.</li> </ul>	<ul style="list-style-type: none"> <li>• A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>• Please be advised that relocating the existing highway underground or constructing a land bridge over the highway to mitigate noise within the project limits are not considered economically feasible options. A Noise Assessment was conducted in 2015 as part of the current detail design project and determined that noise levels would be reduced by 5 to 7 dBA for the first row of residential receivers in the proposed noise barrier retrofit locations.</li> <li>• While trees and vegetation do provide some noise attenuation, they are far less effective than noise barriers at mitigating noise. It is not feasible to plant trees on the highway side of the noise barriers from a traffic safety perspective. Regarding plantings on the municipal side of the noise barrier, it is not within the Ministry's best practices to provide dense plantings to hide a noise barrier.</li> </ul>
58	Jan. 20, 2016 / Email	<ul style="list-style-type: none"> <li>• Inquired as to the installation of noise barriers on the south side of Highway 417 between Bronson and Percy and the materials and pattern of the noise barrier.</li> <li>• Inquired if new noise barriers</li> </ul>	<ul style="list-style-type: none"> <li>• A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>• Please be advised that, as part of the ongoing Highway 417 Bronson Avenue Interchange Improvements detail design project, a short segment of noise barrier is proposed to be installed on the south side of the highway to replace the noise attenuation previously provided by the former OCDSB building. This proposed noise barrier segment will tie in to the existing metal noise barriers east of the former OCDSB building, which are also proposed to be replaced with new noise barriers as part of the upcoming detail design project for the Highway 417 Midtown</li> </ul>

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
		<p>will replace the noise attenuation formerly provided by the OCDSB building which has been demolished.</p> <ul style="list-style-type: none"> <li>• Inquired if the existing steel noise barrier from east of Bronson to Lyon will be retrofitted with the new concrete barrier technology.</li> <li>• Inquired if there are more effective solutions to buffer increased noise levels expected to arise from increased traffic volume on Chamberlain immediately east of Bronson and south of Chamberlain.</li> <li>• Inquired if anything related to the Highway 417 Bronson Avenue eastbound off-ramp extension and Chamberlain Avenue reconfiguration deviated from the original Recommended Plan. Expressed concern with the trajectory of Chamberlain, landscaped area between the new Chamberlain route and residences that back onto Glebe Memorial Park, and left-right access from Bronson to Chamberlain / Imperial.</li> </ul>	<p>Bridges.</p> <ul style="list-style-type: none"> <li>• The proposed noise barrier treatment for the noise barrier replacing the OCDSB building will likely consist of translucent acrylic noise barrier panels, as the barrier is located along a retaining wall.</li> <li>• Increased traffic volumes due to the extension of the Bronson westbound off-ramp / realignment of Chamberlain Avenue are not anticipated.</li> <li>• As part of the Highway 417 Bronson Avenue Interchange Improvements detail design project, Chamberlain Avenue will be realigned and extended to be directly in line with the Highway 417 eastbound off-ramp at Bronson Avenue. Traffic exiting the highway will be able to travel directly east through the intersection, which will eliminate the existing jog that requires exiting vehicles to turn south on Bronson to access Chamberlain. As such, the volume of traffic using the intersection of Chamberlain / Imperial Avenue adjacent to Drummond's gas station will be significantly reduced.</li> <li>• The realigned portion of Chamberlain at the intersection with Imperial adjacent to Drummond's gas station will meet the 2-lane Chamberlain extension at a tighter radius and require motorists to yield and merge onto Chamberlain to proceed east.</li> <li>• Oversized speed signs, enhanced landscaping, and a new sidewalk along the north side separated from the road by a 1 m wide boulevard will be implemented along the Chamberlain extension to reinforce a more residential character along the arterial road.</li> <li>• On the south side of the Chamberlain / Imperial intersection, a new sidewalk will be reconfigured to tie into the existing sidewalk, which will be offset from Chamberlain by a distance varying from 0-25 metres. The buffer created by this realignment will be developed with enhanced landscaping. Fencing will be installed adjacent to the road on the south side, from the Chamberlain / Imperial intersection to west of Percy Street.</li> <li>• In order to enhance existing green space in the vicinity of the proposed interchange improvements, a landscape plan has been prepared for the following areas: <ul style="list-style-type: none"> <li>• North side of the eastbound off-ramp and the ramp extension between Bronson Avenue and Percy Street</li> <li>• South side of the ramp extension just east of Bronson at the former Ottawa Board of Education building site</li> <li>• South side of the realigned Chamberlain Avenue and immediately north of Glebe Memorial Park, between Imperial Avenue and Percy Street</li> <li>• North side of Orangeville Street between Booth Street and Lebreton Street S.</li> </ul> </li> <li>• Landscaping in these areas will include a mixture of ground cover, shrub plantings, and deciduous and coniferous trees, as appropriate.</li> <li>• Please be advised that you will be notified directly of the upcoming 30-day public review period of the Design and Construction reports for both the Highway 417 Noise Barrier Retrofits and</li> </ul>

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
			Highway 417 Bronson Avenue Interchange Improvements projects.
59	Jan. 20, 2016 / PIC Comment Form	<ul style="list-style-type: none"> <li>Concerned with areas not scheduled for noise barrier retrofits as part of the project.</li> <li>Noted that noise has increased on the south side of the highway since the removal of the OCDSB building on Bronson Avenue and the removal of vegetation on Chamberlain Avenue as part of the Kent Street overpass replacement.</li> <li>Noted that the barrier on the north side of the highway reflects traffic noise to the south side.</li> <li>Requests a noise barrier on the south side between the Bronson exit to Lyon Street, and was informed at the PIC that this may be completed during the Bronson Avenue bridge replacement.</li> </ul>	<ul style="list-style-type: none"> <li>A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>Regarding increases in noise levels due to the demolition of the former OCDSB building, please be advised that a segment of noise barrier is proposed to be installed as part of the ongoing Highway 417 Bronson Avenue Interchange Improvements detail design project to replace the noise attenuation previously provided by the OCDSB building.</li> <li>Vegetation was removed on the south side of Highway 417 along Chamberlain Avenue east of Kent Street as part of the construction staging for the Highway 417 Kent Street Overpass Replacement detail design project. Please be advised that this area may be used as a future staging area by the City of Ottawa for work on the watermain along Chamberlain Avenue and by the Ministry for the Highway 417 Bank Street overpass rehabilitation. A revegetation plan was developed as part of the Kent Street Overpass Replacement project. The area will be revegetated following the completion of the above-noted projects, and may be revegetated with sod in the interim if the area is not used by the City.</li> <li>Please also be advised that a noise barrier is proposed to be installed along the Highway 417 Bronson Avenue eastbound off-ramp between Booth Street and Cambridge Street as part of the Highway 417 Bronson Avenue Interchange Improvements detail design project. Construction of the interchange improvements and associated proposed noise barriers will likely occur with the rehabilitation of the Bronson Avenue overpass which has undergone preliminary design as part of the Highway 417 Midtown Bridges project.</li> </ul>
60	Jan. 20, 2016 / PIC Comment Form	<ul style="list-style-type: none"> <li>Requested information regarding the Bronson / Imperial intersection proposed new configuration, with specifics on Imperial Avenue changes, the closure of the current in-road in front of 52 and 54 Imperial Avenue and the new 2-way entry-exit at the current exit from Imperial Avenue onto Chamberlain.</li> </ul>	<ul style="list-style-type: none"> <li>A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>As part of the ongoing Highway 417 Bronson Avenue Interchange Improvements detail design project, Chamberlain Avenue will be realigned and extended to be directly in line with the Highway 417 eastbound off-ramp at Bronson Avenue. Traffic exiting the highway will be able to travel directly east through the intersection, which will eliminate the existing jog that requires exiting vehicles to turn south on Bronson to access Chamberlain. As such, the volume of traffic using the intersection of Chamberlain / Imperial Avenue adjacent to Drummond's gas station will be significantly reduced.</li> <li>The realigned portion of Chamberlain at the intersection with Imperial adjacent to Drummond's gas station will meet the 2-lane Chamberlain extension at a tighter radius and require motorists to yield and merge onto Chamberlain to proceed east.</li> <li>Oversized speed signs, enhanced landscaping, and a new sidewalk along the north side</li> </ul>

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			<p>separated from the road by a 1 m wide boulevard will be implemented along the Chamberlain extension to reinforce a more residential character along the arterial road.</p> <ul style="list-style-type: none"> <li>On the south side of the Chamberlain / Imperial intersection, a new sidewalk will be reconfigured to tie into the existing sidewalk, which will be offset from Chamberlain by a distance varying from 0-25 metres. The buffer created by this realignment will be developed with enhanced landscaping. Fencing will be installed adjacent to the road on the south side, from the Chamberlain / Imperial intersection to west of Percy Street.</li> <li>In order to enhance existing green space in the vicinity of the proposed interchange improvements, a landscape plan has been prepared for the following areas: <ul style="list-style-type: none"> <li>North side of the eastbound off-ramp and the ramp extension between Bronson Avenue and Percy Street</li> <li>South side of the ramp extension just east of Bronson at the former Ottawa Board of Education building site</li> <li>South side of the realigned Chamberlain Avenue and immediately north of Glebe Memorial Park, between Imperial Avenue and Percy Street</li> <li>North side of Orangeville Street between Booth Street and Lebreton Street S.</li> </ul> </li> <li>Landscaping in these areas will include a mixture of ground cover, shrub plantings, and deciduous and coniferous trees, as appropriate.</li> <li>Further modifications to the intersection of Bronson and Chamberlain / Imperial, including the limitation of traffic through Chamberlain / Imperial or closure of the 'in-road', are considered to be outside the scope of the current detail design assignment. Any additional comments or concerns regarding Bronson / Chamberlain / Imperial should be directed to Mr. Philippe Landry, Manager – Traffic Management &amp; Operational Support, at the City of Ottawa at philippe.landry@ottawa.ca.</li> <li>Please be advised that you will be notified directly of the upcoming 30-day public review period of the Design and Construction reports for both the Highway 417 Noise Barrier Retrofits and Highway 417 Bronson Avenue Interchange Improvements projects.</li> </ul>
61	Jan. 20, 2016 / PIC Comment Form	<ul style="list-style-type: none"> <li>Supportive of future noise barrier retrofits through Centretown, which were discussed during MPP Naqvi's first mandate.</li> <li>Inquired why the GTA gets allocated most of the infrastructure money.</li> </ul>	<ul style="list-style-type: none"> <li>The PIC Comment Sheet indicated that a response was not required. The comment was noted.</li> </ul>
62	Jan. 20, 2016 / PIC Comment	<ul style="list-style-type: none"> <li>Expressed support for the noise barriers.</li> </ul>	<ul style="list-style-type: none"> <li>The PIC Comment Sheet indicated that a response was not required. The comment was noted.</li> </ul>



NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
	Form		
63	Jan. 20, 2016 / PIC Comment Form	<ul style="list-style-type: none"> <li>Inquired as to any plans for major work to be staged at the east side of Kent Street/Highway 417 overpass (south side of the highway).</li> </ul>	<ul style="list-style-type: none"> <li>A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>Please be advised that vegetation was removed on the south side of Highway 417 along Chamberlain Avenue east of Kent Street as part of the construction staging for the Highway 417 Kent Street Overpass Replacement detail design project. This area may be used as a future staging area by the City of Ottawa for work on the watermain along Chamberlain Avenue and by the Ministry for the Highway 417 Bank Street overpass rehabilitation. A revegetation plan was developed as part of the Kent Street Overpass Replacement project. The area will be revegetated following the completion of the above-noted projects. The area may also be revegetated with sod in the interim if it is not used by the City.</li> </ul>
64	Jan. 20, 2016 / PIC Comment Form	<ul style="list-style-type: none"> <li>Appreciated the opportunity to discuss the project with the MTO project manager.</li> <li>Expressed relief that the noise barriers would be installed shortly and will have a significant impact on mental and physical health of nearby residents.</li> </ul>	<ul style="list-style-type: none"> <li>The PIC Comment Sheet indicated that a response was not required. The comment was noted.</li> </ul>
65	Jan. 20, 2016 / PIC Comment Form	<ul style="list-style-type: none"> <li>Expressed support for the noise barriers to be installed on the south side from Lyon Street to Bank Street.</li> <li>Requested an extension of the noise barriers on the Kent Street off-ramp, where traffic decelerates and continues under the Kent Street overpass.</li> </ul>	<ul style="list-style-type: none"> <li>See combined response for Comment #48.</li> </ul>
66	Jan. 20, 2016 / PIC Comment Form	<ul style="list-style-type: none"> <li>Noted that improved noise barriers are needed.</li> <li>Hopes this work will be combined with bridge replacements.</li> <li>Commented regarding the area between the O-Train and Preston being</li> </ul>	<ul style="list-style-type: none"> <li>A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>Please be advised that anticipated construction timing of the individual noise barriers walls varies. Where appropriate, construction of the noise barrier retrofits will be coordinated with bridge rehabilitation/replacements as part of the Highway 417 Midtown Bridges project, for which a preliminary design has been completed to date. For example, the proposed noise barrier wall from Bronson Avenue to west of Percy Street on the north side of Highway 417 will be constructed in conjunction with the rehabilitation of the Bronson Avenue overpass. Select walls may be bundled with an upcoming paving project on Highway 417 from Island Park Drive</li> </ul>

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
		<p>considered greenspace and wildlife corridor.</p> <ul style="list-style-type: none"> <li>Interested in notices of sidewalk closures on Preston Street which is a very busy sidewalks at all hours year round.</li> </ul>	<p>to Nicholas Street, scheduled for construction in Spring / Summer 2016. The remaining noise barriers will be constructed in coordination with other work in the Highway 417 corridor.</p> <ul style="list-style-type: none"> <li>Please be advised that the area between the O-Train and Preston Street contains a multi-use pathway but is not identified as a significant wildlife corridor.</li> <li>Please also be advised that sidewalk closures required to accommodate the installation of the proposed noise barrier retrofits across overpass structures will be limited to nightly closures to minimize impacts on pedestrians. The Contractor will be responsible for issuing notices regarding construction of the noise barrier retrofits, including information on construction timing and night work.</li> </ul>
67	Jan. 20, 2016 / PIC Comment Form	<ul style="list-style-type: none"> <li>Expressed support for noise barrier retrofits on the north side of the highway between Bronson and Lyon.</li> <li>Concerned about construction noise impacts.</li> <li>Concerned that acrylic barriers on the south side east of Elgin Street will reflect sound northbound and amplify impact to residents of nearby apartment buildings.</li> <li>Concerned about nightly pedestrian detour from Metcalfe to O'Connor – especially if work is concurrent with any 2016 work for the O'Connor Bikeway.</li> <li>Concerned about sidewalk closures with impacts to residents who walk to Loblaws at night, south of the highway.</li> <li>Requested consideration for deferring nighttime sidewalk closures until 11 pm, temporary crosswalks to</li> </ul>	<ul style="list-style-type: none"> <li>A response was provided by email by MMM on Feb. 5, 2016 and included the following:</li> <li>Regarding construction noise, please be advised that the Contractor will be required to abide by the City of Ottawa Noise By-law for day-to-day operations. Night work is required for the construction of the noise barriers to minimize disruptions to vehicular and pedestrian traffic. A Noise By-law Exemption will be obtained from the City of Ottawa prior to construction.</li> <li>Regarding concerns that the acrylic barriers on the south side of the highway east of Elgin Street will reflect sound northbound, please be advised that under the MTO Noise Policy noise levels and mitigation are measured from Outdoor Living Areas of residential receivers measured at the ground floor. Attenuation for the upper floors of the apartment buildings north of Highway 417 is not covered under the MTO Noise Policy. The area on the north side of Highway 417 between Kent Street and Metcalfe Street will be added as a new candidate site to the Ministry's Noise Barrier Retrofit List this spring. However, this site is not currently included in the Southern Highways Program and construction timing is unknown.</li> <li>Nightly sidewalk closures required to complete overhead work for the installation of noise barriers across structures have been kept to the minimum required for the Contractor to be able to complete the work. The Contractor will be responsible for issuing notices regarding construction of the noise barrier retrofits, including information on construction timing and night work so that members of the general public affected by the proposed works can make adjustments to their travel plans during specific closures.</li> <li>During sidewalk closures, the Contractor will be required to maintain a minimum 1.5 m wide pedestrian facility that is accessible for all pedestrians, including those with disabilities and mobility difficulties. While temporary crosswalks are not planned during sidewalk closures and detours, the Contractor will be responsible for providing temporary pedestrian signings, directional signing, maintenance of sidewalks, and relocations, maintenance and removal of pedestrian barriers to assist in the control of pedestrian traffic and to provide a safe environment for pedestrians during the proposed construction works.</li> </ul>

NO.	DATE / FORM OF CONTACT	COMMENTS RECEIVED	HOW IT WAS ADDRESSED / RESPONSE SENT
		detour pedestrian traffic onto east sidewalk under Metcalfe (south side at Catherine, north side at Isabella).	
68	January 29, 2016 / Email	<ul style="list-style-type: none"> <li>• Commented that project website has not been maintained to include project updates.</li> <li>• Requested information regarding installation policy and the technical description of the proposed noise barriers.</li> <li>• Lives in a neighbourhood that will be affected by the extension of Highway 174's LRT.</li> <li>• Inquired about a potential opportunity to see both overall policy, and its actual implementation for this project.</li> </ul>	<ul style="list-style-type: none"> <li>• The individual was added to the project contact list on February 1, 2016.</li> <li>• A response was provided by MTO on January 29, 2016 and included the following:</li> <li>• Hwy 417 is under the jurisdiction of the Province of Ontario, while Ottawa Road 174 is under the jurisdiction of the City of Ottawa.</li> <li>• The noise barrier project on Highway 417 is being undertaken by the Ministry of Transportation of Ontario, according to provincial policy guidelines. I have attached a copy of the MTO noise policy for your reference.</li> <li>• Please note that the City of Ottawa will have their own noise attenuation policies. As a result, you need to touch base directly with the City of Ottawa for any questions relating to their noise policy or to noise attenuation for city projects on 174.</li> <li>• The display boards from the Ministry's recent Public Information Centre for our project (<a href="http://queenswayexpansioneast.com/wp-content/uploads/2016/01/FINAL-Hwy-417-Noise-Barriers-PIC-Boards_Jan-20-2016.pdf">http://queenswayexpansioneast.com/wp-content/uploads/2016/01/FINAL-Hwy-417-Noise-Barriers-PIC-Boards_Jan-20-2016.pdf</a>) show typical noise barrier installations (see photo inset on the "Recommended Plan" board.)</li> <li>• In MTO applications, noise barriers are typically 5 m in height and constructed using sound absorptive concrete panels. In specific locations, the noise barrier will be made up of clear acrylic panels. Both these are shown on the PIC "Recommended Plan" board.</li> </ul>

### 3 DETAILED DESCRIPTION OF THE RECOMMENDED PLAN

#### 3.1 MAJOR FEATURES OF THE PROPOSED WORKS

New 5 m high ground-mounted and structure-mounted noise barrier walls will be constructed to attenuate noise originating from Highway 417 in the following general locations:

- On the south side of Highway 417 from the O-Train to Preston Street;
- On the north side of Highway 417 from Bronson Avenue to Lyon Street; and
- On the south side of Highway 417 from Lyon Street to the Rideau Canal.

The noise barrier retrofits have been divided into the following individual noise barrier walls, as illustrated in **Exhibit 3-1**:

- Noise Wall 1 – From east of the O-Train to east of Preston Street (south side of Highway 417)
- Noise Wall 2 – From west of Bronson Avenue to west of Percy Street (north side of Highway 417)
- Noise Walls 3A / 3B / 3C – From west of Percy street to east of Lyon Street (north side of Highway 417)
- Noise Wall 4 – From Lyon Street to east of Bank Street (south side of Highway 417)
- Noise Wall 5 – From west of O'Connor Street to east of Metcalfe Street (south side of Highway 417)
- Noise Wall 6 – From east of Metcalfe Street to east of the Rideau Canal (south side of Highway 417)

The Recommended Plan is illustrated in **Exhibit 3-5 to Exhibit 3-11**.

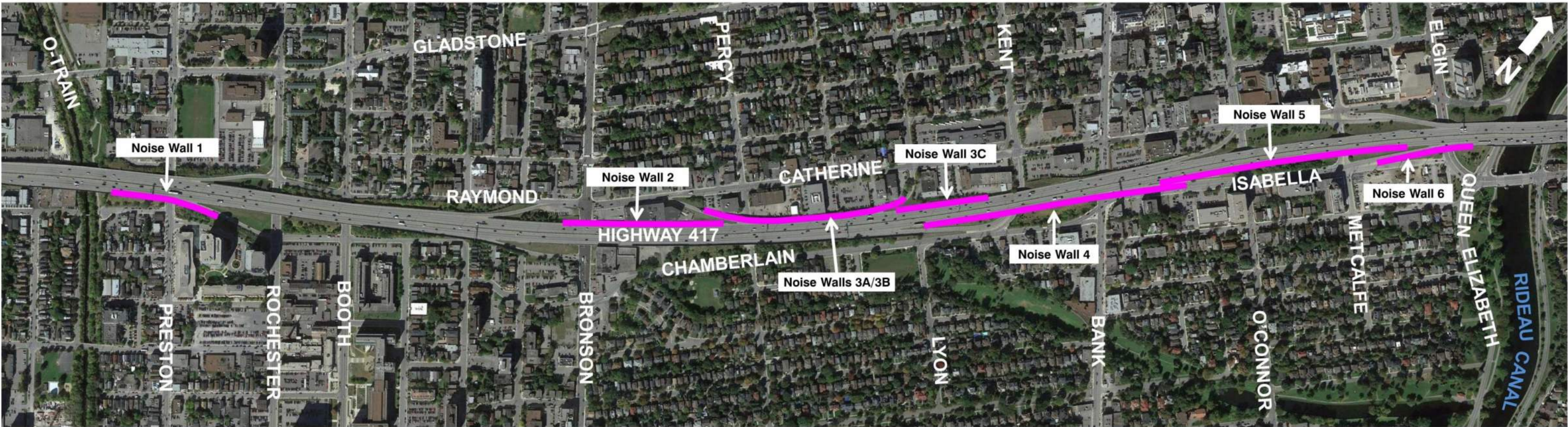
The section of retrofit noise barrier across the Highway 417 Preston Street overpass will be constructed when the structure is rehabilitated.

Noise Wall 2, from west of Bronson Avenue to west of Percy Street, will be constructed when the Highway 417 Bronson Avenue overpass structure is rehabilitated.

The section of retrofit noise barrier across the Highway 417 Percy Street overpass will be constructed when the structure is rehabilitated.



Exhibit 3-1: Location of Noise Barrier Retrofits from the O-Train to the Rideau Canal





### 3.1.1 CONTEXT SENSITIVE DESIGN

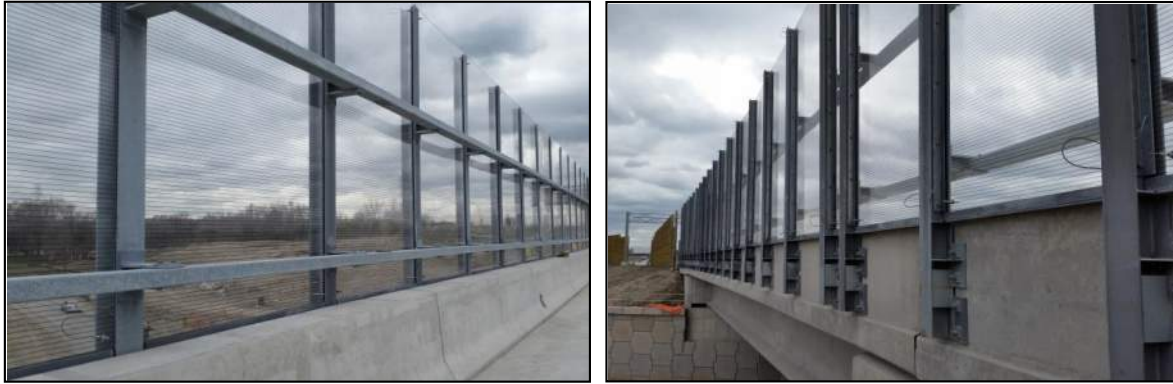
The proposed Highway 417 Noise Barrier Retrofits will incorporate the recommendations documented in the Context Sensitive Design (CSD) study for the Queensway, completed in 2011. The purpose of the CSD study was to ensure that the overall planned expansion and rehabilitation of Highway 417 reflects a holistically-planned aesthetic and vision for the Ottawa Queensway corridor. As a Capital Arrival Route, guiding principles of the CSD concepts for Highway 417 include: clean, concise, discernible features that articulate the highway as an arrival route into / through Ottawa's core as the Nation's Capital; simple design features that are easily perceived without distraction to motor vehicle operators; the use of durable, low maintenance materials; aesthetic design coordinated between the varying CSD elements; and constructible design solutions (MTO & Stantec, 2011).

The major opportunities to incorporate CSD recommendations as part of the Noise Barrier Retrofits project include:

- Use of concrete composite noise barrier panels in keeping with the same hues and tones constructed for the new noise barriers at Lees Avenue as part of the Highway 417 Expansion from Nicholas Street to OR 174 (see **Exhibit 3-2**);
- Use of clear acrylic panels across Highway 417 overpass structures within the project limits to provide a visual connection to the neighbourhood for users of the Queensway (see **Exhibit 3-3**); and
- Use of translucent acrylic panels where noise barriers are required to be installed on top of retaining walls (see **Exhibit 3-4**).



**Exhibit 3-2: Ground-mounted concrete noise barriers; highway side treatment (left) and back side treatment (right) as installed at Lees Avenue**



**Exhibit 3-3: Structure-mounted clear acrylic noise barriers**



**Exhibit 3-4: Structure-mounted translucent acrylic noise barriers**

### **3.1.2 STRUCTURES**

New 5 m high acrylic noise barriers will be mounted on the following existing structures on Highway 417:

- Highway 417 westbound lanes:
  - Across the Bronson Avenue structure and on the retaining wall from Bronson Avenue to west of Percy Street; and
  - On the retaining wall from west of Percy Street to Lyon Street.
- Highway 417 eastbound lanes:
  - Across the Kent Street overpass;
  - Across the Bank Street overpass;
  - On the retaining wall from Bank Street to west of O'Connor Street;
  - Across the O'Connor Street overpass;
  - Across the Metcalfe Street overpass; and
  - Across the Elgin Street overpass.

### **3.1.3 TRAFFIC BARRIERS**

Where ground-mounted noise barriers are proposed to be installed, existing traffic barriers will be removed and reconstructed as precast traffic barriers with integral noise barriers.

### **3.1.4 ATMS**

Existing Advanced Traffic Management System (ATMS) ducts that are located underground, embedded on structures or surface-mounted will be relocated to accommodate the new noise barriers.

### **3.1.5 SIGNAGE**

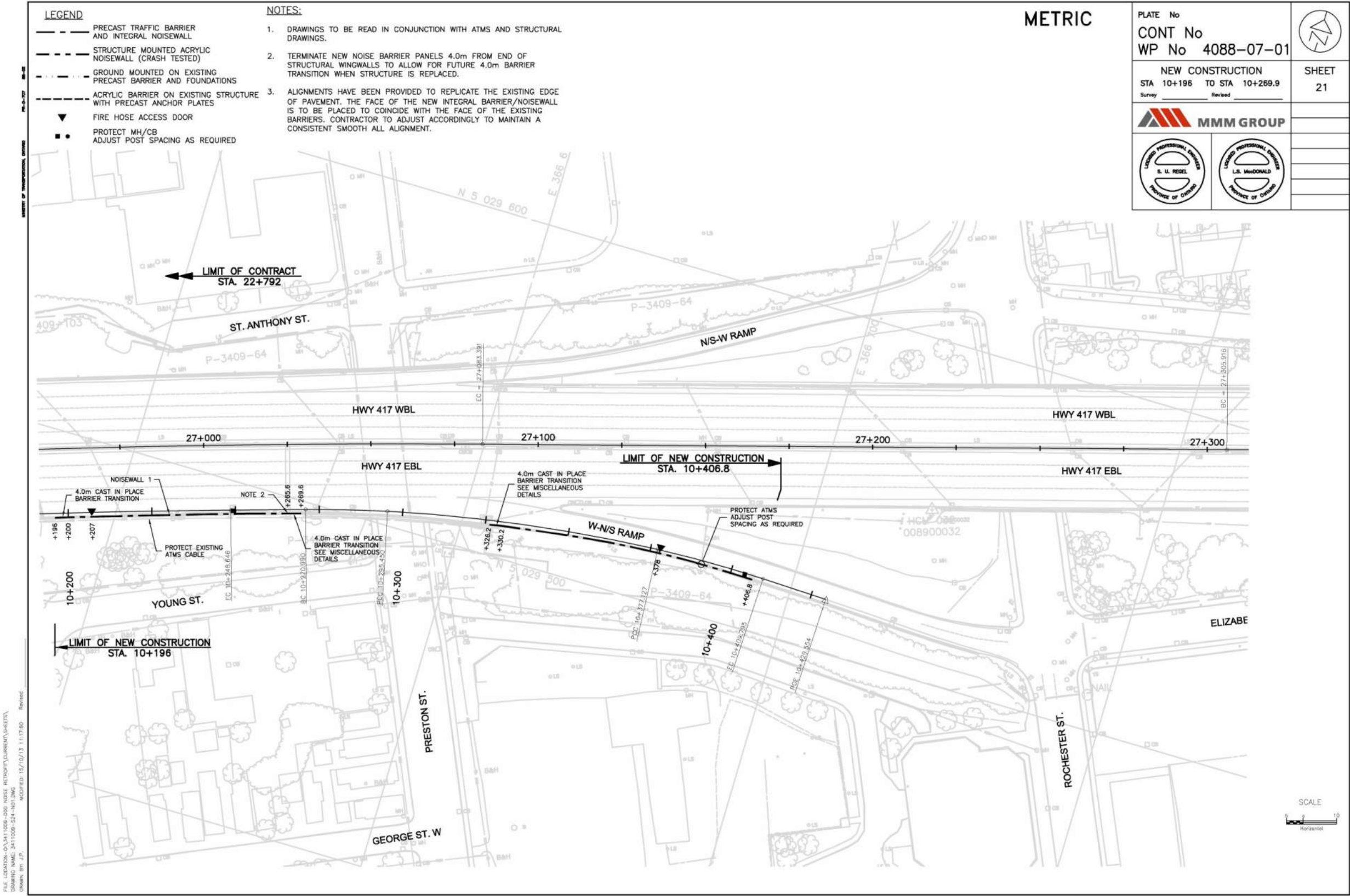
No existing overhead signs on Highway 417 will be affected as part of the proposed works. Existing ground-mounted signs will be removed and relocated to situate them in front of the new noise walls, where required.

### **3.1.6 FENCING**

Existing fencing will be removed and reinstated following construction, where required.



Exhibit 3-5: Recommended Plan – Noise Wall 1



**LEGEND**

- PRECAST TRAFFIC BARRIER AND INTEGRAL NOISEWALL
- STRUCTURE MOUNTED ACRYLIC NOISEWALL (CRASH TESTED)
- GROUND MOUNTED ON EXISTING PRECAST BARRIER AND FOUNDATIONS
- ACRYLIC BARRIER ON EXISTING STRUCTURE WITH PRECAST ANCHOR PLATES
- ▼ FIRE HOSE ACCESS DOOR

**NOTES:**

- DRAWINGS TO BE READ IN CONJUNCTION WITH ATMS AND STRUCTURAL DRAWINGS.
- TERMINATE NEW NOISE BARRIER PANELS 4.0m FROM END OF STRUCTURAL WINGWALLS TO ALLOW FOR FUTURE 4.0m BARRIER TRANSITION WHEN STRUCTURE IS REPLACED.
- ALIGNMENTS HAVE BEEN PROVIDED TO REPLICATE THE EXISTING EDGE OF PAVEMENT. THE FACE OF THE NEW INTEGRAL BARRIER/NOISEWALL IS TO BE PLACED TO COINCIDE WITH THE FACE OF THE EXISTING BARRIERS. CONTRACTOR TO ADJUST ACCORDINGLY TO MAINTAIN A CONSISTENT SMOOTH ALL ALIGNMENT.

**METRIC**

PLATE No  
CONT No  
WP No

NEW CONSTRUCTION  
STA 20+074.5 TO STA 20+250

SHEET 27

**MMM GROUP**

REGISTERED PROFESSIONAL ENGINEER  
S. U. PEGEL  
PROVINCE OF ONTARIO

REGISTERED PROFESSIONAL ENGINEER  
L.S. McDONALD  
PROVINCE OF ONTARIO

**LIMIT OF CONTRACT**  
STA. 22+792

**LIMIT OF NEW CONSTRUCTION**  
STA. 20+074.5

BRONSON ST.

RUE RAYMOND

N/S-W RAMP

W-N/S RAMP

CAMBRIDGE ST. S

ASPH

NOISEWALL 2

P-3409-64

P-3409-70

P-3409-59

P-3409-64

HWY 417 WBL

HWY 417 EBL

ASPH

ASPH

20+000

20+100

20+200

20+250

27+700

27+800

27+900

600

100

200

300

400

500

600

700

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1000

1100

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3



Exhibit 3-7: Highway 417 Noise Barrier Retrofits Recommended Plan – Noise Walls 2, 3A and 3B

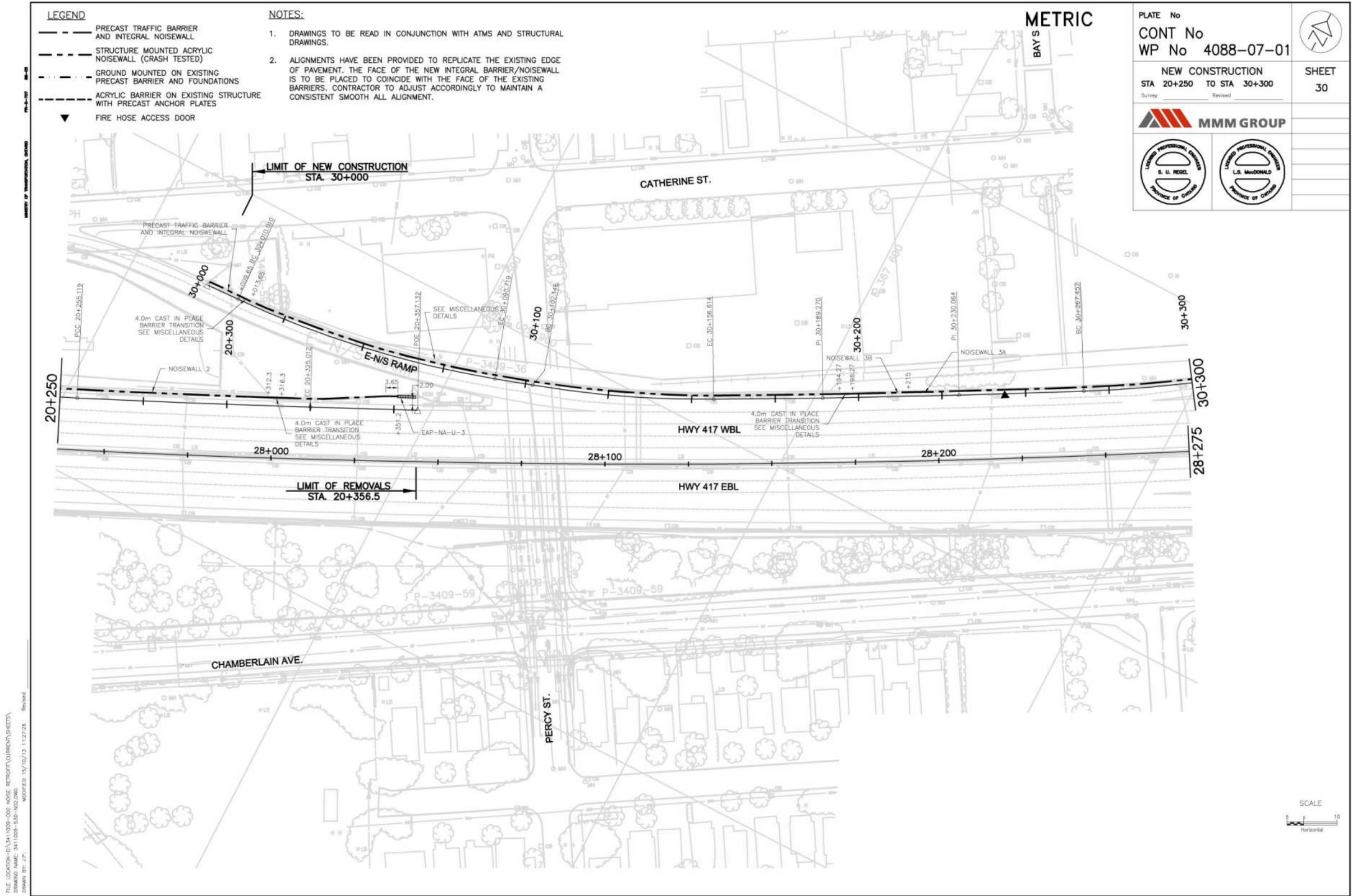


Exhibit 3-8: Highway 417 Noise Barrier Retrofits Recommended Plan – Noise Walls 3A, 3C and 4

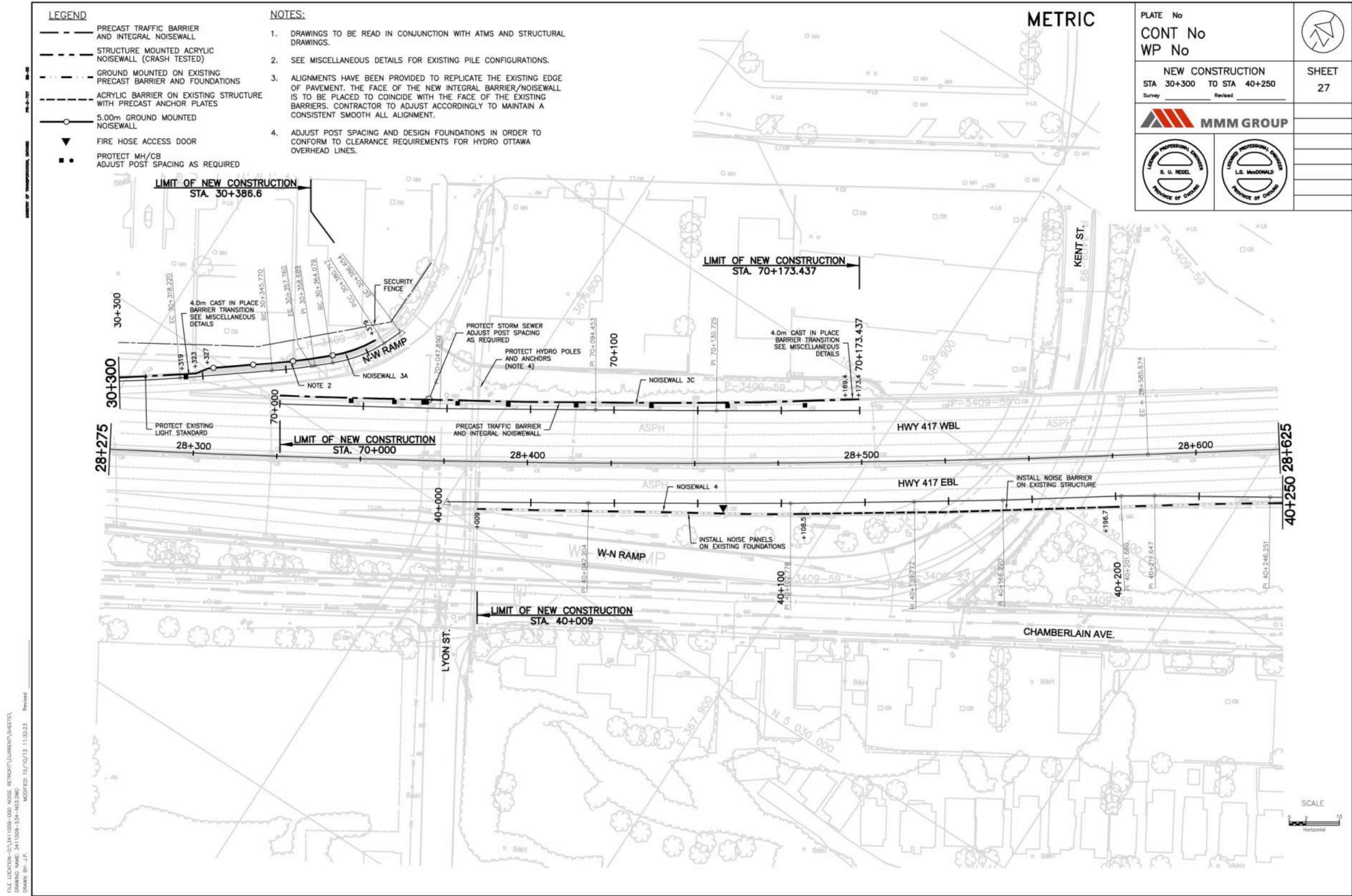
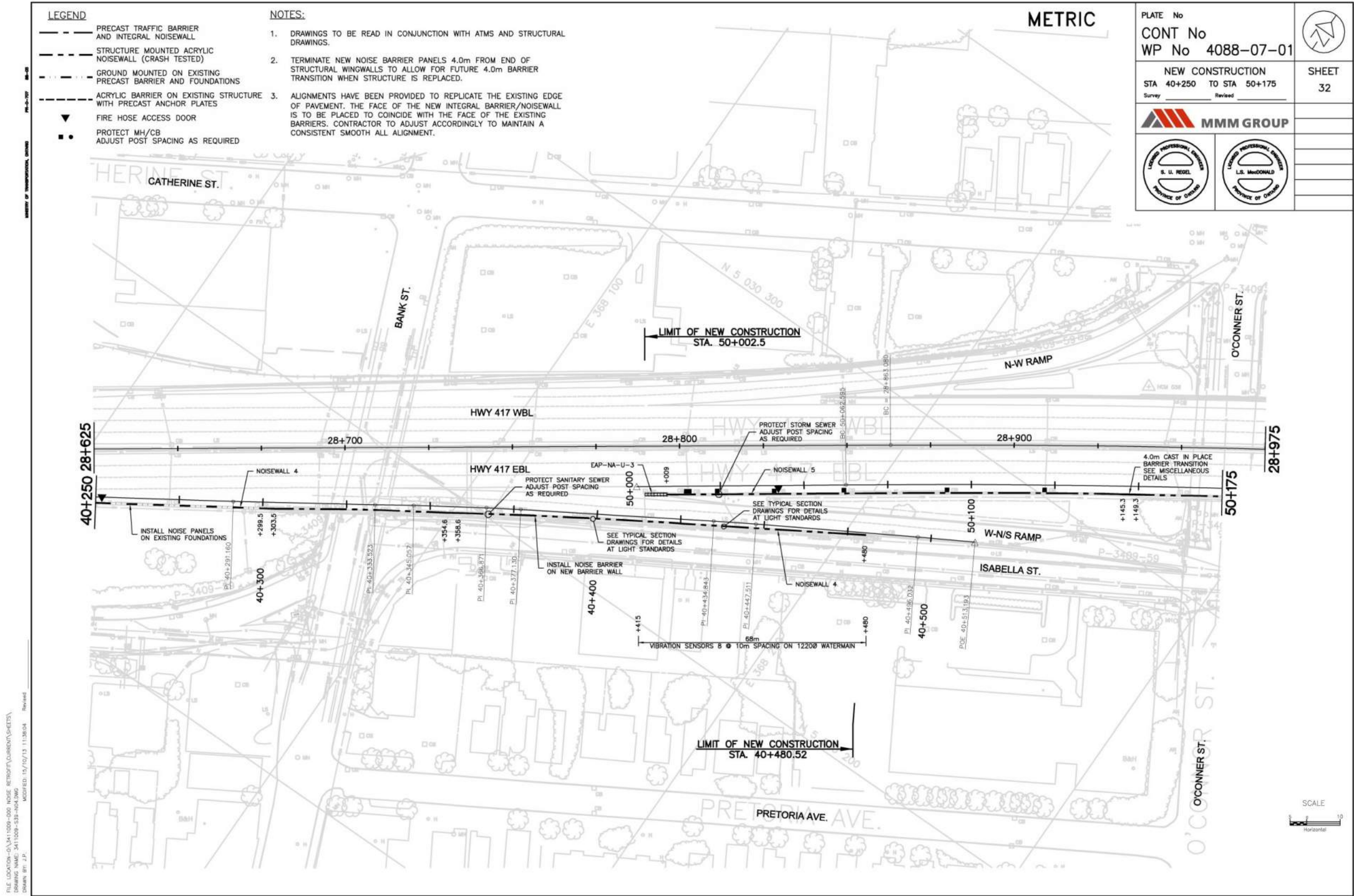




Exhibit 3-9: Highway 417 Noise Barrier Retrofits Recommended Plan – Noise Walls 4 and 5



**LEGEND**

- PRECAST TRAFFIC BARRIER AND INTEGRAL NOISEWALL.
- STRUCTURE MOUNTED ACRYLIC NOISEWALL (CRASH TESTED)
- GROUND MOUNTED ON EXISTING PRECAST BARRIER AND FOUNDATIONS
- ACRYLIC BARRIER ON EXISTING STRUCTURE WITH PRECAST ANCHOR PLATES
- ▼ FIRE HOSE ACCESS DOOR
- PROTECT MH/CB  
ADJUST POST SPACING AS REQUIRED

**NOTES:**

- DRAWINGS TO BE READ IN CONJUNCTION WITH ATMS AND STRUCTURAL DRAWINGS.
- ALIGNMENTS HAVE BEEN PROVIDED TO REPLICATE THE EXISTING EDGE OF PAVEMENT. THE FACE OF THE NEW INTEGRAL BARRIER/NOISEWALL IS TO BE PLACED TO COINCIDE WITH THE FACE OF THE EXISTING BARRIERS. CONTRACTOR TO ADJUST ACCORDINGLY TO MAINTAIN A CONSISTENT SMOOTH ALL ALIGNMENT.

# METRIC

PLATE No		
CONT No		
WP No 4088-07-01		
NEW CONSTRUCTION		
STA 50+175	TO STA 60+175	SHEET 36
MMM GROUP		
 S. U. REGEL PROVINCE OF DUTY	 L.S. McDONALD PROVINCE OF DUTY	

CATHERINE ST.  
METCALFE ST.  
ISABELLA ST.  
PRETORIA AVE.

P-3409-36  
P-3409-59

N 5° 03' 00"

Hwy 417 WBL  
Hwy 417 EBL

STATION MARKS:  
28+975, 29+000, 29+100, 29+200, 29+300, 50+200, 50+300, 50+400, 50+500, 60+100, 60+175

EAP-NA-U-3

LIMIT OF NEW CONSTRUCTION STA. 50+470.8

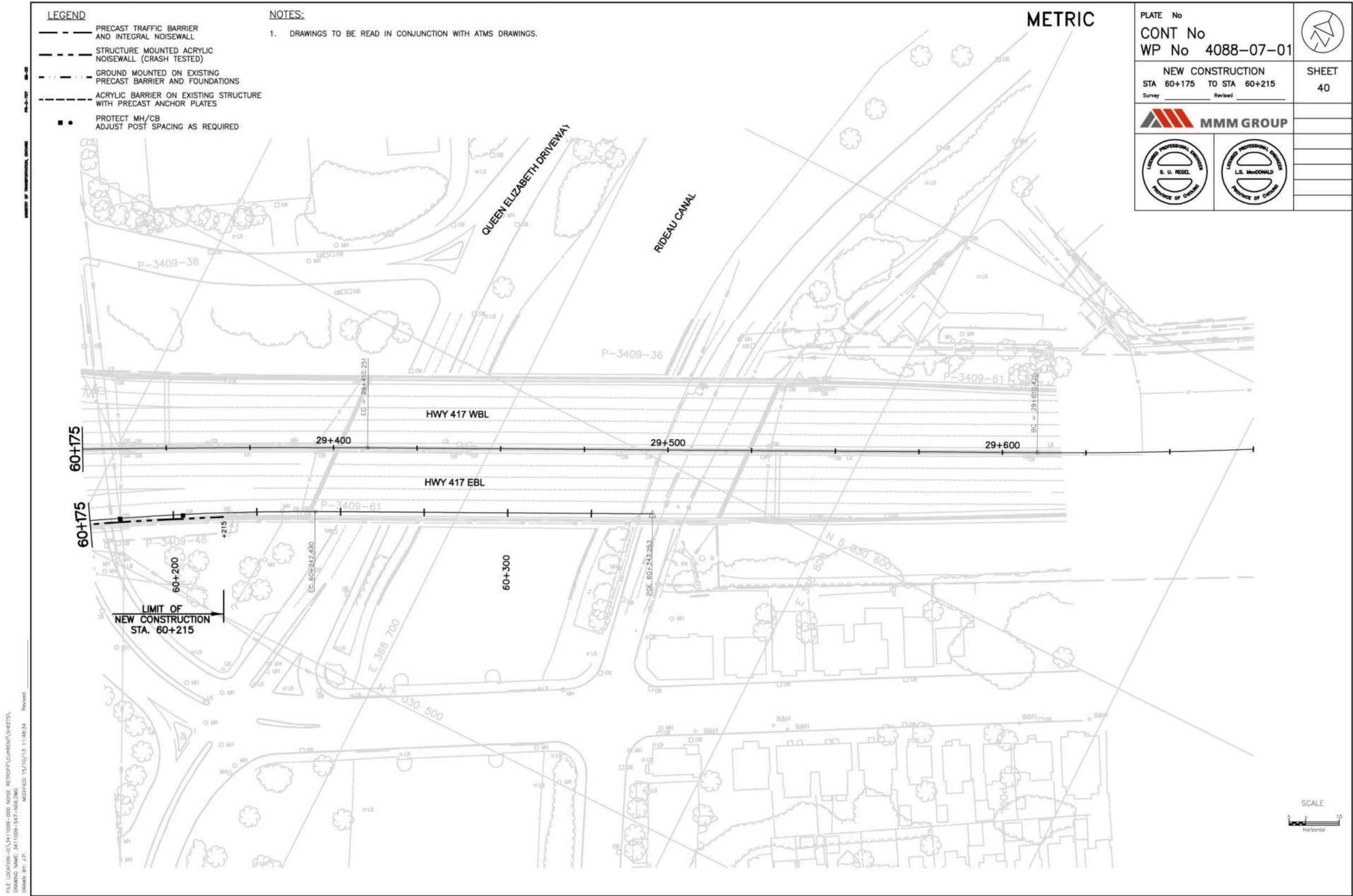
LIMIT OF NEW CONSTRUCTION STA. 60+057

Notes on drawing:  
4.0m CAST IN PLACE BARRIER TRANSITION SEE MISCELLANEOUS DETAILS  
NOISEWALL 5  
PROTECT STORM SEWER ADJUST POST SPACING AS REQUIRED  
PROTECT ATM'S ADJUST POST SPACING AS REQUIRED  
EAP-NA-U-3  
PROTECT ATM'S ADJUST POST SPACING AS REQUIRED  
PROTECT STORM SEWER ADJUST POST SPACING AS REQUIRED  
4.0m CAST IN PLACE BARRIER TRANSITION SEE MISCELLANEOUS DETAILS

SCALE  
Horizontal



Exhibit 3-11: Highway 417 Noise Barrier Retrofits Recommended Plan – Noise Wall 6



### 3.2 CONSTRUCTION AND TRAFFIC STAGING

Construction of the Highway 417 Noise Barrier Retrofits will be completed in multiple work stages over multiple construction seasons. Construction timing for the new noise barriers will depend on the availability of provincial funding and priorities, and will be coordinated with other improvements to the Highway 417 corridor to the extent possible in order to minimize traffic impacts. Proposed construction and traffic staging for the individual noise wall locations is detailed in **Table 3-1**. Work may occur concurrently on individual noise wall locations and on Highway 417 Eastbound and Westbound. See **Section 4.3.4** for the traffic detour routes proposed to mitigate the required sidewalk closures for various noise wall locations, as well as the proposed detour route for the closure of the Highway 417 westbound off-ramp to Bronson Avenue during construction staging for Noise Walls 3A / 3B, as detailed below.



Table 3-1: Construction and Traffic Staging

NOISE WALL	WORK DESCRIPTION	TRAFFIC AND PEDESTRIAN IMPACTS				
		TIME	HIGHWAY 417	RAMPS	CITY STREETS	SIDEWALKS
1	<ul style="list-style-type: none"><li>Full depth removal of pavement on asphalt shoulder</li><li>Remove existing concrete barrier</li><li>Install new integral barrier and noise wall</li></ul>	Day	--	--	--	--
		Night	<ul style="list-style-type: none"><li>1 lane closed Eastbound</li><li>Duration: 8 pm to 6 am for approx. 4 to 6 weeks</li></ul>	<ul style="list-style-type: none"><li>Eastbound Rochester off-ramp closed</li><li>Duration: 8 pm to 6 am for approx. 4 to 6 weeks</li></ul>	--	--
2 / 3A	<ul style="list-style-type: none"><li>Remove asphalt shoulder and existing barrier for ground mounted barrier sections</li><li>Install new integral barrier and noise wall for ground mounted sections</li><li>Full depth removal of asphalt and roadway granulars for structure-mounted portion of Noise Wall 2</li><li>Rehabilitate cantilever portion of Noise Wall 2</li><li>Remove and rehabilitate barrier wall of Noise Wall 2</li><li>Install acrylic noise barrier for Noise Wall 2</li><li>Install ground-mounted noise barrier behind existing barrier wall and retaining wall of structural section of Noise Wall 3A</li></ul>	Day	<ul style="list-style-type: none"><li>Westbound reduced by 1 lane</li><li>Duration: 24 hour basis for approx. 10 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>Westbound Lyon on-ramp closed</li><li>Westbound Bronson off-ramp open</li><li>Duration: 24 hour basis for approx. 10 to 12 weeks</li></ul>	--	--
		Night	<ul style="list-style-type: none"><li>Westbound reduced by 1 lane</li><li>Duration: 24 hour basis for approx. 10 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>Westbound Lyon on-ramp closed</li><li>Westbound Bronson off-ramp open</li><li>Duration: 24 hour basis for approx. 10 to 12 weeks</li></ul>	--	<ul style="list-style-type: none"><li>Noise Wall 2 – During the removal of the existing barrier and all overhead work, alternating sidewalk closures will be required under the Bronson Avenue overpass. One of the sidewalks under the structure will remain open at all times.</li><li>Duration: Nightly for approx. 1 to 3 weeks</li></ul>
3A / 3B	<ul style="list-style-type: none"><li>Remove asphalt shoulder for ground-mounted barrier sections</li><li>Install integral barrier and noise wall for ground-mounted sections</li><li>Remove and rehabilitate barrier wall structural portion of Noise Wall 3B</li><li>Install acrylic noise barrier for Noise Wall 3B</li></ul>	Day	--	<ul style="list-style-type: none"><li>Westbound Bronson off-ramp closed – Signed detour in place directing traffic to the Westbound Metcalfe off-ramp with Airport signage</li><li>Westbound Lyon on-ramp closed</li><li>Duration: 24 hour basis for approx. 6 to 8 weeks</li></ul>	--	--
		Night	--	<ul style="list-style-type: none"><li>Westbound Bronson off-ramp closed – Signed detour in place directing traffic to the Westbound Metcalfe off-ramp with Airport signage</li><li>Westbound Lyon on-ramp closed</li><li>Duration: 24 hour basis for approx. 6 to 8 weeks</li></ul>	--	<ul style="list-style-type: none"><li>Noise Wall 3B – During the removal of the existing barrier and all overhead work, alternating sidewalk closures will be required under the Percy Street overpass. One of the sidewalks under the structure will remain open at all times.</li><li>Duration: Nightly for approx. 1 to 3 weeks</li></ul>

NOISE WALL	WORK DESCRIPTION	TRAFFIC AND PEDESTRIAN IMPACTS				
		TIME	HIGHWAY 417	RAMPS	CITY STREETS	SIDEWALKS
3C	<ul style="list-style-type: none"> <li>Remove asphalt shoulder and existing barrier</li> <li>Install new integral barrier and noise wall</li> </ul>	Day	--	--	--	--
		Night	<ul style="list-style-type: none"> <li>Westbound reduced by 1 lane</li> <li>Duration: 8 pm to 6 am for approx. 3 to 4 weeks</li> </ul>	<ul style="list-style-type: none"> <li>Westbound O'Connor on-ramp / auxiliary lane closed</li> <li>Duration: 8 pm to 6 am for approx. 3 to 4 weeks</li> </ul>	--	--
4	<ul style="list-style-type: none"> <li>Install noise wall panels from STA. 28+384.7 to STA. 28+482.6 and from STA. 28+572.2 to STA. 28+670</li> <li>Remove and rehabilitate barrier walls on all structures and retaining walls</li> <li>Erect noise barrier panels</li> <li>Install precast barrier and integral noise wall and footings at all ground-mounted locations</li> <li>Install acrylic noise panels on Kent St. structure and retaining walls</li> </ul>	Day	<ul style="list-style-type: none"> <li>All lanes open Eastbound</li> </ul>	<ul style="list-style-type: none"> <li>1 lane must exit and 1 either/or lane open at Eastbound Kent off-ramp</li> <li>Eastbound Metcalfe off-ramp open</li> </ul>	--	--
		Night	<ul style="list-style-type: none"> <li>1 lane open Eastbound to STA. 28+600</li> <li>Duration: 8 pm to 6 am for approx. 2 to 3 weeks</li> <li>2 lanes open Eastbound from STA. 28+600 easterly</li> <li>Duration: 8 pm to 6 am for approx. 8 to 12 weeks</li> </ul>	<ul style="list-style-type: none"> <li>2 lanes forced off at Eastbound Kent off-ramp</li> <li>Eastbound Metcalfe off-ramp closed</li> <li>Duration: 8 pm to 6 am for approx. 2 to 3 weeks</li> <li>Westbound Kent on-ramp closed for erection of panels across ramps</li> <li>Duration: 8 pm to 6 am for approx. 1 week</li> </ul>	--	<ul style="list-style-type: none"> <li>During the installation of the noise barrier panels over the West Chamberlain Avenue to Kent Street North lane, the sidewalk on the east side of Kent will be closed under the overpass. Pedestrian traffic will be directed to Bank Street along the sidewalk on the south side of Chamberlain.</li> <li>Duration: Nightly for approx. 1 week</li> </ul>
4 / 5	<ul style="list-style-type: none"> <li>Install noise wall panels from STA. 28+625 to STA. 28+678.8</li> <li>Repair concrete fascia on retaining walls</li> <li>Place anchors and concrete for noise barrier support posts on retaining walls</li> <li>Erect noise barrier panels</li> <li>Install precast barrier and integral noise wall and footings at all ground mounted locations</li> </ul>	Day	<ul style="list-style-type: none"> <li>All lanes open Eastbound</li> </ul>	<ul style="list-style-type: none"> <li>1 lane must exit and 1 either/or lane open at Eastbound Kent off-ramp</li> <li>Eastbound Metcalfe off-ramp open</li> </ul>	<ul style="list-style-type: none"> <li>2 lanes on Isabella Street with 3.5 m lane widths</li> <li>Duration: 24 hour basis for approx. 8 to 12 weeks</li> </ul>	--
		Night	<ul style="list-style-type: none"> <li>2 lanes open Eastbound</li> <li>9 pm to 6 am: single lane reduction</li> <li>10 pm to 6 am: two lane reduction</li> <li>Duration: Approx. 8 to 12 weeks</li> </ul>	<ul style="list-style-type: none"> <li>2 lanes forced off at Eastbound Kent off-ramp</li> <li>Eastbound Metcalfe off-ramp closed</li> <li>Duration: 9 pm to 6 am for approx. 8 to 12 weeks</li> </ul>	<ul style="list-style-type: none"> <li>Isabella Street reduced to 1 lane</li> <li>Duration: 8 pm to 6 am for approx. 8 to 12 weeks</li> <li>Lane reductions on Bank Street as required to complete overhead work</li> <li>Duration: 9 pm to 7 am for approx. 1 week</li> </ul>	<ul style="list-style-type: none"> <li>During the removal of the existing barrier and all overhead work, alternating sidewalk closures will be required under the Bank Street overpass. One of the sidewalks will remain open at all times under the structure.</li> <li>Duration: Nightly for approx. 1 to 3 weeks</li> </ul>
5	<ul style="list-style-type: none"> <li>Install noise wall panels from STA. 50+003 to STA. 50+472</li> <li>Repair concrete fascia on structures and retaining walls</li> <li>Place anchors and concrete for noise barrier support posts on structures and retaining walls</li> <li>Erect noise barrier panels</li> <li>Install precast barrier and integral noise wall and footings at all ground mounted locations</li> </ul>	Day	<ul style="list-style-type: none"> <li>All lanes open Eastbound</li> </ul>	<ul style="list-style-type: none"> <li>1 lane must exit and 1 either/or lane open at Eastbound Kent off-ramp</li> <li>Eastbound Metcalfe off-ramp open</li> </ul>	<ul style="list-style-type: none"> <li>2 lanes on Isabella Street with 3.5 m lane widths</li> <li>Duration: 24 hour basis for approx. 8 to 12 weeks</li> </ul>	--
		Night	<ul style="list-style-type: none"> <li>2 lanes open Eastbound</li> <li>Duration: 8 pm to 6 am for approx. 8 to 12 weeks</li> </ul>	<ul style="list-style-type: none"> <li>2 lanes forced off at Eastbound Kent off-ramp</li> <li>Eastbound Metcalfe off-ramp closed</li> <li>Duration: 8 pm to 6 am for approx. 8 to 12 weeks</li> </ul>	<ul style="list-style-type: none"> <li>Isabella Street reduced to 1 lane</li> <li>Metcalfe north and south of Isabella closed for barrier wall removal, post and panel installation</li> <li>Duration: 8 pm to 6 am for approx. 8 to 12 weeks</li> <li>Lane reductions on O'Connor Street and Metcalfe Street as required to</li> </ul>	<ul style="list-style-type: none"> <li>During the removal of the existing barrier and all overhead work, alternating sidewalk closures will be required under the O'Connor Street overpass. One of the sidewalks will remain open at all times under the structure.</li> <li>Duration: Nightly for approx. 1 to 3</li> </ul>

NOISE WALL	WORK DESCRIPTION	TRAFFIC AND PEDESTRIAN IMPACTS				
		TIME	HIGHWAY 417	RAMPS	CITY STREETS	SIDEWALKS
					complete overhead work <ul style="list-style-type: none"><li>Duration: 9 pm to 7 am for approx. 1-2 weeks</li></ul>	weeks <ul style="list-style-type: none"><li>During the removal of the existing barrier and all overhead work, the west sidewalk will be closed under the Metcalfe Street overpass.</li><li>Pedestrian traffic will be directed to O'Connor via the sidewalks on the south side of Catherine or Isabella.</li><li>Duration: Nightly for approx. 1 to 3 weeks</li></ul>
6	<ul style="list-style-type: none"><li>Install precast barrier and integral noise wall and footings at ground mounted wall locations</li><li>Rehabilitate structural and retaining wall overhang fascia, install base plate parts and acrylic noise panels</li></ul>	Day	<ul style="list-style-type: none"><li>All lanes open Eastbound</li></ul>	<ul style="list-style-type: none"><li>Eastbound Metcalfe on-ramp reduced to one lane</li><li>Duration: 24 hour basis for approx. 10 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>All lanes on Isabella open</li><li>Lane 2 revised to left turn only</li><li>Lane 3 access to 417 Eastbound</li><li>Duration: 24 hour basis for approx. 10 to 12 weeks; Police presence for traffic control during PM peak</li></ul>	--
		Night	<ul style="list-style-type: none"><li>All lanes open Eastbound</li></ul>	<ul style="list-style-type: none"><li>Eastbound Metcalfe on-ramp reduced to one lane</li><li>Duration: 24 hour basis for approx. 10 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>All lanes on Isabella open</li><li>Lane 2 revised to left turn only</li><li>Lane 3 access to 417 Eastbound</li><li>Duration: 24 hour basis for approx. 10 to 12 weeks</li><li>Lane reductions on Elgin Street as required to complete overhead work</li><li>Duration: 9 pm to 7 am for approx. 1 week</li></ul>	<ul style="list-style-type: none"><li>During the removal of the existing barrier and all overhead work, alternating sidewalk closures will be required under the Elgin Street overpass. At least one of the sidewalks will be maintained under the structure.</li><li>In the case of the closure of the west sidewalk, pedestrian traffic will be directed south along the extension of Elgin to Pretoria / Queen Elizabeth in order to cross to the east sidewalk under the structure.</li><li>Duration: Nightly for approx. 1 to 3 weeks</li></ul>

## 4 ENVIRONMENTAL ISSUES AND COMMITMENTS

This section details the assessment of direct and indirect environmental impacts associated with the proposed works. This section also describes the mitigation measures developed to minimize the impacts identified for each environmental issue. Mitigation measures include planning decisions, design features, construction requirements and constraints.

The key to ensuring effective environmental quality control and risk management during the project is the development and proactive implementation of an approach that:

- Identifies environmental sensitivities;
- Presents environmental protection measures in a way that can be translated into contractual requirements and for which compliance can be verified; and
- Includes a monitoring program, as required, that verifies that environmental protection measures are being implemented and are effective.

A key component of the mitigation strategy is ensuring that the Contract Administrator and Contractor are made aware of, and are prepared to deal with, all environmental issues that may arise during construction.

Environmental contract specifications, including standard and non-standard special provisions (SP), Ontario Provincial Standard Specifications (OPSS) and MTO General Conditions of Contract, will be included in the contract documents to address specific environmental and operational concerns.

### 4.1 PROJECT BACKGROUND (PRELIMINARY DESIGN)

As noted previously, the Preliminary Recommended Plan was documented in a Transportation Environmental Study Report (TESR), which received environmental clearance in 2008. The TESR documents environmentally significant aspects of the proposed plan and was supported by a series of technical reports including:

- Botanical and Wildlife Survey Report;
- Fisheries and Aquatic Ecosystem Technical Report;
- Social, Land Use and Planning Review Report;
- Landscape Documentation;
- Noise Report;
- Air Quality Assessment; and
- Stage 1 and Stage 2 Archaeological Assessment Reports.

The key environmental concerns and commitments identified in the TESR for the entire preliminary design study area (Highway 417 from west of Highway 416 easterly to Anderson Road) are summarized in **Table 4-1**. This table has been provided as a reference and forms the foundation upon which the impact assessment of the detail design for this project was developed.

Table 4-1: Preliminary Design Summary of Environmental Concerns and Commitments

I.D.#	Issue/Concern Potential Effects	Responsible Agencies	I.D. #	Mitigation/Protection/ Monitoring
1.0	Fisheries and Aquatic Ecosystems <ul style="list-style-type: none"> <li>Impact on fish habitat at culvert extension/pier extension locations identified as fish habitat.</li> <li>Impact on fish habitat due to sediment and debris entry into watercourses.</li> </ul>	Department of Fisheries and Oceans	1.1	<ul style="list-style-type: none"> <li>Habitat improvements to be determined at the detail design phase. Typical measures to be considered include: embedment of culverts, use of natural stream substrate or other suitable river stone material in culvert bottom, placement of riparian plantings at disturbed areas at culvert ends and placement of 300 mm thickness of river stone around pier extensions.</li> </ul>
			1.2	<ul style="list-style-type: none"> <li>Restrict timing of in-stream works.</li> </ul>
			1.3	<ul style="list-style-type: none"> <li>Prepare fisheries compensation plan during Detailed Design and submit to DFO for approval; secure Federal Fisheries Act authorization during Detailed Design.</li> </ul>
			1.4	<ul style="list-style-type: none"> <li>Prevent sediment and debris from entering watercourse through the use of silt fencing, check dams and other suitable debris capture devices.</li> </ul>
			1.5	<ul style="list-style-type: none"> <li>Restrict activities adjacent to watercourses (i.e. storage of materials, refuelling, excess materials placement).</li> </ul>
			1.6	<ul style="list-style-type: none"> <li>Monitor construction activities. Monitor environmental protection measures and implementation of the fisheries compensation plan.</li> </ul>
2.0	Terrestrial Ecosystems <ul style="list-style-type: none"> <li>Impact on wildlife habitats due to removal of vegetation at the edge of forested areas.</li> </ul>	Ministry of Natural Resources	2.1	<ul style="list-style-type: none"> <li>Minimize vegetation removal; provide protection for those trees to remain.</li> </ul>
			2.2	<ul style="list-style-type: none"> <li>Replace vegetation where feasible (refer to landscape concept plan).</li> </ul>
3.0	Surface Water / Stormwater Management <ul style="list-style-type: none"> <li>Increased runoff resulting from increased pavement platform.</li> </ul>	City of Ottawa, Ministry of Transportation	3.1	<ul style="list-style-type: none"> <li>Reduce runoff to pre-construction rates by using underground detention.</li> </ul>
4.0	Land Use <ul style="list-style-type: none"> <li>Property acquisition required (including homes on Westmont Avenue and Concord Street).</li> <li>Traffic delays / access restrictions during construction.</li> <li>Impact on emergency service routes/access.</li> <li>Restricted pedestrian/cyclist passage at the Queensway.</li> </ul>	City of Ottawa, Ministry of Transportation	4.1	<ul style="list-style-type: none"> <li>Compensation for property acquisition will be negotiated through the property acquisition process.</li> </ul>
			4.2	<ul style="list-style-type: none"> <li>Prepare Traffic Management Plan during Detailed Design.</li> </ul>
			4.3	<ul style="list-style-type: none"> <li>Maintain existing number of lanes on Queensway at most times during construction except for minor reductions during off-peak travel times. Preclude lane restrictions during peak travel times.</li> </ul>
			4.4	<ul style="list-style-type: none"> <li>Implement elements of an Advanced Traffic Management System including changeable message signs and incident detection.</li> </ul>
			4.5	<ul style="list-style-type: none"> <li>Maintain access to businesses and other properties throughout construction.</li> </ul>
			4.6	<ul style="list-style-type: none"> <li>Ensure ongoing communication with emergency services during construction.</li> </ul>
			4.7	<ul style="list-style-type: none"> <li>Free flow channelizations at ramp terminals removed, where possible, to better accommodate pedestrians/cyclists.</li> </ul>
			4.8	<ul style="list-style-type: none"> <li>Accommodate the needs of pedestrians and cyclists during construction. Do not close adjacent crossings at the same time during construction. Provide alternative route and advance warning during temporary closures.</li> </ul>
5.0	Contaminated Property / Waste Management <ul style="list-style-type: none"> <li>Encroachment onto potentially contaminated properties.</li> </ul>	Ministry of Environment, Ministry of Transportation	5.1	<ul style="list-style-type: none"> <li>Undertake Environmental Site Assessment for properties to be acquired during Detailed Design.</li> </ul>

I.D.#	Issue/Concern Potential Effects	Responsible Agencies	I.D. #	Mitigation/Protection/ Monitoring
6.0	Aesthetics/Landscape Composition <ul style="list-style-type: none"> <li>Loss of vegetation to accommodate Recommended Design.</li> <li>Effect on visual landscape and scenic resources available to motorists.</li> <li>Effect on adjacent dwellers sensitive to views of facility.</li> </ul>	City of Ottawa, National Capital Commission, Ministry of Transportation	6.1	<ul style="list-style-type: none"> <li>Minimize vegetation removal; provide protection for those trees to remain.</li> </ul>
			6.2	<ul style="list-style-type: none"> <li>Replace vegetation where feasible (refer to landscape concept plan).</li> </ul>
			6.3	<ul style="list-style-type: none"> <li>Enhance aesthetic design of bridges and retaining walls, where appropriate, to minimize visual intrusion.</li> </ul>
			6.4	<ul style="list-style-type: none"> <li>Provide visual screening and aesthetic enhancement through landscape design (refer to landscape concept plan).</li> </ul>
			6.5	<ul style="list-style-type: none"> <li>Develop final landscape plan during detail design, in consultation with the City and NCC, and present to the public at that time.</li> </ul>
7.0	Noise <ul style="list-style-type: none"> <li>Increase in noise levels adjacent for highway as a result of proposed highway modifications.</li> <li>Noise from construction equipment and vehicles during construction</li> </ul>	City of Ottawa, Ministry of Transportation	7.1	No further action required. Mitigation not required since the increase in noise levels is less than 5 dBA for the proposed road modifications relative to the future noise levels without the road modifications
			7.2	<ul style="list-style-type: none"> <li>Require contractor to maintain equipment in an operating condition that prevents unnecessary noise, including but not limited to non-defective muffler systems, properly secured components and the lubrication of moving parts.</li> </ul>
			7.3	<ul style="list-style-type: none"> <li>Restrict idling of equipment to the minimum necessary to perform the specified work.</li> </ul>
8.0	Air Quality <ul style="list-style-type: none"> <li>Potential detrimental impact on air quality (increased smog/pollutants).</li> </ul>	Ministry of Environment Environment Canada Health Canada	8.1	<ul style="list-style-type: none"> <li>No further action required. Mitigation not required since concentration levels of contaminants predicted to be below applicable government guidelines for the Recommended Design. Furthermore, results of regional air quality assessment concluded that the proposed modifications to Highway 417 would have an unnoticeable and insignificant impact on smog pollutants.</li> </ul>
9.0	Archaeology <ul style="list-style-type: none"> <li>Stage 1 and Stage 2 assessments did not indicate any concerns for significant pre-contact or historic archaeological sites.</li> </ul>	Ministry of Culture	9.1	<ul style="list-style-type: none"> <li>Include contract provisions that require that the Ministry of Culture be notified immediately if deeply buried archaeological remains are encountered during construction.</li> </ul>

A detailed description of existing environmental conditions is documented in the TESR. Updated information on existing environmental conditions and sensitivities, as well as potential impacts and proposed mitigation, are summarized in the following sections.

## 4.2 NATURAL ENVIRONMENT

Landscape composition within the project limits was determined through a review of aerial photography and background documents including the 2007 TESR.

At the initiation of detail design, the Botanical and Wildlife Survey Report (Blythe and Associates, 2003) prepared during preliminary design was reviewed to obtain background information on the terrestrial environment within the project limits. The botanical and wildlife surveys conducted during preliminary design were carried out in early summer from June 17th to 20th, 2002.

Species at Risk records (based on the *Endangered Species Act*, 2007), were also obtained from the Ministry of Natural Resources and Forestry's (MNRF) Natural Heritage Information Centre (NHIC) database, in order to update the previous data from the MNRF's Vulnerable, Threatened, Endangered and Extirpated List (VTEE OMNR, 1996). In addition, a letter giving notice of Study Commencement was sent to MNRF Kemptville District on March 8, 2013. Additional correspondence with the Kemptville District MNRF provided background data on existing Species at Risk and significant natural features and areas (i.e. Provincially Significant Wetlands [PSW], Areas of Natural and Scientific Interest [ANSI], etc.) in the project limits.

During preliminary design, terrestrial ecosystem sensitivities were identified and evaluated within the study area from Highway 416 easterly to Anderson Road. A reconnaissance terrestrial field survey was conducted on April 25 and 26, 2002 to determine the nature and extent of botanical and wildlife habitats within the study area, with follow-up detailed assessment surveys conducted from June 17 to 20, 2002.

The results of these surveys and a preliminary assessment of potential impacts were documented in the *Botanical and Wildlife Survey Report* (January 2003). During detail design, these reports were reviewed to obtain background information on the terrestrial environment within the study corridor. As there are no watercourses within the Highway 417 Noise Barrier Retrofits project limits, background information on fish and fish habitat collected during preliminary design was not reviewed.

The following sections describe updated existing environmental conditions and sensitivities, potential impacts to the natural environment associated with the proposed works, and the mitigation measures that have been included in the contract documents.

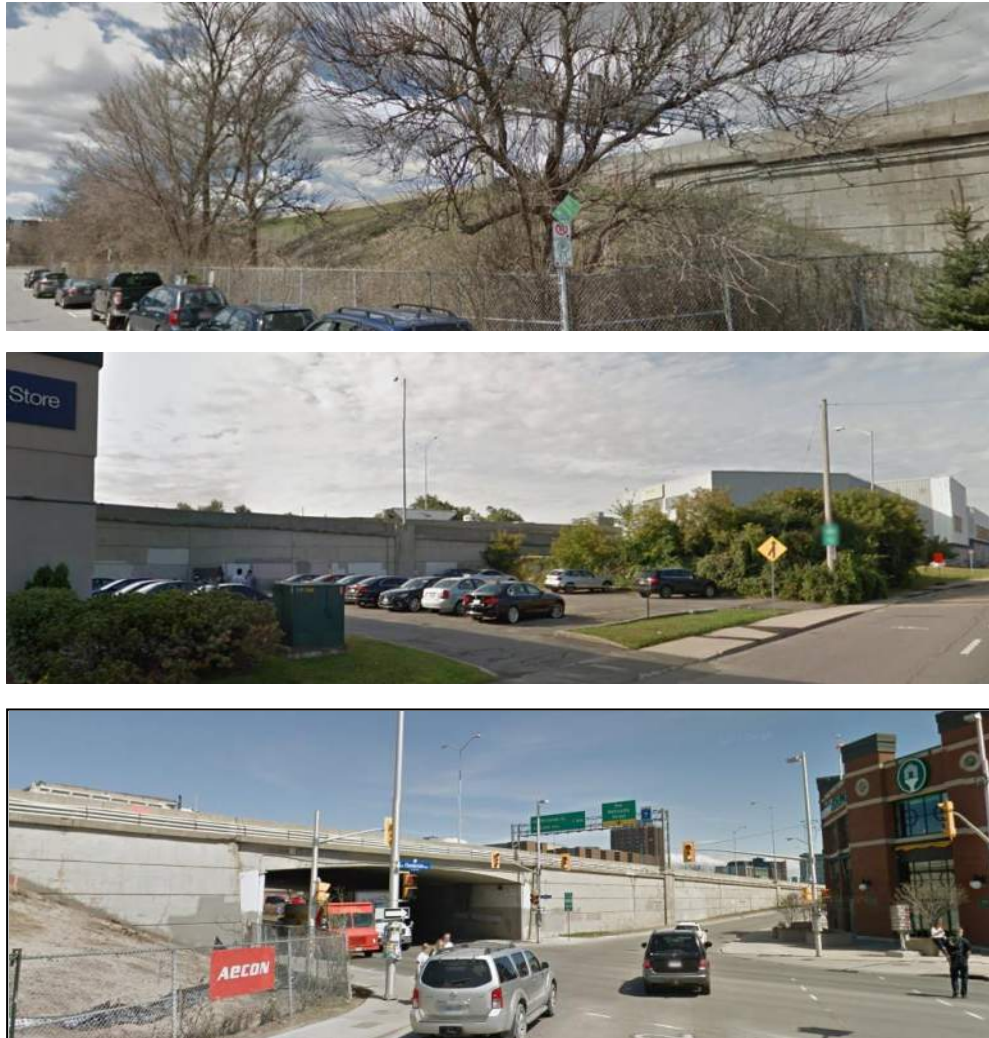
### 4.2.1 TERRESTRIAL ECOSYSTEMS

#### 4.2.1.1 Vegetation

According to the Botanical and Wildlife Survey Report (2003), there were no records of regionally or provincially rare species of plants or animals within the project limits based on a review of the previous VTEE (OMNR, 1996). In addition, there are no Areas of Natural and Scientific Interest (ANSIs) or other natural features of interest within the project limits.



Vegetation in the vicinity of the proposed noise barrier retrofits along Highway 417 is limited. Other vegetation in close proximity to the highway within the project limits occurs on private commercial and residential properties, and in City parks. Existing vegetation in the immediate vicinity of the highway within the project limits is considered to be of low ecological significance (TESR, MTO, 2007). As shown in **Exhibit 4-1** and **Exhibit 4-2**, a small amount of landscaped / cultural vegetation exists within the vicinity of the proposed noise barrier retrofit locations. Discontinuous grass, landscape trees and plantings are predominant along the Highway 417 right-of-way.



**Exhibit 4-1: Landscape composition in the vicinity of the proposed noise barrier retrofits**

*Top: Typical landscaped/cultural vegetation and downward slopes on the south side of Highway 417 along Young Street, looking west. Middle: Landscaping on commercial property on the north side of Highway 417 along Catherine Street, looking west. Bottom: Typical urban conditions on the south side of Highway 417 at Bank Street, looking east.*





**Exhibit 4-2: Existing terrestrial vegetation within the project limits**

A review of the NHIC database identified provincially significant plant species, outlined in **Table 4-2**, as having historical or current records within the general project limits.

**Table 4-2: Provincially Significant Plant Species**

Plant Species	Habitat
Cattail Sedge	Moist woods along or near watercourses.
Ram's-head Lady's-slipper	Sand woodlands, slightly shaded areas in open limestone barrens, and in mossy coniferous swamps.
American Waterwort	Shallow water, river edges, and shores of ponds, lakes, pools and ditches.
Limestone Oak Fern	Ledges and slopes in limestone or dolomite rock, and in moist humus in forests on calcareous rock.
Greene's Rush	Open sandy ground.
Southern Twayblade	Bogs, openings in Sphagnum peatlands with Black Spruce and Tamarack.
Pale-bellied Frost Lichen	In wooded areas, on the bark of hardwood trees such as White Ash, Black Walnut, and American Elm, and on Hop Hornbeam trunks.
Pitch Pine	Shallow soil of dry rock outcrops and ridges.
Woodland Pinedrops	Mixed woods.
Lurking Leskea	Hardwood swamps or other marshy habitats subject to flooding.

Based on the review of existing conditions during detail design, suitable habitat for these plant species is not present within the project limits.

A letter of Study Commencement was sent to the Ministry of Natural Resources and Forestry (MNRF) Kemptville District on March 8, 2013. MNRF provided response on November 25, 2013, which indicated that potential plant Species at Risk which may be found in the vicinity of the proposed noise barrier retrofit locations include Butternut (Threatened).

### Potential Impacts

As existing vegetation is considered to be of low ecological significance, minor vegetation removals required to complete the installation of the proposed noise barrier retrofits are anticipated to have negligible impacts.

Butternuts usually occur alone or in small groups in deciduous forests, and are often found along streams with moist, well-drained soil, and are also found on well-drained gravel sites and rarely on dry rocky soil. No butternuts were observed in the project limits and therefore will not be affected by the minor removal of landscaped / cultural vegetation required for the proposed works.

### Mitigation

The following design and site-specific mitigation measures will be included in the Contract documents to mitigate impacts to vegetation:

- Vegetation removal and grading will be minimized to only what is required for the proposed works;
- Required vegetation removal and protection measures will be conducted in accordance with OPSS 201 (tree clearing);
- The use of appropriate vegetation clearing techniques will be employed and exposed surfaces will be re-stabilized and re-vegetated as soon as possible following construction; and
- All construction-related debris will be cleared / disposed of following construction.

#### **4.2.1.2 Wildlife and Species at Risk**

### Wildlife

As the area within the project limits is highly urbanized, wildlife habitat is limited. A small amount of landscaped vegetation along the highway may provide limited habitat for urban tolerant wildlife. No significant wildlife features, significant wildlife habitat, or significant wildlife species, including area sensitive birds and other provincially rare species were identified within the project limits.

Migratory birds as well as common resident species may use bridges within the project limits for nesting.

### Species at Risk

Correspondence with MNRF indicated that potential wildlife Species at Risk which may be found in the vicinity of the proposed noise barrier retrofit locations include Barn Swallow (Threatened) and Chimney Swift (Threatened).

### Potential Impacts

In general, suitable habitat for the potential Species at Risk that may occur in the project limits is not present, as the area is highly urbanized and these species were not observed during site reconnaissance.

Barn swallows typically build their cup-shaped nests on structures such as open barns, under bridges, and in culverts, and often re-use their nests from year to year. Impacts to any potential Barn swallow habitat within the project limits are not anticipated.

Chimney swifts are most likely to be found nesting or roosting in chimneys and other manmade structures, and prefer to stay close to water for insect food sources. The proposed works will not affect any suitable habitat for this species that may be present within the project limits.

### Mitigation

The following mitigation measures will be included in the Contract documents to protect wildlife during construction:

- The Contractor will not harass or kill any wildlife encountered during construction;
- Should any Species at Risk be encountered at any time in the construction area, the Contract Administrator will be contacted immediately.
- The Contractor will not destroy the active nests (nests with eggs or young birds), or wound or kill birds, of species protected under the *Migratory Birds Convention Act, 1994* and/or Regulations under that Act. If active nests are encountered, the Ministry's Contract Administrator should be contacted; and
- The Contractor should refrain from clearing trees and installing noise barriers across overpass structures between April 1<sup>st</sup> and August 15<sup>th</sup> to protect nesting migratory birds. In the event that tree removal or noise barrier installation across overpass structures must occur within the above-noted window, the Contractor should retain a Qualified Avian Biologist to conduct a nesting survey prior to the proposed works.

## **4.2.2 FISHERIES AND AQUATIC ECOSYSTEMS**

There are no watercourses within the project limits or the immediate surrounding area. Therefore, fish and fish habitat will not be impacted by the proposed works.

## **4.2.3 DESIGNATED NATURAL AREAS**

The Preliminary Design and EA Study identified that the study area was composed of degraded and significantly altered vegetation and wildlife habitats. During Detail Design, there were no designated natural heritage features or areas, including Provincially Significant Wetlands (PSWs), Areas of Natural and Scientific Interest (ANSIs), or Environmentally Sensitive Areas (ESAs), identified in the vicinity of the proposed noise barrier retrofits.

## **4.2.4 HYDROLOGY / SURFACE WATER**

### Potential Impacts

There is a negligible overall increase in impervious area due to construction of this project in the context of the total watershed areas.

### Mitigation

The main sewer system on Highway 417 will not be affected as part of the proposed works. No additional mitigation for stormwater management is required.

## **4.2.5 EROSION AND SEDIMENT CONTROL**

### Potential Impacts

Uncontrolled erosion and sedimentation occurring during construction can result in a loss of topsoil, disruption of drainage systems, and degradation of downstream water quality.

### Mitigation

Standard erosion and sediment control measures will be implemented during construction to prevent the migration of soils from the site.

## **4.2.6 WASTE AND CONTAMINATION / MANAGEMENT OF EXCESS MATERIALS**

### Potential Impacts

If not managed properly, excess waste and emissions have the potential to contaminate the surrounding environment.

### Mitigation

The following mitigation measures will be included in the contract to manage excess waste and emissions:

- Excess materials generated during construction will be managed in accordance with OPSS 180 (General Specification for the Management of Excess Material);
- All activities, including equipment maintenance, refueling and concrete truck washing should be controlled to prevent entry of petroleum products (e.g. gasoline, oils, lubricants), primers, grout, bonding adhesives or other hazardous or deleterious substances including any debris, waste, rubble or concrete materials in all drainage systems within the project limits unless otherwise specified in the contract. Substances are to be stored and mixed on protected surfaces away from the drainage systems within the project limits in order to prevent contamination of soils and waters. Any such material which inadvertently enters the drainage system within the project limits should be removed by the Contractor, at his/her own expense, in a manner satisfactory to the Contract Administrator;

- For mobile equipment and vehicles, maintenance, refueling and truck washing should be conducted no closer than 30 metres from the drainage system within the project limits in order to prevent water contamination due to accidental spills;
- All large equipment working in or near the drainage systems within the project limits should be well maintained to avoid contaminant leakage, should be free of excess surface oil or grease and should be equipped with spill kits deemed acceptable by the Contract Administrator;
- Where the Contractor manages excess earth as disposable fill, the Contractor should take into account the possibility of salt impacts and ensure that the material is managed responsibly and in an environmentally appropriate manner;
- Where the Contractor intends to manage the excess earth that may be salt impacted on private property, the Contractor should make the Property Owner aware that it may be salt impacted by using the Property Owner's Release. The Contractor is responsible for conducting such sampling and testing as may be necessary to comply with any requirements imposed by the Property Owner as a condition of accepting the excess earth; and
- The Contractor should develop an Environmental Plan detailing all environmental protection measures to be undertaken as part of the work, including an Erosion and Sedimentation Control Plan and Spill Prevention and Response Contingency Plan.

### 4.3 SOCIAL / ECONOMIC ENVIRONMENT

During detail design, a review of existing planning documents, current land uses, and the social environment (i.e. neighbourhood structures within the project limits) was completed to identify any potential impacts associated with the Highway 417 Noise Barrier Retrofits project.

The following sections describe potential impacts to the socio-economic environment associated with the proposed works, and provide a summary of mitigation to be included in the contract documents.

#### 4.3.1 GREENWAYS AND OPEN SPACE LINKAGES

The Rideau Canal represents an open space linkage adjacent to the Highway 417 corridor and is located at the eastern end of the project limits. There is also a multi-use pathway on the east side of the O-train corridor under the Highway 417 O-Train overpass.

##### Potential Impacts

As the Rideau Canal is located east of the limit of the proposed noise barrier retrofits, no impacts to greenways or open space linkages are anticipated and mitigation is not required. As the new noise barriers will be installed from the highway, no impacts are anticipated to the multi-use pathway on the east side of the O-Train corridor.

#### 4.3.2 LAND USE

Existing land uses in the project limits were determined through a review of background documents, including the City of Ottawa geoOttawa online resource, the City of Ottawa Official Plan (City of Ottawa, 2009), as well as aerial photography.

The project is located within an urban setting. As per for the City of Ottawa Official Plan, the lands immediately surrounding the proposed noise barrier retrofits along Highway 417 are designated as

General Urban Area and are characterized by mixed use development including commercial, employment, and residential land uses. The lands between the O-Train corridor and Rochester Street are designated as Mixed Use Centre. The Rideau Canal is located east of the project limits and is surrounded by lands designated as Major Open Space in the City of Ottawa Official Plan.

Specific land uses on the north side of Highway 417 within the project limits include an Adult High School, various churches, paramedic posts east of Bronson Avenue and west of O'Connor Street, the inter-city bus terminal on Catherine Street at Kent Street, and the Ottawa Police headquarters on Elgin Street. Specific land uses on the south side of Highway 417 within the project limits include Glebe Memorial Park, Chamberlain Park and Central Park.

#### Potential Impacts

No impacts to current land uses are anticipated, therefore no mitigation is required.

### **4.3.3 AESTHETICS AND LANDSCAPE**

#### *Landscape Composition*

The topography of the area within the project limits is characterized by relatively flat terrain, with the exception of upward slopes towards the elevated highway.

The proposed noise barrier retrofits are located within the section of the Highway 417 corridor which is characterized by highly urban, mixed use development which includes commercial, employment and residential uses both north and south of the highway (TESR, MTO, 2007). The local street network is more or less comprised of a fine-grained, typical grid pattern of streets and blocks.

As noted in **Section 4.2.1.1**, vegetation in the vicinity of the proposed noise barrier retrofits along Highway 417 is limited and other vegetation in close proximity to the highway occurs on private commercial and residential properties and in City parks. There are small amounts of landscaped / cultural vegetation within the vicinity of the proposed noise barrier retrofit locations, and discontinuous grass, landscape trees and plantings predominate along the Highway 417 right-of-way.

#### *Context Sensitive Design*

The report entitled Context Sensitive Design Concepts for the Queensway – Highway 417 (MTO & Stantec, 2011) was reviewed to determine where design recommendations for the Ottawa Queensway Corridor should be incorporated as part of the Highway 417 Noise Barrier Retrofits project.

#### Potential Impacts

The installation of noise barriers may result in potential impacts to the visual character of the Highway 417 corridor.

#### Mitigation

In accordance with the Queensway Context Sensitive Design recommendations and as described in **Section 3.1.1**, the design treatments for the proposed noise barrier retrofits will include the following:

- Use of concrete composite noise barrier panels in keeping with the same hues and tones constructed for the new noise barriers at Lees Avenue as part of the Highway 417 Expansion from Nicholas Street to OR 174;
- Use of clear acrylic panels across Highway 417 overpass structures within the project limits to provide a visual connection to the neighbourhood for users of the Queensway; and
- Use of translucent acrylic panels where noise barriers are required to be installed on top of retaining walls.

#### 4.3.4 TRAFFIC OPERATIONS

##### Potential Impacts

While the construction staging presented in **Section 3.2** was developed with the goal of minimizing lane and ramp closures to the extent possible, some closures are required to accommodate the proposed construction activities.

##### Mitigation

**Table 4-3** provides a summary of the required traffic impacts including lane and ramp closures on Highway 417 and lane closures on City streets, timing and approximate duration, and mitigation proposed to reduce impacts.

**Table 4-3: Summary of Lane/Ramp Closures**

Noise Wall	Location	Impacts	Proposed Mitigation	Approx. Duration
1	• Eastbound Rochester off-ramp	• Nightly recurring closure from 8 pm to 6 am	• Traffic will be directed to use Carling eastbound off-ramp as an alternate exit	• Nightly for 4 to 6 weeks
2, 3A	• Westbound Lyon on-ramp	• Full-time closure (24 hour basis)	• Traffic will be directed to use the Bronson westbound on-ramp via Catherine	• 10 to 12 weeks
2, 3A / 3B	• Westbound Bronson off-ramp • Westbound Lyon on-ramp	• Full-time closure (24 basis)	• Westbound Bronson off-ramp traffic will be directed to use the Metcalfe westbound off-ramp and Catherine westbound. A signed detour route with Airport signage will be in place and is illustrated in <b>Exhibit 4-3</b> , showing alternate routes during construction. • Westbound Lyon on-	• 6 to 8 weeks

Noise Wall	Location	Impacts	Proposed Mitigation	Approx. Duration
			ramp traffic will be directed to use the Bronson westbound on-ramp via Catherine	
3C	• Westbound O'Connor on-ramp	• Nightly recurring from 8 pm to 6 am	• Traffic will be directed to use the Lyon westbound on-ramp	• Nightly for 3 to 4 weeks
4	• Eastbound Kent off-ramp	• Full night-time closure from 8 pm to 6 am (only for erection of overhead panels on structure)	• Traffic will be directed to use the Bronson eastbound off-ramp or Metcalfe eastbound off-ramp	• 1 week
4/5	• Eastbound Metcalfe off-ramp	• Nightly recurring from 9 pm to 6 am	• Traffic will be directed to use the Bronson eastbound off-ramp and Kent Street eastbound off-ramp	• Nightly for 8 to 12 weeks
6	• Eastbound Metcalfe on-ramp	• Closure of 1 to 2 lanes (24 hour basis)	• City of Ottawa police will provide traffic control	• 10 to 12 weeks

### Mitigation

In addition to the measures outlined in **Table 4-3**, the following mitigation for traffic impacts will also be included in the Contract documents:

#### *Highway 417*

The Contractor will be permitted to work on the Noise Wall locations along the eastbound and westbound lanes of Highway 417 simultaneously in order to minimize impacts to traffic to the extent possible.

Alternate routes for traffic will be identified through advance notification signage on Highway 417 during ramp closures.

Work at Noise Walls 1, 3C, 4 and 5 will be completed at night in accordance with the timing constraints identified in the Contract documents.

Work at Noise Walls 3A, 3B and 6 will be completed during the day in accordance with the timing constraints identified in the Contract documents, except at locations along the walls where overhead work above a crossing street is required, in which case that portion of the overhead work will be performed at night in accordance with timing restrictions identified in the Contract documents.

The closure of the Highway 417 Metcalfe Street westbound off-ramp will not be permitted to occur at the same time as the full closure of the Highway 417 Bronson Avenue westbound off-ramp.

The Contractor will only be allowed a single lane closure on the Highway 417 Metcalfe Street eastbound for the erection and removal of the concrete barriers to create the work zone; full closure of the ramp will not be permitted.



Where the full closure of the Highway 417 Bronson Avenue westbound off-ramp is required for the installation of Noise Walls 3A and 3B, the Contractor will be allowed to close the ramp for a period of 40 calendar days.

### *City Streets*

City of Ottawa police presence will be provided at the following intersections during construction during the PM peak hours (3:00 pm to 6:00 pm):

- Noise Wall 2/3A/3B – Closure of Bronson Avenue and Lyon Street westbound on-ramps: Bronson Avenue and Raymond Street/Catherine Street;
- Noise Wall 2/3A/3B – Closure of Bronson Avenue and Lyon Street westbound on-ramps: Percy Street and Catherine Street; and
- Noise Wall 6: Highway 417 eastbound on-ramp at Metcalfe Street.

A Traffic and Incident Management Plan (TIMP) was prepared as part of the detail design project and was presented to the City of Ottawa Traffic Incident Management Group.

The Contractor will be required to develop a Traffic Control Management Plan and Incident Management Plan, which include actions to mitigate impacts to traffic during construction, as specified in the Contract documents.

### *Emergency Services*

The Contractor will be required to notify emergency service providers of construction at least two weeks in advance, and if any changes to traffic flows are anticipated. Staff representatives from fire and ambulance services should be included in the Contractor's Incident Management Team.

As noted above, the Contractor will be required to develop a Traffic Control Management Plan, which should include the actions detailed below to mitigate impacts to traffic during construction:

- Preparation of a Communication Plan including:
  - Bilingual media releases (Ottawa Citizen and Le Droit newspapers, radio and television notices) and residential/business mail-outs with a description of the work, date of commencement of the work on site, general sequence of construction and lane closures, and a description of night work, including locations; and
  - Motorist information, which will be delivered through bilingual portable variable message signs, temporary advance information signs (TC 64's), and detour signs along Highway 417 and local City streets.
- For ramps closures and staging on Highway 417:
  - Preparation of a Traffic Staging Plan, including the activities and sequencing, associated with each of the ramp closures;
  - The use of portable variable message signs; and
  - Coordination with the City of Ottawa.
- For the construction site:
  - Preparation of a Traffic Control Management Plan for the construction staging confirming staging traffic control layout, pedestrian and cyclist access, and dust control.

In addition, the Contractor will be required to develop an Incident Management Plan which includes scheduling and coordinating with an Incident Management Team (IMT) including staff from City of Ottawa

Traffic and Operations, OC Transpo, Ontario Provincial Police, City of Ottawa Police and Emergency Services.

Access by buses and pedestrians to all existing bus stops will be maintained at all times, except where short-term impacts to bus stops are required for night work related to the construction of noise barriers across municipal roads. The Contractor will provide OC Transpo at least 5 working days' notice to coordinate adjustments required to their facilities as a result of construction. Should the Contractor require access to or impede access/egress of a Transitway, application for access to the Transitway is required 72 hours before access is required.

#### *External Agencies*

The Contractor should be advised that the Ottawa Redblacks and Ottawa Senators have an interest in the operation of Highway 417, including ramps within the corridor. On game days, the Contractor should not be allowed to put in any lane closures or ramp closures from 2 hours prior to and until 2 hours after the completion of the games.

### **4.3.5 PEDESTRIAN ACCESS**

#### Potential Impacts

**Table 4-4** provides a summary of pedestrian impacts related to temporary sidewalk closures required during construction, timing and approximate duration, and mitigation proposed to reduce impacts.

**Table 4-4: Summary of Pedestrian Impacts**

Noise Wall	Location	Impacts	Proposed Mitigation	Approx. Duration
2	• Both sides of Bronson Avenue under Highway 417	• Alternating sidewalk closures during the removal of the existing barrier and all overhead work	• One of the sidewalks under the structure will remain open at all times	• Nightly for 1 to 3 weeks
3B	• Both sides of Percy Street under Highway 417	• Alternating sidewalk closures during the removal of the existing barrier and all overhead work	• One of the sidewalks under the structure will remain open at all times	• Nightly for 1 to 3 weeks
4	• East side of Kent Street under Highway 417	• East side sidewalk closure during the installation of noise barrier panels over the West Chamberlain to Kent Street North lane	• Pedestrian traffic will be directed to Bank Street along the sidewalk on the south side of Chamberlain	• Nightly for 1 week
4	• Both sides of Bank Street under Highway 417	• Alternating sidewalk closures during the removal of the existing barrier and all	• One of the sidewalks under the structure will remain open at all times	• Nightly for 1 to 3 weeks

Noise Wall	Location	Impacts	Proposed Mitigation	Approx. Duration
		overhead work		
5	<ul style="list-style-type: none"> <li>Both sides of O'Connor Street under Highway 417</li> </ul>	<ul style="list-style-type: none"> <li>Alternating sidewalk closures during the removal of the existing barrier and all overhead work</li> </ul>	<ul style="list-style-type: none"> <li>One of the sidewalks under the structure will remain open at all times</li> </ul>	<ul style="list-style-type: none"> <li>Nightly for 1 to 3 weeks</li> </ul>
5	<ul style="list-style-type: none"> <li>Both sides of Metcalfe Street under Highway 417</li> </ul>	<ul style="list-style-type: none"> <li>West sidewalk closure during the removal of the existing barrier and all overhead work</li> </ul>	<ul style="list-style-type: none"> <li>Pedestrian traffic will be directed to O'Connor Street via the sidewalks on the south side of Catherine Street or Isabella Street</li> </ul>	<ul style="list-style-type: none"> <li>Nightly for 1 to 3 weeks</li> </ul>
6	<ul style="list-style-type: none"> <li>Both sides of Elgin Street under Highway 417</li> </ul>	<ul style="list-style-type: none"> <li>Alternating sidewalk closures during the removal of the existing barrier and all overhead work</li> </ul>	<ul style="list-style-type: none"> <li>One of the sidewalks under the structure will remain open at all times</li> <li>In the case of the closure of the west sidewalk, pedestrian traffic will be directed south along the extension of Elgin Street to Pretoria/Queen Elizabeth in order to cross to the east sidewalk under the structure</li> </ul>	<ul style="list-style-type: none"> <li>Nightly for 1 to 3 weeks</li> </ul>

### Mitigation

Alternate routes have been identified for pedestrians during the construction of Noise Walls 2, 3B, 4, 5 and 6 and associated nightly sidewalk closures, and are illustrated in **Exhibit 4-3** and **Exhibit 4-4**.

The Contractor will be responsible for vehicular and pedestrian traffic control on or adjacent to City streets and will be required to develop a Construction Site Pedestrian Control Plan (CSPCP) which will ensure the provision of a safe and accessible path of travel for all pedestrians, including those with disabilities and/or increased mobility needs. The Plan will address: temporary pedestrian signing; directional signing; maintenance of sidewalk; relocation, maintenance, and removal of pedestrian barriers; and all necessary delineation to provide a safe environment for pedestrians.

Regardless of whether or not there is an existing sidewalk within the right-of-way, a minimum 1.5 m wide pedestrian facility should be provided along at least one side of the corridor at all times and should include a free and unobstructed hard surfaced pedestrian surface acceptable for use by all pedestrians, including those with disabilities, with access to all buildings and street crossings. The facility should be maintained clean and in a good state of repair, through or around the construction site at all times. The

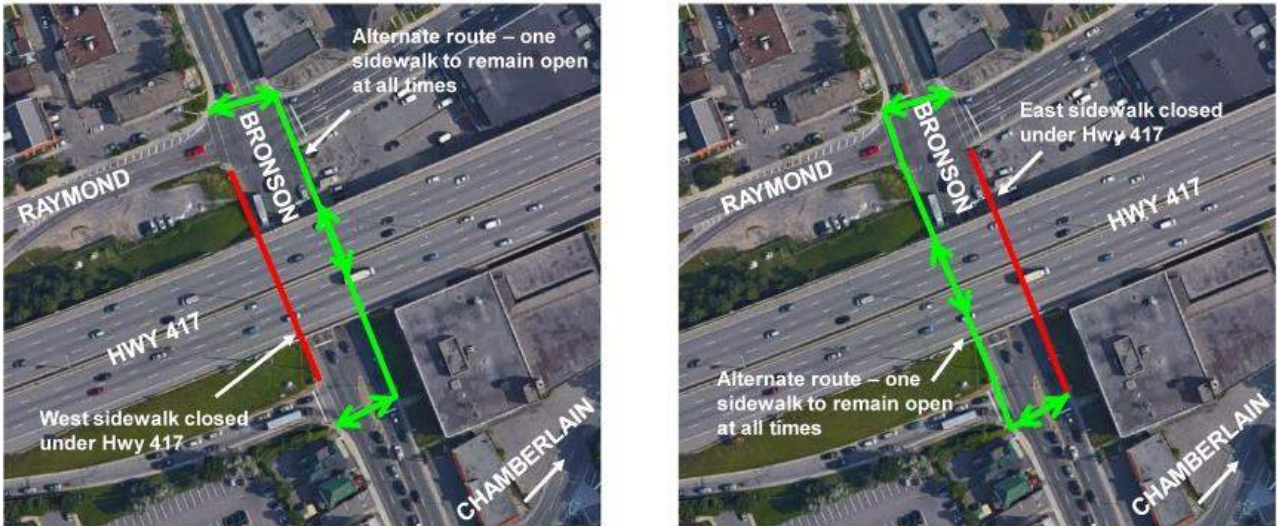
1.5 m width should not be reduced by protruding objects. If overhead works are required, 2.1 m clear headroom should be provided along the entire 1.5 m width.

Temporary barriers or fencing should be installed to separate pedestrian traffic from construction operations or hazards. Where pedestrians must be detoured, the Contractor should install bilingual signage at the intersection preceding the detour.



Exhibit 4-3: Highway 417 Noise Barrier Retrofits – Alternate Routes – Noise Walls 2 to 4

NOISE WALL 2 – BRONSON AVENUE ALTERNATE SIDEWALK CLOSURE



NOISE WALL 3B – PERCY STREET ALTERNATE SIDEWALK CLOSURE



NOISE WALLS 3A / 3B - SIGNED DETOUR FOR WESTBOUND BRONSON OFF-RAMP CLOSURE



NOISE WALL 4 – KENT STREET EAST SIDE SIDEWALK CLOSURE





Exhibit 4-4: Highway 417 Noise Barrier Retrofits – Alternate Routes – Noise Walls 4 to 6

NOISE WALL 4 – BANK STREET ALTERNATE SIDEWALK CLOSURES



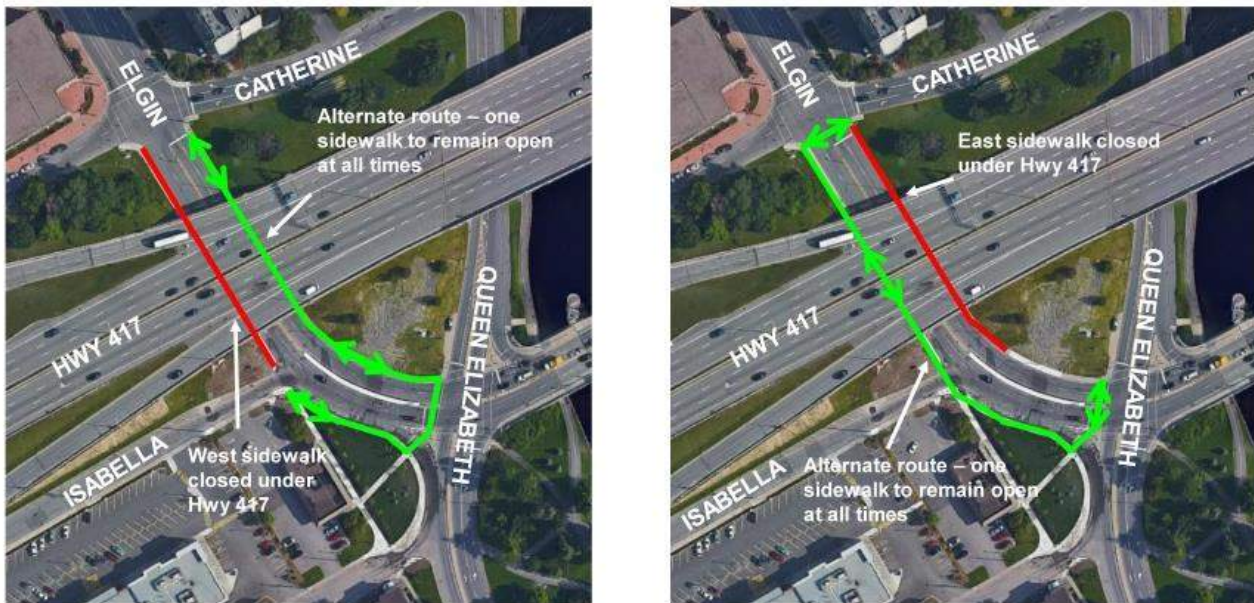
NOISE WALL 5 – O’CONNOR STREET ALTERNATE SIDEWALK CLOSURES



NOISE WALL 5 – METCALFE STREET WEST SIDE SIDEWALK CLOSURE



NOISE WALL 6 – ELGIN STREET ALTERNATE SIDEWALK CLOSURES





### 4.3.6 PROPERTY REQUIREMENTS

#### Potential Impacts

Temporary Limited Interests (TLIs) are required for the storage of construction equipment and materials in the following locations:

- On the north side of the Highway 417 Bronson Avenue westbound off-ramp, adjacent to the existing retaining wall east and west of Percy Street (Noise Wall 3B);
- On the north side of Isabella Street east of Bank Street adjacent to the existing retaining wall (Noise Wall 4); and
- On the south side of the Metcalfe Street eastbound on-ramp to the curb on Isabella Street (Noise Wall 6).

Permission to Enter is required for construction access in the following locations:

- Through the parking lot on Catherine Street east of the City of Ottawa property and west of the Canadian Red Cross building (Noise Wall 3A); and
- Through the City of Ottawa gate on the east side of Percy Street for access to Noise Wall 3B.

#### Mitigation

Temporary Limited Interest and Permission to Enter agreements will be arranged with affected property owners prior to construction taking place.

Prior to the use of any access points required across private property or Temporary Limited Interests, the Contractor will be required to complete an onsite survey to document the condition of the private property both for the entire access route, the Temporary Limited Interest area, as well as the MTO property adjacent to the retaining walls, in conjunction with the affected property owner.

The Contractor will be responsible for all repairs required for any damages as a result of his use of the access routes, limited interests or MTO property.

### 4.3.7 AIR QUALITY

During preliminary design, it was identified that the contribution of the Highway 417 Expansion Project is projected to be below applicable government guidelines for air quality and would have an unnoticeable and insignificant impact on smog pollutants. Therefore, mitigation is not proposed.

### 4.3.8 NOISE

A Noise Assessment (Novus Environmental, 2015) was conducted during detail design to determine existing noise levels affecting Noise Sensitive Areas (i.e. residences with Outdoor Living Areas) located adjacent to Highway 417 within the project limits, and to determine predicted noise levels after the installation of new 5 m noise barriers and accounting for projected future traffic to the year 2021. Existing noise levels were measured in three areas within the project limits, based on the locations of the proposed noise barrier retrofits. The locations of noise sensitive receptors at which noise levels were

measured in the three areas are illustrated in **Exhibit 4-5 to Exhibit 4-7**. Existing noise levels affecting Noise Sensitive Areas (NSAs) located adjacent to the highway are as follows:

- Area 1 – Between 50 and 66 dBA on the south side of Highway 417 between the O-Train corridor and Preston Street;
- Area 2 – Between 50 and 66 dBA on the north side of Highway 417 between Bronson Avenue and Lyon Street; and
- Area 3 – Between 50 and 71 dBA on the south side of Highway 417 between Lyon Street and the Rideau Canal.

Noise levels affecting Noise Sensitive Areas in all three areas exceed the provincial noise level objective of 55 dBA.

Predicted reductions in noise levels for the first rows of residential receivers (i.e. residences located closest to the highway) in the three areas following the installation of new 5 m noise barriers are summarized in **Table 4-5**. The following noise sensitive receptors represent the first row of residential receivers in the three areas, as illustrated in **Exhibit 4-5 to Exhibit 4-7**:

- Area 1 – R1\_01 through R1\_05;
- Area 2 – R2\_01 through R2\_10; R3\_01 through R3\_03; and R3\_08 through R3\_11; and
- Area 3 – R4\_01 through R4\_07; and R5\_01 through R5\_04.

**Table 4-5: Existing and Predicted Noise Levels for the First Row of Residential Receivers**

Noise Wall Location	Noise Sensitive Receptors	Noise Levels for First Row of Residential Receivers		
		Existing	With New 5 m Noise Barrier	Overall Average Reduction*
Area 1: South side of Highway 417 between the O-Train and Preston Street	<ul style="list-style-type: none"> <li>• Noise sensitive receptors representing the first row of residential receivers: <b>5</b></li> <li>• Number of residences in the first row of residential receivers: <b>14</b></li> </ul>	55 – 66 dBA	51 – 63 dBA	<b>7 dBA</b>
Area 2: North side of Highway 417 between Bronson Avenue and Lyon Street	<ul style="list-style-type: none"> <li>• Noise sensitive receptors representing the first row of residential receivers: <b>17</b></li> <li>• Number of residences in the first row of residential receivers: <b>83</b></li> </ul>	50 – 66 dBA	50 – 56 dBA	<b>5 dBA</b>



Noise Wall Location	Noise Sensitive Receptors	Noise Levels for First Row of Residential Receivers		
		Existing	With New 5 m Noise Barrier	Overall Average Reduction*
Area 3: South side of Highway 417 between Lyon Street and the Rideau Canal	<ul style="list-style-type: none"> <li>Noise sensitive receptors representing the first row of residential receivers: <b>11</b></li> <li>Number of residences in the first row of residential receivers: <b>57</b></li> </ul>	50 – 71 dBA	50 – 60 dBA	<b>6 dBA</b>

\*The overall average reduction for the first row of residential receivers includes weighting of the number of Noise Sensitive Areas.

The Noise Assessment concluded that overall average reductions in noise levels following the installation of the new 5 m noise barriers will be between 5 and 7 dBA for the three areas.

### Potential Impacts

Night-time work will be required for the installation of the proposed noise barrier retrofits throughout the project limits, in order to minimize impacts to traffic on Highway 417 as a result of long-term lane and ramp closures that would be required for all proposed works to be completed during the day. It is also anticipated that there will be an increase in noise levels during construction.

Night-time construction activities may include sawcutting, removal of concrete barriers, construction of new traffic and noise barrier walls, and other construction activities.

### Mitigation

The area within the project limits is governed by the City of Ottawa Noise By-Law (By-Law no. 2004-253), and therefore, an exemption from subsection 7(1) of the Noise By-law is required for any night-time construction activity. The Contractor will abide by all municipal control by-laws for the day-to-day construction. However, night-time work will be required in order to minimize traffic impacts and the duration of construction, including required lane and ramp closures on Highway 417. A noise by-law exemption will be obtained prior to construction for the required night work.

Equipment will be required to be maintained in an operating condition that prevents unnecessary noise, including but not limited to non-defective muffler systems, properly secured components, and the lubrication of moving parts. Idling of equipment should be restricted to the minimum necessary to perform the specified work.

Exhibit 4-5: Modelled noise sensitive receptors and proposed noise barrier retrofits – Area 1



Figure No. 3

### Area 1 - Modelled Noise Sensitive Receptors and Recommended Retrofit Noise Barriers

Highway 417 Barrier Retrofit  
Ottawa, Ontario



True  
North

Scale: 1 : 4,000

Date: 14 / 05 / 01

File No.: 13-0279

Drawn By: KAC

**novus**  
ENVIRONMENTAL



Exhibit 4-6: Modelled noise sensitive receptors and proposed noise barrier retrofits – Area 2





Exhibit 4-7: Modelled noise sensitive receptors and proposed noise barrier retrofits – Area 3



### 4.3.9 UTILITIES

#### Potential Impacts

There are no major utility conflicts or relocations required as part of this project.

Existing underground and overhead hydro infrastructure will be maintained during construction.

ATMS ducts that are underground, embedded on structures or surface-mounted will be relocated to accommodate the new noise barriers.

A City of Ottawa 1220 mm watermain is located south of Highway 417 within the project limits, on the north side of Isabella Avenue between Bank Street and O'Connor Street, and crossing the Highway 417 Rochester eastbound off-ramp just south of the existing barrier wall running along the edge of the ramp between Rochester Street and Preston Street. The proposed works have the potential to cause vibrations which may impact the watermain during construction.

#### Mitigation

The affected ATMS ducts will be relocated to accommodate the new noise barriers, as specified in the Contract drawings.

During construction, the Contractor will be required to protect and monitor the existing 1220 mm watermain as specified in the Contract documents.

## 4.4 CULTURAL ENVIRONMENT

### 4.4.1 ARCHAEOLOGY

#### Potential Impacts

No evidence of archaeological resources was identified within the existing Highway 417 right-of-way during the Stage I and Stage II Archaeological Assessments undertaken during preliminary design. As the existing right-of-way has been significantly disturbed, there are no present archaeological concerns.

#### Mitigation

In the event that deeply buried archaeological deposits are discovered in the course of construction, the Ministry of Tourism, Culture and Sport (416-314-1177) should be notified immediately. Should previously undocumented archaeological resources be discovered, they may be new archaeological sites and therefore subject to Section 48 (1) of the *Ontario Heritage Act*. The proponent or person discovering the archaeological resources must cease alteration of the site immediately and engage a licensed consultant archaeologist to carry out a determination of their nature and significance.

In the event that human remains are encountered during construction, the Cemeteries Regulation Unit of the Ministry of Consumer Service (1-800-889-9768) should be notified. In situations where human remains are associated with archaeological resources, the Ministry of Tourism, Culture and Sport should

also be contacted to ensure that the site is not subject to unlicensed alterations which would be a contravention of the *Ontario Heritage Act*.

## 4.5 SUMMARY OF ENVIRONMENTAL EFFECTS, PROPOSED MITIGATION, COMMITMENTS TO FURTHER WORK

**Table 4-6** summarizes the identified key environmental concerns and proposed mitigating measures.

**Table 4-6: Summary of Environmental Concerns and Commitments**

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
NATURAL ENVIRONMENT				
1.0 Fisheries and Aquatic Ecosystems (Section 4.2.2)			TESR Reference: Section 6.2.1.1 and Table 6.5	
1.1	No watercourses were identified within the project limits or the immediate surrounding area.	MTO MNR MOE	1.1.1	None required.  N/A
2.0 Vegetation (Section 4.2.1.1)			TESR Reference: Section 6.2.1.2 and Table 6.5	
2.1	The installation of noise barriers will result in minor disturbances to vegetation.	MTO MNR MOE	2.1.1	Vegetation removal and grading will be minimized to only what is required for the proposed works. Required vegetation removal and protection measures will be conducted in accordance with OPSS 201 (tree clearing).  The use of appropriate vegetation clearing techniques will be employed and exposed surfaces will be re-stabilized and re-vegetated as soon as possible following construction.  Contract Drawings OPSS 201 – Clearing, Close Cut Clearing, Grubbing, & Removal of Surface & Piled Boulders
			2.1.2	All construction-related debris will be cleared / disposed of following construction.  OPSS 180 – The Management of Excess Materials
3.0 Wildlife and Species at Risk (Section 4.2.1.2)			TESR Reference: Section 6.2.1.2 and Table 6.5	
3.1	Potential impacts to wildlife during construction.	MTO MNR MOE	3.1.1	The Contractor will not harass or kill any wildlife encountered during construction.  Should any Species at risk be encountered at any time in the construction area, the Contract Administrator will be contacted immediately.  Operational Constraint (Environmental) – Prevention of Wildlife Harassment
3.2	Potential impacts to migratory birds during construction.	MTO MNR MOE	3.2.1	The Contractor will not destroy the active nests (nests with eggs or young birds), or wound or kill birds, of species protected under the <i>Migratory Birds Convention Act, 1994</i> and/or Regulations under that Act. If active nests are encountered, the Ministry's Contract Administrator should be contacted.

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
				<b>Operational Constraint (Environmental) – Migratory Bird Protection – General</b>
			3.2.2	The Contractor should refrain from clearing trees and installing noise barriers across overpass structures between April 1 <sup>st</sup> and August 15 <sup>th</sup> to protect nesting migratory birds. In the event that tree removal or noise barrier installation across overpass structures must occur within the above-noted window, the Contractor should retain a Qualified Avian Biologist to conduct a nesting survey prior to the proposed works.
				<b>Operational Constraint (Environmental) – Migratory Bird Protection – General</b>
<b>4.0 Designated Natural Areas (Section 4.2.3)</b>				<b>TESR Reference: Section 6.2.1.2 and Table 6.5</b>
4.1	No designated natural heritage features or areas were identified.	MTO MNR MOE	4.1.1	None required.  <b>N/A</b>
<b>5.0 Hydrology / Surface Water (Section 4.2.4)</b>				<b>TESR Reference: Section 6.2.1.3 and Table 6.5</b>
5.1	There is a negligible overall increase in impervious area due to construction of this project in the context of the total watershed areas.	MTO MNR MOE	5.1.1	None required.  <b>N/A</b>
<b>6.0 Erosion and Sediment Control (Section 4.2.5)</b>				<b>TESR Reference: None</b>
6.1	Uncontrolled erosion and sedimentation occurring during construction can result in a loss of topsoil and degradation of downstream water quality.	MTO MNR MOE	6.1.1	Standard erosion and sediment control measures should be implemented during construction to prevent the migration of soils from the site.  <b>Operational Constraint (Environmental) – Erosion and Sediment Control</b> <b>Operational Constraint (Environmental) – Environmental Plan</b> <b>Contract Drawings</b>
<b>7.0 Waste and Contamination / Management of Excess Materials (Section 4.2.6)</b>				<b>TESR Reference: Section 6.2.2.3 and Table 6.5</b>
7.1	If not managed properly, excess waste and emissions have the potential to contaminate the surrounding environment.	MTO MNR MOE	7.1.1	Excess materials generated during construction will be managed in accordance with OPSS 180 (General Specification for the Management of Excess Material).  <b>OPSS 180 - The Management of Excess Material</b>



I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
			7.1.2	<p>All activities, including equipment maintenance, refueling and concrete truck washing should be controlled to prevent entry of petroleum products (e.g. gasoline, oils, lubricants), primers, grout, bonding adhesives or other hazardous or deleterious substances including any debris, waste, rubble or concrete materials in all drainage systems within the project limits unless otherwise specified in the contract. Substances are to be stored and mixed on protected surfaces away from the drainage systems within the project limits in order to prevent contamination of soils and waters. Any such material which inadvertently enters the drainage system within the project limits should be removed by the Contractor, at his/her own expense, in a manner satisfactory to the Contract Administrator.</p> <p><b>Operational Constraint (Environmental) – Equipment Refueling, Maintenance and Washing</b></p>
			7.1.3	<p>For mobile equipment and vehicles, maintenance, refueling and truck washing should be conducted no closer than 30 metres from the drainage system within the project limits in order to prevent water contamination due to accidental spills.</p> <p><b>Operational Constraint (Environmental) – Equipment Refueling, Maintenance and Washing</b></p>
			7.1.4	<p>All large equipment working in or near the drainage systems within the project limits should be well maintained to avoid contaminant leakage, should be free of excess surface oil or grease and should be equipped with spill kits deemed acceptable by the Contract Administrator.</p> <p><b>Operational Constraint (Environmental) – Equipment Refueling, Maintenance and Washing</b></p>
			7.1.5	<p>Where the Contractor manages excess earth as disposable fill, the Contractor shall take into account the possibility of salt impacts and ensure that the material is managed responsibly and in an environmentally appropriate manner. Where the Contractor intends to manage the excess earth that may be salt impacted on private property, the Contractor should make the Property Owner aware that it may be salt impacted by using the Property Owner's Release. The Contractor is responsible for conducting such sampling and testing as may be necessary to comply with any requirements imposed by the Property Owner as a condition of accepting the excess earth.</p> <p><b>Operational Constraint (Environmental) – Management of Excess Earth with Salt Impacts</b></p>
			7.1.6	<p>The Contractor should develop an Environmental Plan detailing all environmental protection measures to be undertaken as part of the work, including an Erosion and Sedimentation Control</p>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
				Plan and Spill Prevention and Response Contingency Plan.  <b>Operational Constraint (Environmental) – Environmental Plan</b>
<b>SOCIAL-ECONOMIC ENVIRONMENT</b>				
<b>8.0 Greenways and Open Space Linkages (Section 4.3.1)</b>			<b>TESR Reference: Section 6.2.1.4</b>	
8.1	No impacts to greenways or open space linkages are anticipated.	MTO MNR MOE NCC	8.1.1	None required.  <b>N/A</b>
<b>9.0 Land Use (Section 4.3.2)</b>			<b>TESR Reference: Section 6.2.2.1 and Table 6.5</b>	
9.1	No impacts to current land uses are anticipated.	MTO City of Ottawa	9.1.1	None required.  <b>N/A</b>
<b>10.0 Aesthetics and Landscape (Section 4.3.3)</b>			<b>TESR Reference: Section 6.2.2.4 and Table 6.5</b>	
10.1	The installation of noise barriers may result in potential impacts to the visual character of the Highway 417 corridor.	MTO NCC City of Ottawa	10.1.1	In accordance with the Queensway Context Sensitive Design recommendations, the design treatments for the proposed noise barrier retrofits will include the following: <ul style="list-style-type: none"> <li>• Use of concrete composite noise barrier panels in keeping with the same hues and tones constructed for the new noise barriers at Lees Avenue as part of the Highway 417 Expansion from Nicholas Street to OR 174;</li> <li>• Use of clear acrylic panels across Highway 417 overpass structures within the project limits to provide a visual connection to the neighbourhood for users of the Queensway; and</li> <li>• Use of translucent acrylic panels where noise barriers are required to be installed on top of retaining walls.</li> </ul> <b>Contract Drawings</b>
<b>11.0 Traffic Operations (Section 4.3.4)</b>			<b>TESR Reference: Section 6.2.2.2</b>	
11.1	Lane and ramp closures associated with the installation of noise barriers are required on Highway 417 and will potentially result in traffic delays and queues.	MTO City of Ottawa	11.1.1	The Contractor will be permitted to work on the Noise Wall locations along the eastbound and westbound lanes of Highway 417 simultaneously in order to minimize impacts to traffic to the extent possible.  Alternate routes for traffic will be identified through advance notification signage on Highway 417 during ramp closures.  Work at Noise Walls 1, 3C, 4 and 5 will be completed at night in accordance with the timing constraints identified in the Contract documents.

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
				<p>Work at Noise Walls 3A, 3B and 6 will be completed during the day in accordance with the timing constraints identified in the Contract documents, except at locations along the walls where overhead work above a crossing street is required, in which case that portion of the overhead work will be performed at night in accordance with timing restrictions identified in the Contract documents.</p> <p>The closure of the Highway 417 Metcalfe Street westbound off-ramp will not be permitted to occur at the same time as the full closure of the Highway 417 Bronson Avenue westbound off-ramp.</p> <p>The Contractor will only be allowed a single lane closure on the Highway 417 Metcalfe Street eastbound for the erection and removal of the concrete barriers to create the work zone; full closure of the ramp will not be permitted.</p> <p>Where the full closure of the Highway 417 Bronson Avenue westbound off-ramp is required for the installation of Noise Walls 3A and 3B, the Contractor will be allowed to close the ramp for a period of 40 calendar days.</p> <p><b>Contract Drawings</b> <b>Operational Constraint - Traffic</b></p>
			11.1.2	<p>A Traffic and Incident Management Plan (TIMP) was prepared as part of the Detail Design project and was presented to the City of Ottawa Traffic Incident Management Group.</p> <p>The Contractor will be required to develop a Traffic Control Management Plan and Incident Management Plan, which include actions to mitigate impacts to traffic during construction, as specified in the Contract documents.</p> <p><b>SP: Control of Vehicular and Pedestrian Traffic – City Streets Only</b> <b>SP: Traffic Control Plan – City Streets</b> <b>SP: Traffic Control Management Plan</b> <b>Incident Management Plan – Item No.</b></p>
			11.1.3	<p>Applicable signage and advance notification will be provided for ramp closures and lane reductions.</p> <p>The Contractor will be required to develop a Traffic Control Management Plan, which should include the actions detailed below to mitigate impacts to traffic during construction:</p> <ul style="list-style-type: none"> <li>Preparation of a Communication Plan including: <ul style="list-style-type: none"> <li>Bilingual media releases (Ottawa Citizen and Le Droit newspapers, radio and</li> </ul> </li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
				<p>television notices) and residential/business mail-outs with a description of the work, date of commencement of the work on site, general sequence of construction and lane closures, and a description of night work, including locations; and</p> <ul style="list-style-type: none"> <li>Motorist information, which will be delivered through bilingual portable variable message signs, temporary advance information signs (TC 64's), and detour signs along Highway 417 and local City streets.</li> <li>For ramps closures and staging on Highway 417: <ul style="list-style-type: none"> <li>Preparation of a Traffic Staging Plan, including the activities and sequencing, associated with each of the ramp closures;</li> <li>The use of portable variable message signs; and</li> <li>Coordination with the City of Ottawa.</li> </ul> </li> <li>For the construction site: <ul style="list-style-type: none"> <li>Preparation of a Traffic Control Management Plan for the construction staging confirming staging traffic control layout, pedestrian and cyclist access, and dust control.</li> </ul> </li> </ul> <p><b>Contract Drawings</b>  <b>Traffic Control Signing – Item No.</b>  <b>SP: Traffic Control Plan – City Streets</b>  <b>SP: Traffic Control Management Plan</b>  <b>Incident Management Plan – Item No.</b></p>
11.2	Potential impacts to the operation of Emergency Services and OC Transpo resulting from Highway 417 lane reductions and ramp closures, as well as lane reductions on City streets.	MTO City of Ottawa (including Emergency Services and OC Transpo)	11.2.1	<p>The Contractor will be required to notify emergency service providers of construction at least two weeks in advance, and if any changes to traffic flows are anticipated.</p> <p><b>Operational Constraint – Emergency Service Providers</b></p>
			11.2.2	<p>City of Ottawa police presence will be provided at the following intersections during construction during the PM peak hours (3:00 pm to 6:00 pm):</p> <ul style="list-style-type: none"> <li>Noise Wall 2/3A/3B – Closure of Bronson Avenue and Lyon Street westbound on-ramps: Bronson Avenue and Raymond Street/Catherine Street;</li> <li>Noise Wall 2/3A/3B – Closure of Bronson Avenue and Lyon Street westbound on-ramps: Percy Street and Catherine Street; and</li> <li>Noise Wall 6: Highway 417 eastbound on-ramp at Metcalfe Street.</li> </ul> <p><b>SP: Control of Vehicular and Pedestrian Traffic – City Streets</b></p>
			11.2.3	<p>Staff representatives from fire and ambulance services will be included in the Incident Management Team.</p> <p><b>Incident Management Plan – Item No.</b></p>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
			11.2.4	<p>Access by buses and pedestrians to all existing bus stops will be maintained at all times, except where short-term impacts to bus stops are required for night work related to the construction of noise barriers across municipal roads.</p> <p>The Contractor will provide OC Transpo at least 5 working days' notice to coordinate adjustments required to their facilities as a result of construction. Should the Contractor require access to or impede access/egress of a Transitway, application for access to the Transitway is required 72 hours before access is required.</p> <p><b>SP: Control of Vehicular and Pedestrian Traffic – City Streets Only</b></p>
11.3	Highway 417 lane reductions and ramp closures have the potential to impact Ottawa Redblacks and Ottawa Senators event traffic.	MTO City of Ottawa Ottawa Redblacks Ottawa Senators	11.3.1	<p>The Contractor should be advised that the Ottawa Redblacks and Ottawa Senators have an interest in the operation of Highway 417, including ramps within the corridor. On game days, the Contractor should not be allowed to put in any lane closures or ramp closures from 2 hours prior to and until 2 hours after the completion of the games.</p> <p><b>Operational Constraint – Coordination with External Agencies</b></p>
<b>12.0 Pedestrian and Cyclist Access (Section 4.3.5)</b>				<b>TESR Reference: Section 6.2.2.1 and Table 6.5</b>
12.1	<p>Impacts to pedestrian access to City streets are expected on the following sidewalks during night-time construction of overhead work:</p> <ul style="list-style-type: none"> <li>Both sides of Bronson Avenue under Highway 417 (Noise Wall 2);</li> <li>Both sides of Percy Street under Highway 417 (Noise Wall 3B);</li> <li>East side of Kent Street under Highway 417 (Noise Wall 4);</li> <li>Both sides of Bank Street under Highway 417 (Noise Wall 4);</li> <li>Both sides of O'Connor Street under Highway 417</li> </ul>	MTO City of Ottawa	12.1.1	<p>The Contractor will be responsible for vehicular and pedestrian traffic control on or adjacent to City streets and will be required to develop a Construction Site Pedestrian Control Plan (CSPCP) which will ensure the provision of a safe and accessible path of travel for all pedestrians, including those with disabilities and/or increased mobility needs. The Plan will address: temporary pedestrian signing; directional signing; maintenance of sidewalk; relocation, maintenance, and removal of pedestrian barriers; and all necessary delineation to provide a safe environment for pedestrians.</p> <p><b>Traffic Control Signing – Item No.</b>  <b>SP: Control of Vehicular and Pedestrian Traffic – City Streets Only</b>  <b>SP: Construction Site Pedestrian Control Plan</b></p>
			12.1.2	<p>During night-time sidewalk closures under Highway 417 on Bronson Avenue, Percy Street, Bank Street, O'Connor Street, and Elgin Street, at least one of the sidewalks will be maintained under the structures at all times. In the case of the closure of the west sidewalk on Elgin Street under Highway 417, pedestrian traffic will be directed south along the extension of Elgin Street to Pretoria Avenue/Queen Elizabeth Drive in order to cross to the east sidewalk under the structure.</p> <p>During the night-time closure of the sidewalk on the east side of Kent Street under Highway 417, pedestrian traffic will be directed to Bank Street along the sidewalk on the south side of</p>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
	(Noise Wall 5); <ul style="list-style-type: none"> <li>Both sides of Metcalfe Street under Highway 417 (Noise Wall 5); and</li> <li>Both sides of Elgin Street under Highway 417 (Noise 6).</li> </ul>			Chamberlain Avenue.  During the night-time closure of the sidewalk on the west side of Metcalfe Street under Highway 417, pedestrian traffic will be directed to O'Connor Street via the sidewalks on the south side of Catherine Street or Isabella Street.  Regardless of whether or not there is an existing sidewalk within the right-of-way, a minimum 1.5 m wide pedestrian facility should be provided along at least one side of the corridor at all times and should include a free and unobstructed hard surfaced pedestrian surface acceptable for use by all pedestrians, including those with disabilities, with access to all buildings and street crossings. The facility should be maintained clean and in a good state of repair, through or around the construction site at all times. The 1.5 m width should not be reduced by protruding objects. If overhead works are required, 2.1 m clear headroom should be provided along the entire 1.5 m width.  Temporary barriers or fencing should be installed to separate pedestrian traffic from construction operations or hazards. Where pedestrians must be detoured, the Contractor should install bilingual signage at the intersection preceding the detour.  <b>Contract Drawings</b> <b>Traffic Control Signing – Item No.</b> <b>SP: Construction Site Pedestrian Control Plan</b>
<b>13.0 Property Requirements (Section 4.3.6)</b>				<b>TESR Reference: Section 6.2.2.1</b>
13.1	Temporary Limited Interests are required for the storage of construction equipment and materials in the following locations: <ul style="list-style-type: none"> <li>On the north side of the Highway 417 Bronson Avenue westbound off-ramp, adjacent to the existing retaining wall east and west of Percy Street (Noise Wall 3B);</li> <li>On the north side of Isabella Street east of</li> </ul>	MTO Property Owners	13.1.1	Temporary Limited Interest and Permission to Enter agreements should be arranged with affected property owners prior to construction taking place.  <b>Operational Constraint – Construction Access</b>
			13.1.2	Prior to the use of any access points required across private property or Temporary Limited Interests, the Contractor will be required to complete an onsite survey to document the condition of the private property both for the entire access route, the Temporary Limited Interest area, as well as the MTO property adjacent to the retaining walls, in conjunction with the affected property owner.  <b>Operational Constraint – Construction Access</b>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
	<p>Bank Street adjacent to the existing retaining wall (Noise Wall 4); and</p> <ul style="list-style-type: none"> <li>On the south side of the Metcalfe Street eastbound on-ramp to the curb on Isabella Street (Noise Wall 6).</li> </ul> <p>Permission to Enter is required for construction access in the following locations:</p> <ul style="list-style-type: none"> <li>Through the parking lot on Catherine street east of the City of Ottawa property and west of the Canadian Red Cross building (Noise Wall 3A); and</li> <li>Through the City of Ottawa gate on the east side of Percy Street for access to Noise Wall 3B.</li> </ul>		13.1.3	<p>The Contractor will be responsible for all repairs required for any damages as a result of his use of the access routes, limited interests or MTO property.</p> <p><b>Operational Constraint – Construction Access</b></p>
<b>14.0 Air Quality (Section 4.3.7)</b>				<b>TESR Reference: Section 6.2.2.6 and Table 6.5</b>
14.1	The contribution of the overall Highway 417 Expansion Project is projected to be below applicable government guidelines for air quality and would have an unnoticeable and insignificant impact on smog pollutants. Therefore, mitigation is not proposed.	MTO MOE	14.1.1	<p>None required.</p> <p><b>N/A</b></p>
<b>15.0 Noise (Section 4.3.8)</b>				<b>TESR Reference: Section 6.2.2.5 and Table 6.5</b>
15.1	It is anticipated that there will be an increase in noise levels during construction. Night-time work will be required in order	MTO MOE City of Ottawa	15.1.1	<p>The Contractor will abide by all municipal noise control by-laws for the day-to-day construction. A noise by-law exemption will be obtained prior to construction for required night work.</p> <p><b>SP 199F31: Environmental Exemptions and Permits</b></p>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
	to minimize impacts to traffic related to lane/ramp closures on the highway.		15.1.2	Equipment will be required to be maintained in an operating condition that prevents unnecessary noise, including but not limited to non-defective muffler systems, properly secured components, and the lubrication of moving parts.  <b>SP 199F33: Construction Noise Constraints</b>
			15.1.3	Idling of equipment should be restricted to the minimum necessary to perform the specified work.  <b>SP 199F33: Construction Noise Constraints</b>
<b>16.0 Utilities (Section 4.3.9)</b>				<b>TESR Reference: None</b>
16.1	ATMS ducts that are underground, embedded on structures or surface-mounted will require relocation to accommodate the new noise barriers.	MTO City of Ottawa	16.1.1	The affected ATMS ducts will be relocated to accommodate the new noise barriers, as specified in the Contract drawings.  <b>Contract Drawings</b>
16.2	A City of Ottawa 1220 mm watermain is located south of Highway 417 within the project limits, on the north side of Isabella Avenue between Bank Street and O'Connor Street, and crossing the Highway 417 Rochester eastbound off-ramp just south of the existing barrier wall running along the edge of the ramp between Rochester Street and Preston Street. The proposed works have the potential to cause vibrations which may impact the watermain during construction.	MTO City of Ottawa	16.2.1	During construction, the Contractor will be required to protect and monitor the existing 1220 mm watermain as specified in the Contract documents. During construction, protection and monitoring of the existing 1220 mm watermain is required from just south of the proposed noise barrier east of the O-Train overpass and along the existing retaining wall east of Bank Street for a distance of approximately 65 m.  <b>Operational Constraint – Large Diameter Watermains Over 300 mm Vibration Monitoring – Item No.</b>
<b>CULTURAL ENVIRONMENT</b>				
<b>17.0 Archaeology (Section 4.4.1)</b>				<b>TESR Reference: Section 6.2.3.1 and Table 6.5</b>
17.1	No archaeological resources were found during	MTO MTCS	17.1.1	In the event that deeply buried archaeological deposits are discovered in the course of construction, the Ministry of Tourism, Culture and Sport (416-314-1177) should be notified



I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
	Archaeological Assessments, however there is potential for impact to archaeological resources.			<p>immediately. Should previously undocumented archaeological resources be discovered, they may be new archaeological sites and therefore subject to Section 48 (1) of the <i>Ontario Heritage Act</i>. The proponent or person discovering the archaeological resources must cease alteration of the site immediately and engage a licensed consultant archaeologist to carry out a determination of their nature and significance.</p> <p><b>General Conditions of the Contract</b></p>
			17.1.2	<p>In the event that human remains are encountered during construction, the Cemeteries Regulation Unit of the Ministry of Consumer Service (1-800-889-9768) should be notified. In situations where human remains are associated with archaeological resources, the Ministry of Tourism, Culture and Sport should also be contacted to ensure that the site is not subject to unlicensed alterations which would be a contravention of the <i>Ontario Heritage Act</i>.</p> <p><b>General Conditions of the Contract</b></p>
<p><b>NOTE:</b> The TESR Reference is to the environmental issues, potential effects and mitigation measures and proposed mitigation and commitments to future work tables contained within the MTO <i>Highway 417 (Ottawa Queensway) from Highway 416 Easterly to Anderson Road Preliminary Design Study And Environmental Assessment Transportation Environmental Study Report</i> GWP 663-93-00 (January 2007).</p>				

## 5 MONITORING

The Ministry of Transportation has an internal process to identify and address updates to the Ontario Provincial Standard Specifications (OPSS) and MTO Special Provisions. This includes on-going review of unanticipated events that occur during other construction contracts and incorporation of required updates into future contract provisions. This helps to assess the effectiveness of the contract provisions to ensure that they are providing the expected control and/or protection.

An independent on-site Contract Administrator (CA Consultant) is retained by MTO to ensure that construction proceeds as per the contract documentation, including environmental protection.

During construction, the on-site CA Consultant ensures that implementation of mitigating measures and key design features are consistent with the contract requirements. In addition, the effectiveness of the environmental impact mitigation measures is assessed to ensure that:

- Individual mitigation measures are providing the expected control and/or protection;
- Composite control and/or protection provided by the mitigation measures is adequate; and
- Additional mitigation measures are provided, as required, for any unanticipated environmental problems that may develop during construction.

In the event that problems develop, the CA Consultant is there to ensure that MTO and appropriate agencies are contacted.



**HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN  
AND THE RIDEAU CANAL, DETAIL DESIGN (WP 4088-07-01)**

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**Design and Construction Report**

**APPENDIX A**  
**Notification Material**

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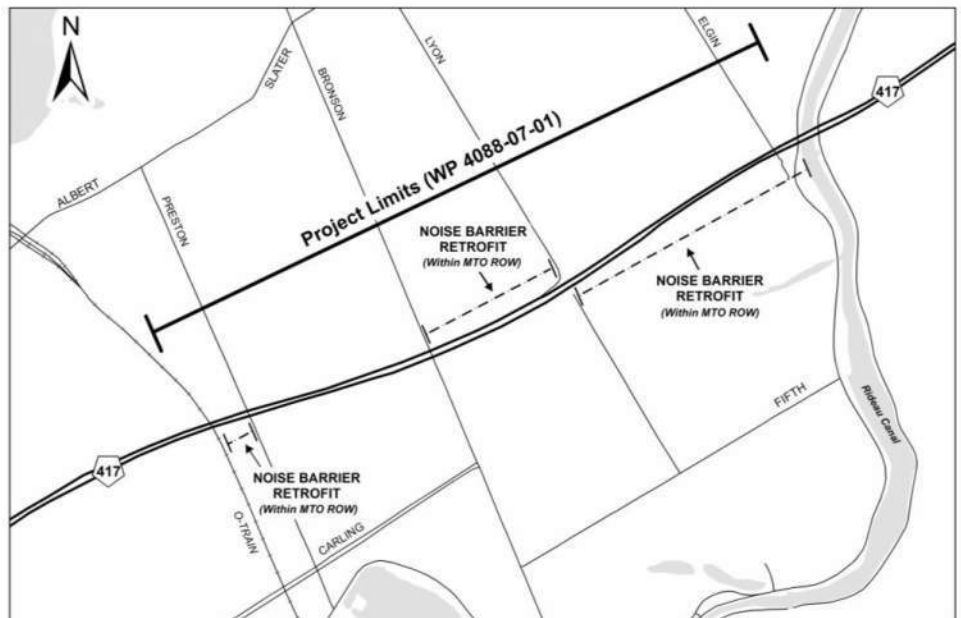
**NOTICE OF STUDY COMMENCEMENT**  
**HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU CANAL,**  
**DETAIL DESIGN (WP 4088-07-01)**

---

## THE STUDY

The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for various noise barrier retrofits along Highway 417 between the O-Train tracks and the Rideau Canal (WP 4088-07-01). The project involves the following components:

- Construction of new retrofit noise barriers at the following locations:
  - O-Train to Preston Street (south side of Highway 417);
  - Bronson Avenue to Lyon Street (north side of Highway 417);
  - Lyon Street to the Rideau Canal (south side of Highway 417);
- Construction of related works including temporary concrete barriers for staging, minor drainage works, and illumination modifications/ATMS; and
- Staged lane reductions/closures and ramp closures to accommodate installation of noise barriers and night work, as anticipated.



## THE PROCESS

This project is following the approved environmental planning process for Group “B” undertakings under the Ministry of Transportation’s *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities* (2000), with the opportunity for public input throughout the project.

The preliminary design was documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in 2008. A Public Information Centre (PIC) will be held to present findings and provide stakeholders with an opportunity to review and comment on the proposed detail design. Notices will be published in local newspapers in advance of the PIC to advertise the time, date, and location of the meeting. Upon completion of the detail design, a Design and Construction Report (DCR) will be prepared and filed for a 30-day public review period. A Notice of DCR Submission will be published in local newspapers at that time to explain the review process and identify locations where the DCR can be reviewed.

## COMMENTS

We are interested in hearing any comments that you may have regarding this study. If you wish to obtain additional information or provide comments, please contact the Consultant Project Manager or MTO Senior Project Engineer listed below, or visit the project website at [www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com).

**Mr. Manny Goetz, P.Eng.**

Consultant Project Manager

McCormick Rankin Corporation  
1145 Hunt Club Road, Suite 300  
Ottawa, ON K1V 0Y3

Phone: (613) 736-7200  
1-877-998-9912 (toll free)

Fax: (613) 736-8710  
E-mail: [mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)

**Mr. David Lindensmith, P.Eng.**

MTO Senior Project Engineer

Ministry of Transportation Eastern Region  
1355 John Counter Blvd., Postal Bag 4000  
Kingston, ON K7L 5A3

Phone: (613) 540-5130  
1-800-267-0295 (toll free)

Fax: (613) 540-5106  
E-mail: [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)

If you have any accessibility requirements in order to participate in this project please contact one of the Project Team members listed above.

Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act (FOIPPA)*. With the exception of personal information, all comments will be part of the public record.

Des renseignements sont disponibles en français en composant (613) 736-7200 poste 3287, auprès de M. Michel Bisson, P.Eng., Courriel: [mbisson@mrc.ca](mailto:mbisson@mrc.ca).



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## AVIS DE DÉBUT D'ÉTUDE

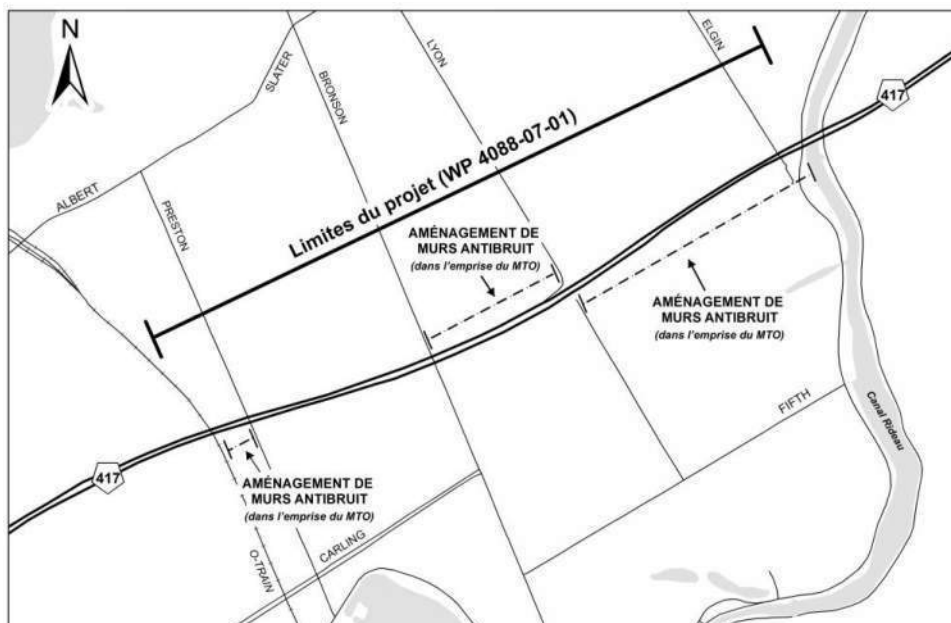
### AMÉNAGEMENT DE MURS ANTIBRUIT SUR L'AUTOROUTE 417 ENTRE L'O-TRAIN ET LE CANAL RIDEAU, CONCEPTION DÉTAILLÉE (WP 4088-07-01)

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#### L'ÉTUDE

Le ministère des Transports de l'Ontario (MTO) a retenu les services de McCormick Rankin (MRC), un membre du Groupe MMM Limitée, pour exécuter la conception détaillée relative à l'aménagement de plusieurs murs antibruit le long de l'autoroute 417 entre la voie de l'O-Train et le canal Rideau (WP 4088-07-01). Le projet comprend les éléments suivants :

- construction de nouveaux murs antibruit aux emplacements suivants :
  - de l'O-Train jusqu'à la rue Preston (côté sud de l'autoroute 417);
  - de l'avenue Bronson jusqu'à la rue Lyon (côté nord de l'autoroute 417);
  - de la rue Lyon jusqu'au canal Rideau (côté sud de l'autoroute 417);
- construction d'ouvrages connexes, y compris des barrières temporaires en béton pour les aires de montage, les travaux mineurs de drainage, les modifications de l'éclairage et le système avancé de gestion de la circulation; et
- réductions et fermetures des voies correspondant aux aires de montage pour accueillir l'installation de murs antibruit et pour les travaux de nuit, tel que prévu.



#### LE PROCESSUS

Ce projet suit le processus de planification environnementale approuvé pour les projets du groupe « B » en vertu de l'*Évaluation environnementale de portée générale pour les installations provinciales de transport* (2000) du ministère des Transports, et le public aura l'occasion de participer pendant toute la durée du projet.

La conception préliminaire a été documentée dans un rapport d'étude environnementale pour les transports (REET), qui a reçu l'autorisation environnementale en 2008. Une séance d'information publique (SIP) aura lieu pour présenter les conclusions et donner aux intervenants l'occasion d'examiner et de commenter la conception détaillée proposée. Des avis seront publiés dans les journaux locaux avant la SIP pour faire connaître l'heure, la date et le lieu de la rencontre. Dès l'achèvement de la conception détaillée, un rapport de design et de construction (RDC) sera préparé et exposé au public durant une période de 30 jours. Un avis de présentation de RDC paraîtra alors dans les journaux locaux pour expliquer le processus d'examen et indiquer les endroits où le rapport sera exposé.

#### COMMENTAIRES

Nous accueillerons volontiers tous vos commentaires éventuels sur cette étude. Si vous souhaitez recevoir des renseignements supplémentaires ou formuler des commentaires, veuillez communiquer avec le chargé de projet de la firme ou l'ingénieur principal du MTO chargé du projet indiqué ci-dessous, ou visiter le site Web du projet au [www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com).

**M. Manny Goetz, P.Eng.**

Chargé de projet de la firme

McCormick Rankin  
1145, chemin Hunt Club, bur. 300  
Ottawa (Ontario) K1V 0Y3

N° de tél. : (613) 736-7200  
1 877 998-9912 (sans frais)

N° de téléc. : (613) 736-8710

Courriel : [mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)

**M. David Lindensmith, P.Eng.**

Ingénieur principal du MTO chargé du projet

Ministère des Transports, région de l'Est  
1355, boul. John Counter, sac postal 4000  
Kingston (Ontario) K7L 5A3

N° de tél. : (613) 540-5130  
1 800 267-0295 (sans frais)

N° de téléc. : (613) 540-5106

Courriel : [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)

Si vous avez des besoins spéciaux en matière d'accessibilité pour participer à ce projet, veuillez communiquer avec un des membres de l'équipe de projet indiqués ci-dessus.

Les renseignements seront recueillis conformément à la *Loi sur l'accès à l'information et la protection de la vie privée (LAIPVP)*. À l'exception des renseignements personnels, tous les commentaires feront partie du domaine public.

Des renseignements sont disponibles en français au numéro (613) 736-7200, poste 3287, auprès de M. Michel Bisson, P.Eng.; courriel: [mbisson@mrc.ca](mailto:mbisson@mrc.ca).



**Study Commencement Letter - Local Elected Representatives**



**Ministry of Transportation**

Planning and Design Section  
1355 John Counter Boulevard  
Postal Bag 4000  
Kingston, Ontario K7L 5A3  
Tel.: 613 540-5130  
Fax: 613-540-5106

**Ministère des Transports**

Section de la planification et de la  
conception  
1355, boulevard John Counter  
CP/Service de sacs 4000  
Kingston (Ontario) K7L 5A3  
Tél.: 613 540-5130  
Télééc. 613 540-5106

March 8, 2013

«Title» «First\_Name» «Last\_Name»  
«Business\_Title»  
«Company»  
«Address»  
«City», «Province» «Postal\_Code»

Dear «Title» «First\_Name» «Last\_Name»:

**RE: COMMENCEMENT OF DETAIL DESIGN  
HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU  
CANAL (WP 4088-07-01)**

---

The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for various noise barrier retrofits along Highway 417 between the O-Train tracks and the Rideau Canal (WP 4088-07-01), as shown on the enclosed notice. The purpose of this letter is to inform you of the project.

The project involves the following components:

- Construction of new retrofit noise barriers at the following locations:
  - O-Train to Preston Street (south side of Highway 417);
  - Bronson Avenue to Lyon Street (north side of Highway 417);
  - Lyon Street to the Rideau Canal (south side of Highway 417);
- Construction of related works including temporary concrete barriers for staging, minor drainage works, and illumination modifications/ATMS; and
- Staged lane reductions/closures and ramp closures to accommodate installation of noise barriers and night work, as anticipated.

Please be advised that the Ontario Government Notice (OGN) advertising Study Commencement will be published in local newspapers as follows:

The Ottawa Citizen	March 8, 2013
Le Droit	March 8, 2013

A copy of the notice is attached for your information.



This project is following the approved environmental planning process for Group 'B' undertakings under the *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities* (2000), with the opportunity for public input throughout the project. The preliminary design was documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in 2008.

A Public Information Centre (PIC) will be held to present findings and provide stakeholders with an opportunity to review and comment on the proposed detail design. Notices will be published in local newspapers and an invitation letter will be sent in advance of the PIC to advertise the time, date, and location of the meeting.

Upon completion of the detail design, a Design and Construction Report (DCR) will be prepared and filed for a 30-day review period. Newspaper notices will be published and a letter will be sent at that time to explain the review process and identify locations where the DCR can be reviewed.

Should you require further information, please feel free to contact the undersigned at the address listed below. More information is also available on the project website at **[www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)**.

**Mr. David Lindensmith, P.Eng.**

MTO Senior Project Engineer

Ministry of Transportation Eastern Region  
1355 John Counter Blvd., Postal Bag 4000  
Kingston, ON K7L 5A3

Phone: (613) 540-5130  
1-800-267-0295 (toll free)

Fax: (613) 540-5106

E-mail: [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)

Under the *Freedom of Information and Protection of Privacy Act (FOIPPA)* comments and information regarding this project, with the exception of personal information, will become part of the public record.

Des renseignements sont disponibles en français en composant (613) 736-7200 poste 3287, auprès de M. Michel Bisson, P.Eng., Courriel: [mbisson@mrc.ca](mailto:mbisson@mrc.ca).

Yours very truly,

David Lindensmith, P.Eng.  
MTO Senior Project Engineer

cc: Mr. Manny Goetz, Consultant Project Manager – McCormick Rankin  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd.



**Ministry of Transportation**

Planning and Design Section  
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Postal Bag 4000  
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**Ministère des Transports**

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conception  
1355, boulevard John Counter  
CP/Service de sacs 4000  
Kingston (Ontario) K7L 5A3  
Tél.: 613 540-5130  
Télééc. 613 540-5106

March 8, 2013

«Title» «First\_Name» «Last\_Name»  
«Group»  
«Address»  
«City», «Province» «Postal\_Code»

Dear «Title» «Last\_Name»:

**RE: COMMENCEMENT OF DETAIL DESIGN  
HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU  
CANAL (WP 4088-07-01)**

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The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for various noise barrier retrofits along Highway 417 between the O-Train tracks and the Rideau Canal (WP 4088-07-01), as shown on the enclosed key map. The purpose of this letter is to inform you of the project.

The project involves the following components:

- Construction of new retrofit noise barriers at the following locations:
  - O-Train to Preston Street (south side of Highway 417);
  - Bronson Avenue to Lyon Street (north side of Highway 417);
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This project is following the approved environmental planning process for Group 'B' undertakings under the *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities* (2000), with the opportunity for public input throughout the project. The preliminary design was documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in 2008.

A Public Information Centre (PIC) will be held to present findings and provide stakeholders with an opportunity to review and comment on the proposed detail design. Notices will be published in local newspapers and an invitation letter will be sent to the project mailing list in advance of the PIC to advertise the time, date and location of the meeting.

Upon completion of the detail design, a Design and Construction Report (DCR) will be prepared and filed for a 30-day review period. Newspaper notices will be published and letters will be sent to the project mailing list at that time to explain the review process and identify locations where the DCR can be reviewed.

Stage 1 and Stage 2 Archaeological Assessments were undertaken during preliminary design. These investigations found no evidence of archaeological resources within the existing right-of-way of Highway 417, which has been significantly disturbed.

Please provide any information, comments or questions that you may have regarding this project. Should you require further information regarding this study, please feel free to contact the undersigned at the address listed below. You can also visit the project website for more information at **[www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)**.

**Mr. David Lindensmith, P.Eng.**

MTO Senior Project Engineer

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1355 John Counter Blvd., Postal Bag 4000  
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Yours very truly,

David Lindensmith, P.Eng.

MTO Senior Project Engineer

cc: Mr. Manny Goetz, Consultant Project Manager – McCormick Rankin  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd.

## Study Commencement Letter to External Agencies



McCormick Rankin  
1145 Hunt Club Road, Suite 300  
Ottawa, ON Canada K1V 0Y3  
t: 613.736.7200 | f: 613.736.8710  
[www.mrc.ca](http://www.mrc.ca)

March 8, 2013

<<ADDRESS>>

Attention: <<NAME>>

**RE: COMMENCEMENT OF DETAIL DESIGN  
HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE  
RIDEAU CANAL (WP 4088-07-01)**

---

The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for various noise barrier retrofits along Highway 417 between the O-Train tracks and the Rideau Canal (WP 4088-07-01), as shown on the enclosed key map. The purpose of this letter is to inform you of the project.

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This project is following the approved environmental planning process for Group 'B' undertakings under the *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities* (2000), with the opportunity for public input throughout the project. The preliminary design was documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in 2008.

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Upon completion of the detail design, a Design and Construction Report (DCR) will be prepared and filed for a 30-day review period. Newspaper notices will be published and letters will be sent to

the project mailing list at that time to explain the review process and identify locations where the DCR can be reviewed.

Please provide any information, comments or questions that you may have regarding this project by **March 22, 2013** on the attached comment form or in a separate letter. Should you require further information regarding this study, please feel free to contact the Consultant Project Manager or the MTO Senior Project Engineer listed below, or visit the project website at **[www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)**.

**Mr. Manny Goetz, P.Eng.**  
Consultant Project Manager

McCormick Rankin  
1145 Hunt Club Road, Suite 300  
Ottawa, ON K1V 0Y3

Phone: (613) 736-7200  
1-877-998-9912 (toll free)  
Fax: (613) 736-8710  
E-mail: [mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)

**Mr. David Lindensmith, P.Eng.**  
MTO Senior Project Engineer

Ministry of Transportation Eastern Region  
1355 John Counter Blvd., Postal Bag 4000  
Kingston, ON K7L 5A3

Phone: (613) 540-5130  
1-800-267-0295 (toll free)  
Fax: (613) 540-5106  
E-mail: [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)

Under the *Freedom of Information and Protection of Privacy Act (FOIPPA)* comments and information regarding this project, with the exception of personal information, will become part of the public record.

Des renseignements sont disponibles en français en composant (613) 736-7200 poste 3287, M. Michel Bisson, P.Eng., Courriel: [mbisson@mrc.ca](mailto:mbisson@mrc.ca).

Yours truly,

**McCormick Rankin, a member of MMM Group Limited**

Manny Goetz, P.Eng.  
Consultant Project Manager

cc: Mr. David Lindensmith, Senior Project Engineer – MTO Eastern Region  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd



McCormick Rankin  
1145 Hunt Club Road, Suite 300  
Ottawa, ON Canada K1V 0Y3  
t: 613.736.7200 | f: 613.736.8710  
[www.mrc.ca](http://www.mrc.ca)

March 8, 2013

«Title» «First\_Name» «Last\_Name» «Association»  
«Address»  
«City», «Province» «Postal\_Code»

Attention: «Title» «First\_Name» «Last\_Name»

**RE: COMMENCEMENT OF DETAIL DESIGN  
HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE  
RIDEAU CANAL (WP 4088-07-01)**

---

The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for various noise barrier retrofits along Highway 417 between the O-Train tracks and the Rideau Canal (WP 4088-07-01), as shown on the enclosed key map. The purpose of this letter is to inform you of the project.

The project involves the following components:

- Construction of new retrofit noise barriers at the following locations:
  - O-Train to Preston Street (south side of Highway 417);
  - Bronson Avenue to Lyon Street (north side of Highway 417);
  - Lyon Street to the Rideau Canal (south side of Highway 417);
- Construction of related works including temporary concrete barriers for staging, minor drainage works, and illumination modifications/ATMS; and
- Staged lane reductions/closures and ramp closures to accommodate installation of noise barriers and night work, as anticipated.

This project is following the approved environmental planning process for Group 'B' undertakings under the *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities* (2000), with the opportunity for public input throughout the project. The preliminary design was documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in 2008.

The purpose of the detail design phase is to prepare the approved plan for implementation, including preparing contract documents, refining the mitigation strategy and obtaining required approvals/permits.

A Public Information Centre (PIC) will be held to provide stakeholders with an opportunity to review and comment on the proposed detail design. Notices will be published in local newspapers and an



invitation letter will be sent to the project mailing list in advance of the PIC to advertise the time, date, and location of the meeting.

Upon completion of the detail design, a Design and Construction Report (DCR) will be prepared and filed for a 30-day review period. Newspaper notices will be published and letters will be sent to the project mailing list at that time to explain the review process and identify locations where the DCR can be reviewed.

Should you require further information regarding this study, please feel free to contact the Consultant Project Manager or the MTO Senior Project Engineer listed below, or visit the project website at **[www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)**.

**Mr. Manny Goetz, P.Eng.**  
Consultant Project Manager

McCormick Rankin  
1145 Hunt Club Road, Suite 300  
Ottawa, ON K1V 0Y3

Phone: (613) 736-7200  
1-877-998-9912 (toll free)

Fax: (613) 736-8710

E-mail: [mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)

**Mr. David Lindensmith, P.Eng.**  
MTO Senior Project Engineer

Ministry of Transportation Eastern Region  
1355 John Counter Blvd., Postal Bag 4000  
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Des renseignements sont disponibles en français en composant (613) 736-7200 poste 3287, auprès de M. Michel Bisson, P.Eng., Courriel: [mbisson@mrc.ca](mailto:mbisson@mrc.ca).

Yours truly,

**McCormick Rankin, a member of MMM Group Limited**

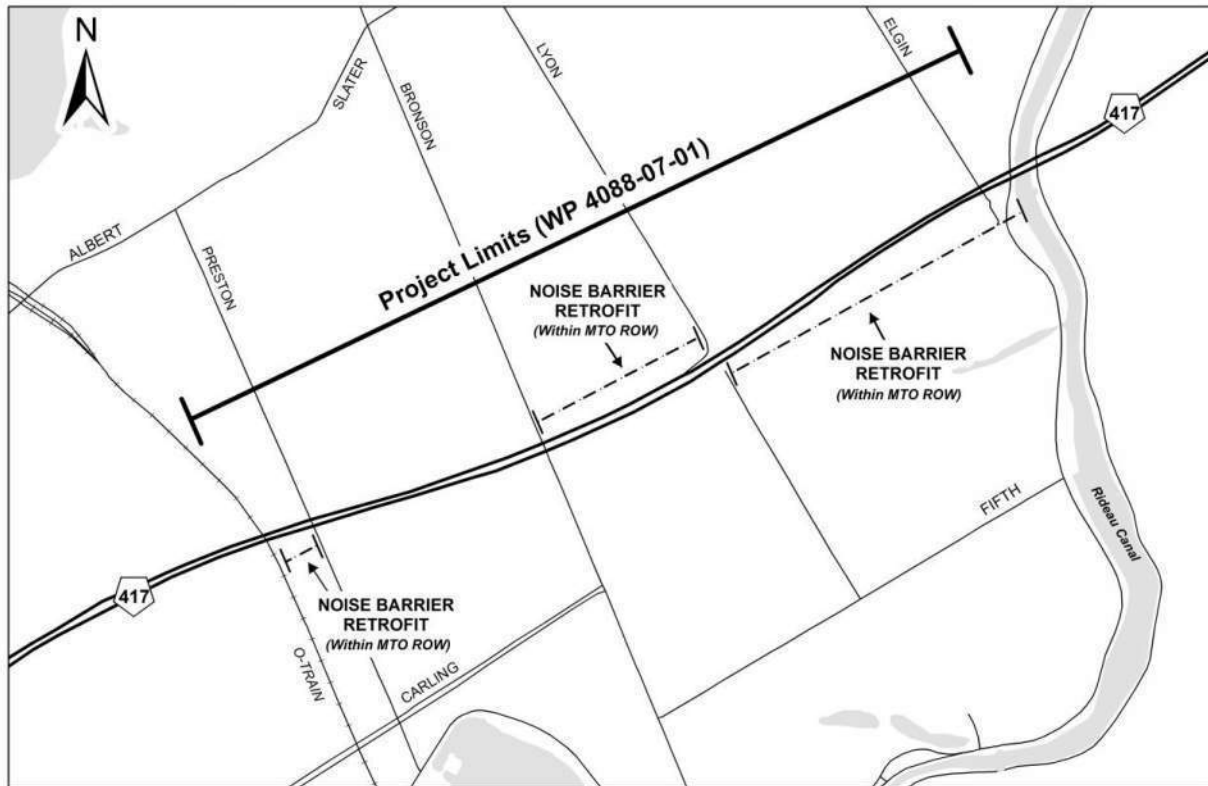
Manny Goetz, P.Eng.  
Consultant Project Manager

cc: Mr. David Lindensmith, Senior Project Engineer – MTO Eastern Region  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd

# KEY MAP

## HIGHWAY 417 NOISE BARRIER RETROFITS

(WP 4088-07-01)





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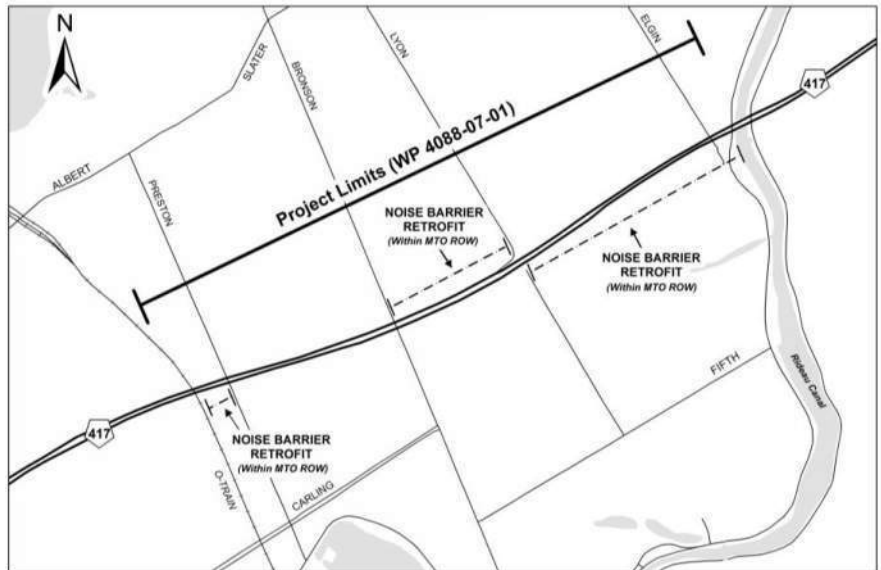
**NOTICE OF PUBLIC INFORMATION CENTRE**  
**HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU CANAL,**  
**DETAIL DESIGN (WP 4088-07-01)**

---

### THE STUDY

The Ministry of Transportation (MTO) has retained WSP | MMM Group to complete the detail design for various noise barrier retrofits along Highway 417 between the O-Train tracks and the Rideau Canal (WP 4088-07-01). The project involves the following components:

- Construction of new retrofit noise barriers at the following locations:
  - O-Train to Preston Street (south side of Highway 417);
  - Bronson Avenue to Lyon Street (north side of Highway 417);
  - Lyon Street to the Rideau Canal (south side of Highway 417 and along ramps);
- Construction of related works, including retaining walls, roadside protection, drainage works, illumination modifications, and utility relocations, as required;
- Traffic management during construction will be coordinated with the City of Ottawa. It is anticipated that staged lane reductions/closures, ramp closures and night work will be required to accommodate installation of noise barriers.



### PUBLIC CONSULTATION

A Public Information Centre (PIC) has been arranged to provide agencies, interested groups, business representatives and members of the general public with an opportunity to review and comment on the proposed design, potential impacts and proposed mitigation. Members of the project team will be available at the PIC to discuss the project with you and answer any questions you may have.

The PIC is scheduled as follows:

**Date:** Wednesday, January 20, 2016  
**Location:** St. Anthony's Hall (Hall A)  
523 St. Anthony Street  
Ottawa, Ontario  
**Time:** 4:00 PM to 8:00 PM

### THE PROCESS

This project is being carried out in accordance with the approved environmental planning process for Group "B" undertakings under the MTO *Class Environmental Assessment* (Class EA) for *Provincial Transportation Facilities* (2000), with the opportunity for public input throughout the project. Near completion of the detail design, a Design and Construction Report (DCR) will be prepared and filed for a 30-day public review period. A newspaper notice will be published at that time to explain the review process and identify the locations where the DCR can be reviewed.

### COMMENTS

We are interested in hearing any comments that you may have regarding this study. If you are unable to attend the PIC and wish to obtain further information, or if you wish to provide comments, please contact either the Consultant Project Manager or the MTO Senior Project Engineer listed below. You can also visit the project website for more information at [www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com).

**Mr. Lincoln MacDonald, P.Eng.**  
Consultant Project Manager  
MMM Group Limited  
1145 Hunt Club Road, Suite 300  
Ottawa, ON K1V 0Y3  
Phone: 613-736-7200  
1-877-998-9912 (toll free)  
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MTO Senior Project Engineer  
Ministry of Transportation Eastern Region  
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1-800-267-0295 (toll free)  
Fax: 613-540-5106  
E-mail: [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)

If you have any accessibility requirements in order to participate in this project please contact one of the Project Team members listed above.

Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act (FOIPPA)*. With the exception of personal information, all comments will be part of the public record.

Des renseignements sont disponibles en français en composant (613) 736-7200 poste 3561, auprès de Meghan MacMillan, Courriel: [macmillanm@mmm.ca](mailto:macmillanm@mmm.ca).

AVIS DE SÉANCE D'INFORMATION PUBLIQUE

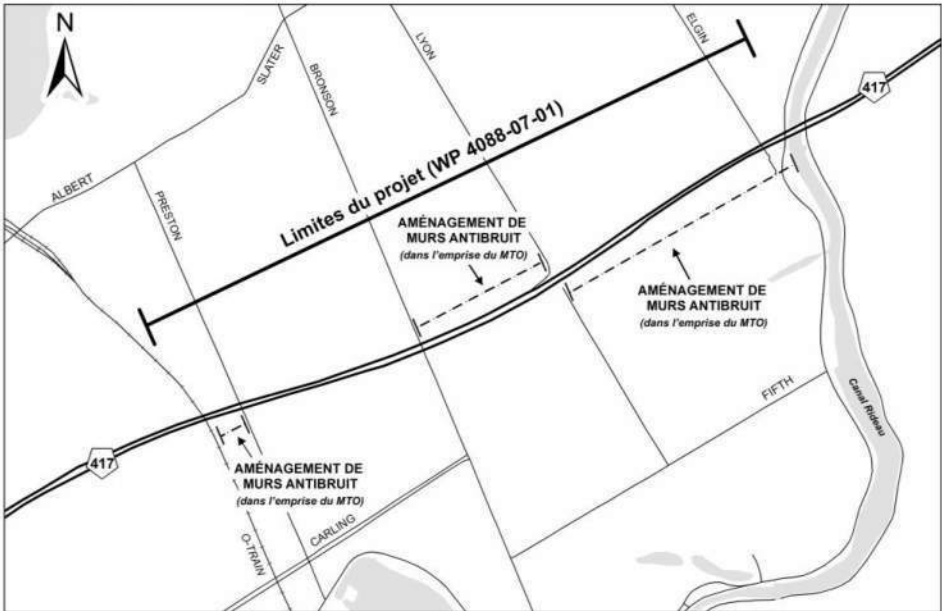
AMÉNAGEMENT DE MURS ANTIBRUIT SUR L'AUTOROUTE 417 ENTRE L'O-TRAIN ET LE CANAL RIDEAU,

CONCEPTION DÉTAILLÉE (WP 4088-07-01)

L'ÉTUDE

Le ministère des Transports (MTO) a retenu WSP | MMM Group pour exécuter la conception détaillée relative à l'aménagement de plusieurs murs antibruit le long de l'autoroute 417 entre la voie de l'O-Train et le canal Rideau (WP 4088-07-01). Le projet comprend les éléments suivants :

- Construction de nouveaux murs antibruit aux emplacements suivants :
  - de l'O-Train jusqu'à la rue Preston (côté sud de l'autoroute 417);
  - de l'avenue Bronson jusqu'à la rue Lyon (côté nord de l'autoroute 417);
  - de la rue Lyon jusqu'au canal Rideau (côté sud de l'autoroute 417);
- Construction d'ouvrages connexes, y compris des murs de soutènement, la protection routière, des travaux de drainage, les modifications de l'éclairage et déplacement des services publics, au besoin; et
- Gestion de la circulation pendant la construction, qui sera coordonnée avec la Ville d'Ottawa. Il faudra procéder à la réduction/fermeture progressive des voies, à la fermeture des bretelles et à des travaux nocturnes pour permettre l'installation des murs antibruit.



CONSULTATION PUBLIQUE

Une séance d'information publique (SIP) est prévue pour donner aux organismes, aux groupes intéressés, aux représentants d'entreprises et aux membres du grand public l'occasion d'examiner et de commenter la conception proposée, les répercussions potentielles et les mesures d'atténuation suggérées pour le projet. Les membres de l'équipe du projet y seront présents pour discuter avec vous et répondre à toutes vos questions éventuelles. La SIP est prévue comme suit :

Date :

Lieu :

Heure :

Le mercredi 20 janvier 2016

Salle de réception St. Anthony (Salle A)  
523, rue St. Anthony  
Ottawa (Ontario)

De 16 h à 20 h

LE PROCESSUS

Cette étude est conforme aux exigences du processus de planification environnementale approuvé pour les projets du groupe « B », en vertu de l'*Évaluation environnementale de portée générale pour les installations de transport provinciales* (2000), du ministère des Transports, qui offre au public la possibilité d'exprimer ses commentaires pendant toute la durée du projet. Vers l'achèvement de la conception détaillée, un rapport de conception et de construction (RCC) sera préparé et mis à la disposition du public durant 30 jours. Un avis de présentation du RCC paraîtra alors dans les journaux locaux pour expliquer le processus d'examen et indiquer les lieux où le RCC pourra être consulté.

COMMENTAIRES

Nous accueillerons volontiers tous vos commentaires sur cette étude. Si vous ne pouvez pas assister à la séance d'information publique et avez des questions ou des commentaires à formuler, n'hésitez pas à communiquer avec l'une ou l'autre des deux personnes ci-dessous (le chargé de projet de la firme-conseil ou l'ingénieur principal du MTO), ou à vous rendre sur le site Web du projet à l'adresse [www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com).

**M. Lincoln MacDonald, ing.**

Chargé de projet de la firme-conseil  
MMM Group Ltd.  
1145, chemin Hunt Club, bureau 300  
Ottawa (Ontario) K1V 0Y3  
Tél. : 613 736-7200  
1 877 998-9912 (sans frais)  
Télé. : 613 736-8710  
Courriel : [macdonaldl@mmm.ca](mailto:macdonaldl@mmm.ca)

**M. David Lindensmith, ing.**

Ingénieur principal chargé du projet du MTO  
Ministère des Transports, Région de l'Est  
1355, boul. John Counter, sac postal 4000  
Kingston (Ontario) K7L 5A3  
Tél. : 613 540-5130  
1 800 267-0295 (sans frais)  
Télé. : 613 540-5106  
Courriel : [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)

Si vous avez des besoins spéciaux en matière d'accessibilité et désirez participer à cette étude, veuillez communiquer avec l'un des membres de l'équipe nommés ci-dessus.

Les renseignements seront recueillis conformément à la *Loi sur l'accès à l'information et la protection de la vie privée* (LAIPVP). À l'exception des renseignements personnels, tous les commentaires seront du domaine public.

Des renseignements sont disponibles en français au numéro 613 736-7200, poste 3561, auprès de Meghan MacMillan (courriel : [macmillanm@mmm.ca](mailto:macmillanm@mmm.ca)).

**PIC Letter to Local Elected Representatives**



**Ministry of Transportation**

Planning and Design Section  
1355 John Counter Boulevard  
Postal Bag 4000  
Kingston, Ontario K7L 5A3  
Tel.: 613 540-5130  
Fax: 613-540-5106

**Ministère des Transports**

Section de la planification et de la  
conception  
1355, boulevard John Counter  
CP/Service de sacs 4000  
Kingston (Ontario) K7L 5A3  
Tél. : 613 540-5130  
Télééc. : 613 540-5106

January 4, 2016

<<NAME>>  
<<TITLE>>  
<<ADDRESS>>

Dear <<NAME>>:

**RE: NOTICE OF PUBLIC INFORMATION CENTRE  
HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU  
CANAL, DETAIL DESIGN (WP 4088-07-01)**

---

The Ministry of Transportation (MTO) has retained MMM Group Limited to complete the detail design for various noise barrier retrofits along Highway 417 between the O-Train tracks and the Rideau Canal (WP 4088-07-01), as shown in the enclosed notice.

The project involves the following components:

- Construction of new retrofit noise barriers at the following locations:
  - O-Train to Preston Street (south side of Highway 417);
  - Bronson Avenue to Lyon Street (north side of Highway 417);
  - Lyon Street to the Rideau Canal (south side of Highway 417 and along ramps); and
- Construction of related works, including retaining walls, roadside protection, drainage works, illumination modifications, and utility relocations, as required.
- Traffic management during construction will be coordinated with the City of Ottawa. It is anticipated that staged lane reductions/closures, ramp closures and night work will be required to accommodate installation of noise barriers.

The purpose of this letter is to invite you to attend a Public Information Centre (PIC) for this project. The PIC is scheduled as follows:

<b>Date:</b>	<b>Wednesday, January 20, 2016</b>
<b>Location:</b>	<b>St. Anthony's Hall 523 St. Anthony Street Ottawa, Ontario</b>
<b>Time:</b>	<b>4:00 PM to 8:00 PM</b>

**You are also invited to attend a one hour advance session scheduled from 3:00 p.m. to 4:00 p.m.**



This PIC will provide an opportunity to review and comment on the proposed design, anticipated environmental impacts and mitigation strategy. Members of the project team will be available to discuss the project and answer any questions you may have.

This project is following the approved environmental planning process for Group 'B' undertakings under the *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities (2000)*, with the opportunity for public input throughout the project.

Please be advised that the Ontario Government Notice (OGN) advertising this PIC will be published in local newspapers as follows:

The Ottawa Citizen  
Ottawa Le Droit

Wednesday, January 6, 2016  
Wednesday, January 6, 2016

A copy of the notice is attached for your information.

If you are unable to attend the PIC but wish to obtain additional information, or submit comments, please feel free to contact the undersigned at the address listed below. More information is also available on the project website at **[www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)**.

**David Lindensmith, P.Eng.**

MTO Senior Project Engineer

Ministry of Transportation Eastern Region  
1355 John Counter Blvd., Postal Bag 4000  
Kingston, ON K7L 5A3

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Yours very truly,

David Lindensmith, P.Eng.  
MTO Senior Project Engineer

cc: Mr. Lincoln MacDonald, P.Eng., Consultant Project Manager – MMM Group Ltd.  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd.

**PIC Letter to Aboriginal Communities**



**Ministry of Transportation**

Planning and Design Section  
1355 John Counter Boulevard  
Postal Bag 4000  
Kingston, Ontario K7L 5A3  
Tel.: 613 540-5130  
Fax: 613-540-5106

**Ministère des Transports**

Section de la planification et de la  
conception  
1355, boulevard John Counter  
CP/Service de sacs 4000  
Kingston (Ontario) K7L 5A3  
Tél.: 613 540-5130  
Télééc.: 613 540-5106

January 4, 2016

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<<ADDRESS>>

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This PIC will provide an opportunity to review and comment on the proposed design, anticipated environmental impacts and mitigation strategy. Members of the project team will be available to discuss the project and answer any questions you may have.

This project is following the approved environmental planning process for Group 'B' undertakings under the *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities (2000)*, with the opportunity for public input throughout the project.

If you are unable to attend the PIC but wish to obtain additional information, or submit comments, please feel free to contact the undersigned at the address listed below. More information is also available on the project website at **[www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)**.

**David Lindensmith, P.Eng.**

MTO Senior Project Engineer

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Yours very truly,

David Lindensmith, P.Eng.

MTO Senior Project Engineer

cc: Mr. Lincoln MacDonald, P.Eng., Consultant Project Manager – MMM Group Ltd.  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd.

**PIC Notification Letter to External Agencies  
(including TAC)**



MMM Group Limited  
1145 Hunt Club Road, Suite 300  
Ottawa, ON Canada K1V 0Y3  
t: 613.736.7200 | f: 613.736.8710  
[www.mmmgrouplimited.com](http://www.mmmgrouplimited.com)

January 6, 2016

<<ADDRESS>>

Attention: <<NAME>>

**RE: NOTICE OF PUBLIC INFORMATION CENTRE  
HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE  
RIDEAU CANAL, DETAIL DESIGN (WP 4088-07-01)**

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**Mr. Lincoln MacDonald, P.Eng.**  
Consultant Project Manager

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Yours truly,

**MMM Group Limited**

Lincoln MacDonald, P.Eng.  
Consultant Project Manager

cc: Mr. David Lindensmith, Senior Project Engineer – MTO Eastern Region  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd.

**PIC Notification Letter to General Public**



MMM Group Limited  
1145 Hunt Club Road, Suite 300  
Ottawa, ON Canada K1V 0Y3  
t: 613.736.7200 | f: 613.736.8710  
[www.mmmgrouplimited.com](http://www.mmmgrouplimited.com)

January 6, 2016

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  - O-Train to Preston Street (south side of Highway 417);
  - Bronson Avenue to Lyon Street (north side of Highway 417);
  - Lyon Street to the Rideau Canal (south side of Highway 417 and along ramps); and
- Construction of related works, including retaining walls, roadside protection, drainage works, illumination modifications, and utility relocations, as required.
- Traffic management during construction will be coordinated with the City of Ottawa. It is anticipated that staged lane reductions/closures, ramp closures and night work will be required to accommodate installation of noise barriers.

The purpose of this letter is to invite you to attend a Public Information Centre (PIC) for this project. The PIC is scheduled as follows:

<b>Date:</b>	<b>Wednesday, January 20, 2016</b>
<b>Location:</b>	<b>St. Anthony's Hall (Hall A) 523 St. Anthony Street Ottawa, Ontario</b>
<b>Time:</b>	<b>4:00 PM to 8:00 PM</b>

This PIC will provide stakeholders with an opportunity to review and comment on the proposed design, anticipated environmental impacts and mitigation strategy. Members of the project team will be available to discuss the project and answer any questions you may have.

This project is following the approved environmental planning process for Group 'B' undertakings under the *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities* (2000), with the opportunity for public input throughout the project.

If you are unable to attend the PIC but wish to obtain additional information, or to submit comments, please feel free to contact the Consultant Project Manager or the MTO Senior Project Engineer listed below. You can also visit the project website for more information at [www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com).

**Mr. Lincoln MacDonald, P.Eng.**  
Consultant Project Manager

MMM Group Limited  
1145 Hunt Club Road, Suite 300  
Ottawa, ON K1V 0Y3

Phone: (613) 736-7200  
1-877-998-9912 (toll free)

Fax: (613) 736-8710

E-mail: [macdonaldl@mmm.ca](mailto:macdonaldl@mmm.ca)

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E-mail: [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)

If you have any accessibility requirements in order to participate in this project please feel free to contact one of the Project Team members listed above.

Under the *Freedom of Information and Protection of Privacy Act* (FOIPPA) comments and information regarding this project, with the exception of personal information, will become part of the public record.

Des renseignements sont disponibles en français en composant (613) 736-7200 poste 3561, auprès de Meghan MacMillan, courriel: [macmillanm@mmm.ca](mailto:macmillanm@mmm.ca).

Yours truly,

**MMM Group Limited**

Lincoln MacDonald, P.Eng.  
Consultant Project Manager

cc: Mr. David Lindensmith, Senior Project Engineer – MTO Eastern Region  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd.

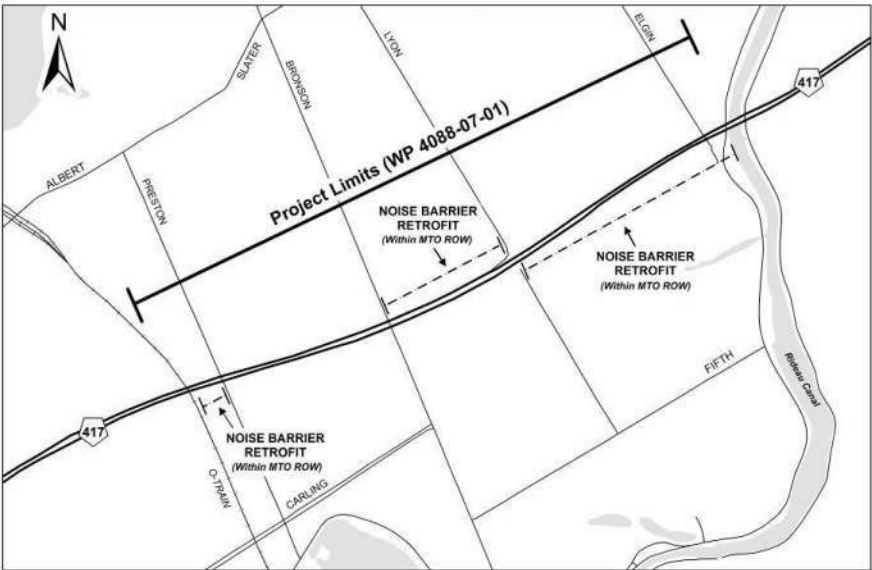


**NOTICE OF SUBMISSION - DESIGN AND CONSTRUCTION REPORT**  
**HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU CANAL,**  
**DETAIL DESIGN (WP 4088-07-01)**

**THE STUDY**

The Ministry of Transportation (MTO) has retained WSP | MMM Group to complete the detail design for various noise barrier retrofits along Highway 417 between the O-Train tracks and the Rideau Canal (WP 4088-07-01). The project involves the following components:

- Construction of new retrofit noise barriers at the following locations:
  - O-Train to Preston Street (south side of Highway 417);
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- Construction of related works, including modifications to existing structural barrier walls, roadside protection, and illumination modifications, as required;
- Traffic management during construction will occur in coordination with the City of Ottawa. It is anticipated that staged lane reductions/closures, ramp closures and night work will be required to accommodate installation of noise barriers.



**THE PROCESS**

This project was carried out in accordance with the approved environmental planning process for Group ‘B’ projects under the Ministry of Transportation’s *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities* (2000), with the opportunity for public input throughout the project.

The purpose of this notice is to inform the public that a Design and Construction Report (DCR) has been prepared and will be available from **February 5<sup>th</sup>, 2016 to March 7<sup>th</sup>, 2016** for a 30-day public review period. The DCR is available on the project website ([www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)) and can be reviewed at the following locations during normal hours of operation:

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**COMMENTS**

Interested persons are encouraged to review the DCR and provide comments by **March 7<sup>th</sup>, 2016**. If there are no outstanding concerns after the 30-day review period has expired, further documentation will not be prepared and construction may commence without further notice. If you have questions or comments please feel free to contact either the Consultant Project Manager or the MTO Project Engineer listed below. You can also visit the project website for more information.

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If you have any accessibility requirements in order to participate in this project please contact one of the Project Team members listed above.

Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act (FOIPPA)*. With the exception of personal information, all comments will be part of the public record.

Des renseignements sont disponibles en français au 613 736-7200, poste 3561, auprès de Meghan MacMillan, (courriel: [macmillanm@mmm.ca](mailto:macmillanm@mmm.ca)).

AVIS DE DÉPÔT – RAPPORT SUR LA CONCEPTION ET LA CONSTRUCTION

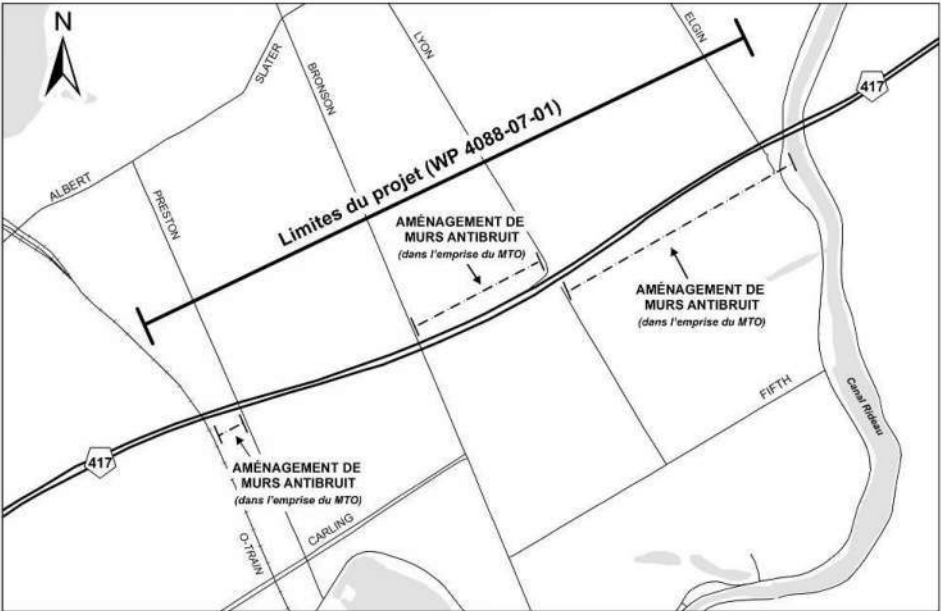
AMÉNAGEMENT DE MURS ANTIBRUIT SUR L'AUTOROUTE 417 ENTRE L'O-TRAIN ET LE CANAL RIDEAU,

CONCEPTION DÉTAILLÉE (WP 4088-07-01)

L'ÉTUDE

Le ministère des Transports (MTO) a retenu WSP | MMM Group pour exécuter la conception détaillée relative à l'aménagement de murs antibruit le long de l'autoroute 417 entre la voie de l'O-Train et le canal Rideau (WP 4088-07-01). Le projet comprend les éléments suivants :

- Construction de nouveaux murs antibruit aux emplacements suivants :
  - de l'O-Train jusqu'à la rue Preston (côté sud de l'autoroute 417);
  - de l'avenue Bronson jusqu'à la rue Lyon (côté nord de l'autoroute 417);
  - de la rue Lyon jusqu'au canal Rideau (côté sud de l'autoroute 417);
- Construction des ouvrages connexes, y compris les modifications aux murs actuels montés sur des structures, la protection des bords de route et les modifications à l'éclairage, au besoin;
- Gestion de la circulation pendant la construction, qui sera coordonnée avec la Ville d'Ottawa. Il faudra procéder à la réduction ou à la fermeture progressive des voies, à la fermeture des bretelles et à des travaux nocturnes pour permettre l'installation des murs antibruit.



LE PROCESSUS

Ce projet s'est déroulé conformément au processus approuvé de planification environnementale prévu pour les projets du groupe B, et ce, conformément à l'Évaluation environnementale de portée générale pour les installations provinciales de transport (2000) du ministère des Transports, qui offre à la population la possibilité d'exprimer ses commentaires pendant toute la durée du projet.

Le présent avis a pour objet d'informer le public qu'un rapport sur la conception et la construction (RCC) a été rédigé et sera disponible durant une période de 30 jours (du 5 février au 7 mars 2016) pour consultation publique. Le RCC figure sur le site Web du projet ([www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)) et peut être consulté aux emplacements suivants pendant les heures d'ouverture normales :

<b>Ville d'Ottawa</b> <b>Centre du service à la clientèle,</b> <b>Hôtel de ville</b> 110, avenue Laurier Ouest Ottawa (Ontario) K1P 1J1	<b>Bibliothèque publique d'Ottawa</b> <b>Succursale Centrale</b> 120, rue Metcalfe Ottawa (Ontario) K1P 5M2	<b>Centre récréatif McNabb</b> 180, rue Percy Ottawa (Ontario) K1R 6E5
Du lundi au vendredi : de 8 h 30 à 17 h	Du lundi au jeudi : de 10 h à 21 h Vendredi : de 10 h à 18 h Samedi : de 10 h à 17 h Dimanche : de 13 h à 17 h	Du lundi au jeudi : de 8 h à 22 h Vendredi et samedi : de 8 h à 20 h Dimanche : de 9 h à 20 h

COMMENTAIRES

Les personnes intéressées sont encouragées à examiner le RCC et à formuler des commentaires d'ici le 7 mars 2016. Si aucune préoccupation ne subsiste après la période d'examen de 30 jours, aucune autre documentation ne sera préparée et la construction pourra commencer sans autre préavis. Si vous avez des questions ou des commentaires, n'hésitez pas à communiquer avec une des personnes indiquées ci-dessous ou à consulter le site Web du projet :

<b>M. Lincoln MacDonald, P.Eng.</b> Chargé de projet de la firme-conseil MMM Group Limited 1145, chemin Hunt Club, bureau 300 Ottawa (Ontario) K1V 0Y3 Tél. : 613 736-7200 1 877 998-9912 (sans frais) Télééc. : 613 736-8710 Courriel : <a href="mailto:macdonaldl@mmm.ca">macdonaldl@mmm.ca</a>	<b>M. David Lindensmith, P.Eng.</b> Ingénieur principal chargé du projet du MTO Ministère des Transports, Région de l'Est 1355, boul. John Counter, sac postal 4000 Kingston (Ontario) K7L 5A3 Tél. : 613 540-5130 1 800 267-0295 (sans frais) Télééc. : 613 540-5106 Courriel : <a href="mailto:dave.lindensmith@ontario.ca">dave.lindensmith@ontario.ca</a>
---	---

Si vous avez des besoins spéciaux en matière d'accessibilité pour participer à ce projet, veuillez communiquer avec un des membres de l'équipe de projet indiqués ci-dessus.

Les renseignements seront recueillis conformément à la Loi sur l'accès à l'information et la protection de la vie privée. À l'exception des renseignements personnels, tous les commentaires deviendront du domaine public.

Des renseignements sont disponibles en français au 613 736-7200, poste 3561, auprès de Meghan MacMillan, (courriel: [macmillanm@mmm.ca](mailto:macmillanm@mmm.ca)).

**Ministry of Transportation**

Planning and Design Section  
1355 John Counter Boulevard  
Postal Bag 4000  
Kingston, Ontario K7L 5A3  
Tel.: 613 540-5130  
Fax: 613-540-5106

**Ministère des Transports**

Section de la planification et de la  
conception  
1355, boulevard John Counter  
CP/Service de sacs 4000  
Kingston (Ontario) K7L 5A3  
Tél.: 613 540-5130  
Télééc. 613 540-5106



February 4, 2016

<<NAME>>  
<<ADDRESS>>

Dear <<NAME>>:

**RE: SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU  
CANAL, DETAIL DESIGN (WP 4088-07-01)**

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The Ministry of Transportation (MTO) has retained MMM Group Limited to complete the detail design for various noise barrier retrofits along Highway 417 between the O-Train tracks and the Rideau Canal (WP 4088-07-01). The project involves the following components:

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The purpose of this letter is to inform you that a Design and Construction Report (DCR), documenting the detail design and associated environmental protection plan, has been prepared and will be available from **February 5<sup>th</sup>, 2016 to March 7<sup>th</sup>, 2016** for a 30-day public review period. The DCR is available on the project website ([www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)) and can be reviewed at the following locations during normal hours of operation.

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Please be advised that the Ontario Government Notice (OGN) advertising submission of the DCR will be published in local newspapers as follows:

The Ottawa Citizen  
Ottawa Le Droit

**Friday, February 5, 2016**  
**Friday, February 5, 2016**

A copy of the notice is attached for your information.

Interested persons are encouraged to review the DCR and provide comments by **March 7<sup>th</sup>, 2016**. If there are no outstanding concerns after the 30-day review period has expired, further documentation will not be prepared and construction may commence without further notice. If you have further questions or comments, please feel free to contact the undersigned at the address listed below.

**David Lindensmith, P.Eng.**  
MTO Senior Project Engineer

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cc: Mr. Lincoln MacDonald, P.Eng., Consultant Project Manager – MMM Group Ltd.  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
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**Ministry of Transportation**

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February 4, 2016

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<<ADDRESS>>

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If requested, a digital copy of the DCR can also be provided via email.

Interested persons are encouraged to review the DCR and provide comments by **March 7<sup>th</sup>, 2016**. If there are no outstanding concerns after the 30-day review period has expired, further documentation will not be prepared and construction may commence without further notice. If you have further questions or comments, please feel free to contact the undersigned at the address listed below.

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Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd.

**DCR Submission -  
Letter to External Agencies (including  
TAC)**



MMM Group Limited  
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[www.mmmgrouplimited.com](http://www.mmmgrouplimited.com)

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Attention: <<NAME>>  
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**DCR Submission -  
Letter to General Public**



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Ottawa, ON Canada K1V 0Y3  
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February 4, 2016

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Attention: <<NAME>>  
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- Construction of related works, including modifications to existing structural barrier walls, roadside protection, and illumination modifications, as required.
- Traffic management during construction will occur in coordination with the City of Ottawa. It is anticipated that staged lane reductions/closures, ramp closures and night work will be required to accommodate installation of noise barriers.

This project was carried out in accordance with the approved environmental planning process for Group 'B' undertakings under the *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities* (2000), with the opportunity for public input throughout the project. The preliminary design was documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in 2008.

The purpose of this letter is to inform you that a Design and Construction Report (DCR), documenting the detail design and associated environmental protection plan, has been prepared and will be available from **February 5<sup>th</sup>, 2016 to March 7<sup>th</sup>, 2016** for a 30-day public review period. The DCR is available on the project website ([www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)) and can be reviewed at the following locations during normal hours of operation.

**City of Ottawa  
Client Service Centre, City Hall**  
110 Laurier Avenue West  
Ottawa, ON K1P 1J1

Monday to Friday:  
8:30 a.m. to 5 p.m.

**Ottawa Public Library  
Main Branch**  
120 Metcalfe Street  
Ottawa, ON K1P 5M2

Monday to Thursday: 10 a.m. to 9 p.m.  
Friday: 10 a.m. to 6 p.m.  
Saturday: 10 a.m. to 5 p.m.  
Sunday: 1 p.m. to 5 p.m.

**McNabb Recreation Centre**  
180 Percy Street  
Ottawa, ON K1R 6E5

Monday to Thursday: 8 a.m. to 10 p.m.  
Friday and Saturday: 8 a.m. to 8 p.m.  
Sunday: 9 a.m. to 8 p.m.

Interested persons are encouraged to review the DCR and provide comments by **March 7<sup>th</sup>, 2016**. If there are no outstanding concerns after the 30-day review period has expired, further documentation will not be prepared and construction may commence without further notice. If you have further questions or comments, please feel free to contact either the Consultant Project Manager or the MTO Project Engineer listed below.

**Mr. Lincoln MacDonald, P.Eng.**  
Consultant Project Manager

MMM Group Limited  
1145 Hunt Club Road, Suite 300  
Ottawa, ON K1V 0Y3

Phone: (613) 736-7200  
1-877-998-9912 (toll free)  
Fax: (613) 736-8710  
E-mail: macdonaldl@mmm.ca

**Mr. David Lindensmith, P.Eng.**  
MTO Senior Project Engineer

Ministry of Transportation Eastern Region  
1355 John Counter Blvd., Postal Bag 4000  
Kingston, ON K7L 5A3

Phone: (613) 540-5130  
1-800-267-0295 (toll free)  
Fax: (613) 540-5106  
E-mail: dave.lindensmith@ontario.ca

If you have any accessibility requirements in order to participate in this project please feel free to contact the undersigned.

Under the *Freedom of Information and Protection of Privacy Act (FOIPPA)* comments and information regarding this project, with the exception of personal information, will become part of the public record.

Des renseignements sont disponibles en français au 613 736-7200, poste 3561, auprès de Meghan MacMillan, (courriel: macmillanm@mmm.ca).

Yours truly,

**MMM Group Limited**

Lincoln MacDonald, P.Eng.  
Consultant Project Manager

cc: Mr. David Lindensmith, P.Eng., Senior Project Engineer – MTO Eastern Region  
Mr. Kevin Ogilvie, Senior Environmental Planner – MTO Eastern Region  
Mr. Tim Dickinson, Consultant Environmental Planner – MMM Group Ltd



**HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN  
AND THE RIDEAU CANAL, DETAIL DESIGN (WP 4088-07-01)**

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**Design and Construction Report**

**APPENDIX B**

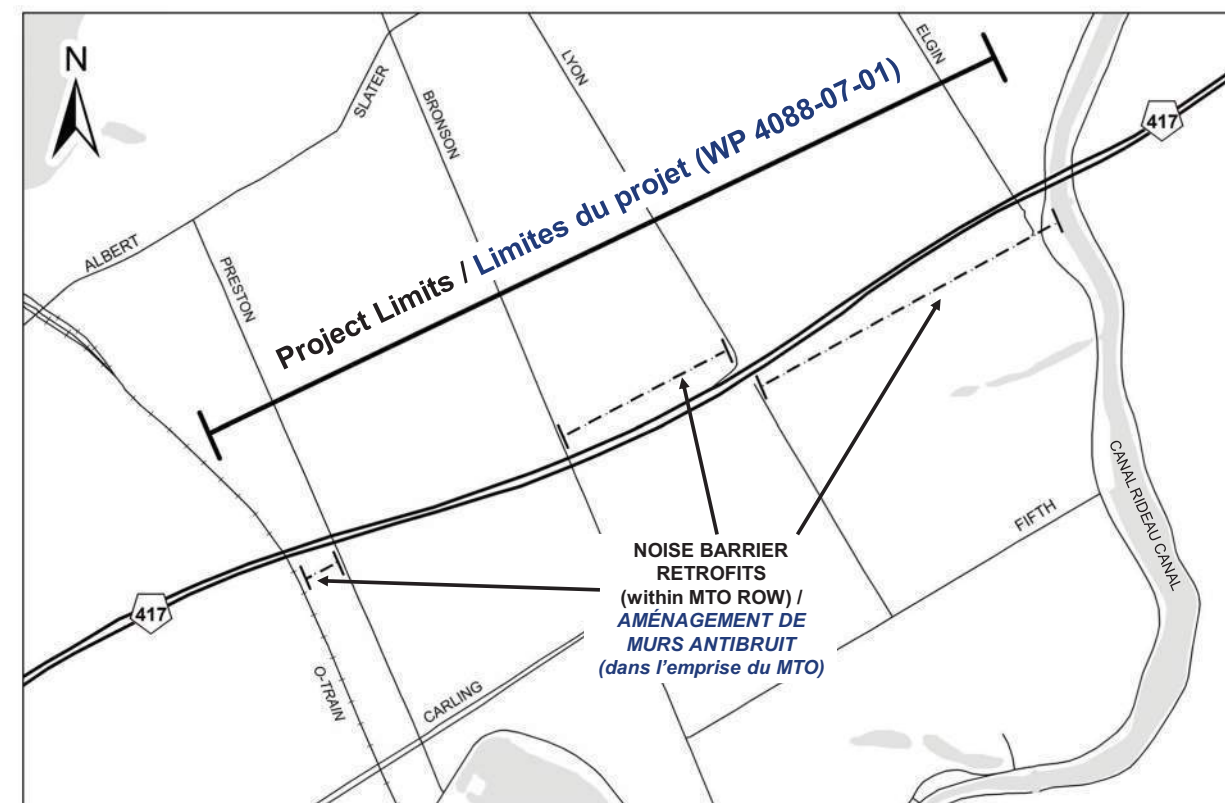
**Public Information Centre Display Materials**



Welcome to the Public Information Centre (PIC) for the Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal (WP 4088-07-01).

At this PIC you will have the opportunity to review and comment on:

- The overall study process;
- Plans for the proposed Highway 417 Noise Barrier Retrofits between the O-Train tracks and the Rideau Canal;
- Construction staging and implementation; and
- Anticipated impacts and proposed mitigation.



Representatives from the Ministry of Transportation (MTO) and MMM Group (MTO's Consultant) are available to discuss the project with you. Please ask questions and share your opinions with us.

We encourage you to complete a comment sheet.

**Please sign in.**

Bienvenue à la séance d'information publique (SIP) sur le projet de conception détaillée concernant l'aménagement de plusieurs murs antibruit le long de l'autoroute 417 entre la voie de l'O Train et le canal Rideau (WP 4088-07-01).

Ici, vous aurez l'occasion d'examiner et de commenter :

- le processus global d'étude;
- plans pour la réfection proposée des murs antibruit de l'autoroute 417 entre les voies de l'O-Train et le canal Rideau;
- les étapes de construction et la mise en oeuvre; ainsi que
- les répercussions attendues et les mesures d'atténuation proposées.

Des représentants du ministère des Transports de l'Ontario (MTO) et de MMM Group (l'expert-conseil retenu par le MTO) sont sur place pour discuter le projet avec vous. N'hésitez pas à poser des questions et à nous donner votre opinion.

Nous vous encourageons à remplir une feuille de commentaires distincte pour chaque projet.

**Veuillez vous inscrire.**





## Detail Design Study Process

This study is following the approved environmental planning process for Group 'B' undertakings under the requirements of the MTO's *Class Environmental Assessment (EA) for Provincial Transportation Facilities* (2000).

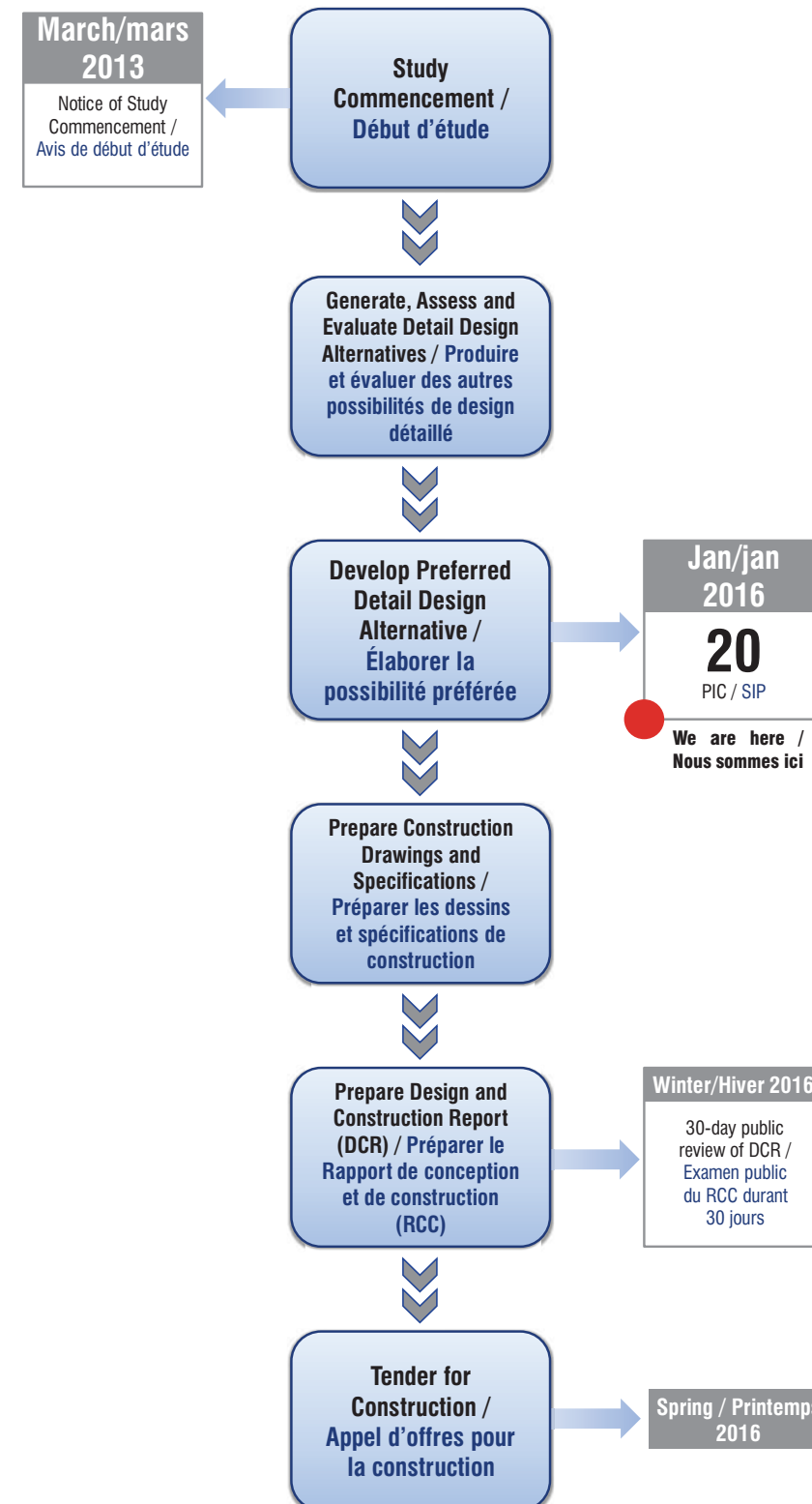
The EA process to be followed for detail design includes on-going opportunities for public involvement.

A Design and Construction Report (DCR) will be prepared for the study and will include:

- A description of the recommended plan, including any changes to transportation engineering and environmental issues identified in Preliminary Design;
- A summary of stakeholder consultation; and
- A detailed description of anticipated environmental effects and recommended mitigation measures that will be incorporated into construction documents.

The DCR will be made available for a 30-day public review period. Newspaper notices will be published at that time to explain the review process and identify the locations where the DCR is available for viewing.

Upon completion of the public review period, this study will be considered to have met the requirements of MTO's Class EA. Under the Class EA, there are no 'bump-up' (Part II Order) opportunities during the DCR review.



## Processus d'étude de la conception détaillée

Cette étude conforme aux exigences du processus de planification environnementale approuvé pour les projets du groupe « B » en vertu de l'*Évaluation environnementale (EE) de portée générale pour les installations provinciales de transport* (2000) du MTO.

Le processus d'EE applicable à la conception détaillée pour cette étude offre des occasions permanentes de participation du public.

Un rapport de conception et de construction (RCC) sera préparé et comprendra :

- une description du plan recommandé, y compris tout changement aux problèmes d'ingénierie des transports et environnementaux exposés au cours de la conception préliminaire;
- un résumé de la consultation des intervenants; et
- une description détaillée des effets environnementaux prévus et des mesures d'atténuation à intégrer aux documents de construction.

Le RCC sera mis à la disposition du public durant 30 jours. Des avis paraîtront alors dans les journaux pour expliquer le processus d'examen et indiquer les emplacements de ce rapport.

Dès la fin de la période d'examen public cette étude sera considéré comme ayant satisfait aux exigences de l'EE de portée générale du MTO, laquelle ne prévoit aucune possibilité de reclassement en prenant un arrêt en vertu de la partie II pendant l'examen du RCC.



## Background: Preliminary Design

Between 2002 and 2008, MTO completed a Preliminary Design and Environmental Assessment (EA) study to review infrastructure and operational issues on Highway 417 from Highway 416 to Anderson Road. The study examined existing and future problems and opportunities and developed a Recommended Plan to guide the future evolution of the Queensway. The Recommended Plan was documented in a Transportation Environmental Study Report (TESR), which received environmental clearance in 2008.

### THE NEED FOR IMPROVEMENTS

The Recommended Plan seeks to:

- Improve the mobility of people and goods by addressing insufficient capacity for travel demands in the study corridor;
- Reduce congestion and associated delay;
- Enhance safety and upgrade existing highway infrastructure to current design standards;
- Improve existing highway infrastructure, significant portions of which are approaching the end of expected service life.

## MTO Candidate Sites for Noise Barrier Retrofits List

In 2005, the MTO also evaluated areas along the Queensway to determine where the construction of retrofit noise barriers is warranted, including: the areas from Bronson Avenue to the Rideau Canal on the north side of the highway; and from Glendale Avenue to the Rideau Canal and in the vicinity of Preston Street on the south side of the highway.

The evaluations are documented in the TESR and concluded that the following locations should be added to the MTO Candidate Sites for Noise Barrier Retrofits List:

- On the north side of Highway 417 from Bronson Avenue to Lyon Street;
- On the south side of Highway 417 from east of Preston Street westerly to the existing Parkdale Avenue noise barrier; and
- A series of barriers on the south side of Highway 417 from Elgin Street westerly towards Glendale Avenue.

## Context Sensitive Design

A study was completed in 2011 to develop Context Sensitive Design (CSD) recommendations for the Ottawa Queensway corridor in order to ensure that the overall planned expansion and rehabilitation of Highway 417 through Ottawa reflects a holistically-planned aesthetic and vision. Proposed CSD recommendations being implemented as part of this Detail Design project include concrete noise barriers in keeping with the same hues and tones used in the construction of the new noise barrier at Lees Avenue, as well as the use of translucent/clear acrylic noise barrier panels across overpass structures to encourage a visual connection to the municipal streets below (see Recommended Plan board for noise barrier treatments).

## Scope of Work: Detail Design

The Detail Design project involves the following components:

- New ground-mounted and structure-mounted noise barriers in the following locations:
  - from the O-Train to Preston Street (south side of Highway 417);
  - from Bronson Avenue to Lyon Street (north side of Highway 417); and
  - from Lyon Street to the Rideau Canal (south side of Highway 417 and along ramps).
- Construction of related works, including modifications to existing structural barrier walls, roadside protection, and illumination modifications, as required.
- Traffic management during construction will be coordinated with the City of Ottawa. It is anticipated that staged lane reductions/closures, ramp closures and night work will be required to accommodate installation of noise barriers.



## Contexte : conception préliminaire

De 2002 à 2008, le MTO a procédé à une évaluation environnementale (EE) et de conception préliminaire pour examiner les questions opérationnelles et d'infrastructure sur l'autoroute 417 de l'autoroute 416 au chemin Anderson. L'étude a permis d'examiner les problèmes actuels et anticipés, ainsi que les possibilités qui s'offraient et a préparé un plan recommandé pour l'évolution du Queensway. Le plan recommandé a été documenté dans un rapport d'étude environnementale sur les transports (REET) qui a reçu l'autorisation officielle en 2008.

### LA NÉCESSITÉ D'AMÉLIORATIONS

Le plan recommandé vise :

- à améliorer la mobilité des gens et des biens en augmentant la capacité de déplacement insuffisante du corridor à l'étude;
- à réduire la congestion et les retards connexes;
- à améliorer la sécurité et l'actuelle infrastructure de l'autoroute en fonction des normes de conception courantes;
- à améliorer l'infrastructure actuelle de l'autoroute, dont plusieurs tronçons approchent de la fin de leur vie utile.

## Sites qui, selon le MTO, doivent figurer sur la liste de réfection des murs antibruit

En 2005, le MTO a aussi évalué des zones situées le long du Queensway pour déterminer si la réfection de murs antibruit est justifiée, y compris les secteurs allant de la rue Bronson au canal Rideau du côté nord de l'autoroute; ainsi que de l'avenue Glendale au canal Rideau et aux abords de la rue Preston du côté sud de l'autoroute.

Les évaluations sont documentées dans le REET et ont conclu que les emplacements suivants devraient être ajoutés à la liste du MTO des lieux susceptibles de faire l'objet d'une réfection des murs antibruit :

- côté nord de l'autoroute 417, de l'avenue Bronson à la rue Lyon;
- côté sud de l'autoroute 417, à partir de l'est de la rue Preston en direction ouest jusqu'à l'actuel mur antibruit de l'avenue Parkdale; et
- une série de murs du côté sud de l'autoroute 417, de la rue Elgin en direction ouest vers l'avenue Glendale.

## Conception tenant compte du contexte

Une étude a été effectuée en 2011 pour établir des recommandations sur la conception tenant compte du contexte (CTCC) relative au corridor du Queensway d'Ottawa, afin que l'expansion et la réfection globales de l'autoroute 417 à travers Ottawa reflètent une esthétique et une vision holistiques. Au nombre des recommandations proposées et mises en oeuvre dans le cadre de ce projet de conception détaillée, figurent des murs antibruit en béton ayant les mêmes teintes et couleurs que le nouveau mur antibruit de l'avenue Lees, ainsi que l'usage de panneaux antibruit en acrylique translucides ou transparents pour les structures de passages supérieurs, afin d'encourager un lien visuel avec les rues municipales en dessous (voir le panneau sur le plan recommandé pour les murs antibruit).

## Envergure des travaux : conception détaillée

Ce projet comprend les éléments suivants :

- Des nouveaux murs antibruit montés sur le sol et sur des structures aux emplacements suivants :
  - de l'O-Train jusqu'à la rue Preston (côté sud de l'autoroute 417);
  - de l'avenue Bronson jusqu'à la rue Kent (côté nord de l'autoroute 417); et
  - de la rue Lyon jusqu'au canal Rideau (côté sud de l'autoroute 417);
- Construction des ouvrages connexes, y compris les modifications aux actuels murs montés sur des structures, la protection des bords de route et les modifications de l'éclairage, au besoin.
- Gestion de la circulation pendant la construction sera coordonnée avec la Ville d'Ottawa. Il est prévu que mis en scène voies réductions / fermetures, fermetures de les bretelles, et le travail de nuit sera nécessaire pour permettre l'installation de murs antibruit.





## Other Upcoming Projects on Highway 417

## Autres projets à venir sur l'autoroute 417



### 2016 Contracts:

- Highway 417 Resurfacing from Island Park Drive to Nicholas Street (to ensure pavement is in good condition in advance of upcoming structural work) and replacement of overhead signs
- Highway 417 Rehabilitation and Widening from west of Maitland Avenue to East of Island Park Drive

### Upcoming Projects:

- Highway 417 Bridge Replacements: O-Train, Bronson Avenue, Percy Street, Preston Street, Booth Street, Rochester Street
- Replacement of Existing Noise Walls:
  - North side of Highway 417 from Island Park Drive to east of Parkdale Avenue (approximately 1.9 km);
  - South side of Highway 417, from west of Island Park Drive to west of the O-Train overpass (approximately 2.3 km); and
  - North side of Highway 417 from the midpoint of the Rochester Street ENS-W ramp to the NSE-W Bronson ramp and Bronson Avenue (approximately 0.5 km)

### Contrats pour 2016 :

- Resurfacement de l'autoroute 417 à partir de la promenade Island Park jusqu'à la rue Nicholas (pour s'assurer que l'asphalte est en bon état avant les travaux structuraux) et remplacement des panneaux de signalisation suspendus
- Réfection et élargissement de l'autoroute 417, à partir de l'ouest de l'avenue Maitland jusqu'à l'est de la promenade Island Park

### Contrats futurs :

- Remplacement des ponts sur l'autoroute 417 : O-Train, avenue Bronson, rue Percy, rue Preston, rue Booth, rue Rochester
- Remplacement des actuels murs antibruit;
  - côté nord de l'autoroute 417, de la promenade Island Park en direction est jusqu'à l'avenue Parkdale (environ 1,9 km);
  - côté sud de l'autoroute 417, de l'ouest de la promenade Island Park jusqu'à l'ouest du passage supérieur de l'O-Train (environ 2,3 km); et
  - -côté nord de l'autoroute 417 à partir du milieu de la bretelle ENS vers l'O de la rue Rochester jusqu'à la bretelle NSE vers l'O de Bronson et à l'avenue Bronson (environ 0,5 km)



Site Context

Contexte des lieux



EXISTING ENVIRONMENTAL CONDITIONS

- Vegetation:** The landscaped and cultural vegetation along the highway within the project limits is typical of urban lands and is considered to be of low ecological significance. No designated natural areas exist within the project limits.
- Wildlife:** No areas of significant habitat and no species at risk or provincially rare species were observed within the study area.
- Traffic:** In the eastbound direction within the study area, there are four lanes from Carling Avenue to Kent Street, three lanes from Kent Street to Metcalfe Street, and four lanes from Metcalfe Street to Nicholas Street.
- In the westbound direction within the study area, there are three lanes from Metcalfe Street to O'Connor Street, where an auxiliary lane joins the mainline to provide four lanes to Carling Avenue.
- In 2010, Annual Average Daily Traffic totalled 174,800 vehicles between Parkdale Avenue and Rochester Street; 162,500 between Rochester Street and Bronson Avenue; 158,900 between Bronson Avenue and Kent Street; 162,800 between Kent Street and O'Connor Street; 161,900 between O'Connor Street and Metcalfe Street; and 158,800 between Metcalfe Street and Nicholas Street.
- There are nine signalized ramp intersections within the study area.
- Noise:** Existing noise levels affecting noise sensitive areas located adjacent to the highway are as follows, as determined through a noise assessment conducted in 2014:
  - Between 50 and 66 dBA at noise sensitive receptors on the south side of Highway 417 between the O-Train tracks and Preston Street;
  - Between 50 and 66 dBA at noise sensitive receptors on the north side of Highway 417 between Bronson Avenue and Lyon Street; and
  - Between 50 and 71 dBA at noise sensitive receptors on the south side of Highway 417 between Lyon Street and the Rideau Canal.
- Archaeology:** No evidence of archaeological resources was found during preliminary design within the existing Highway 417 right-of-way, which has been significantly disturbed.



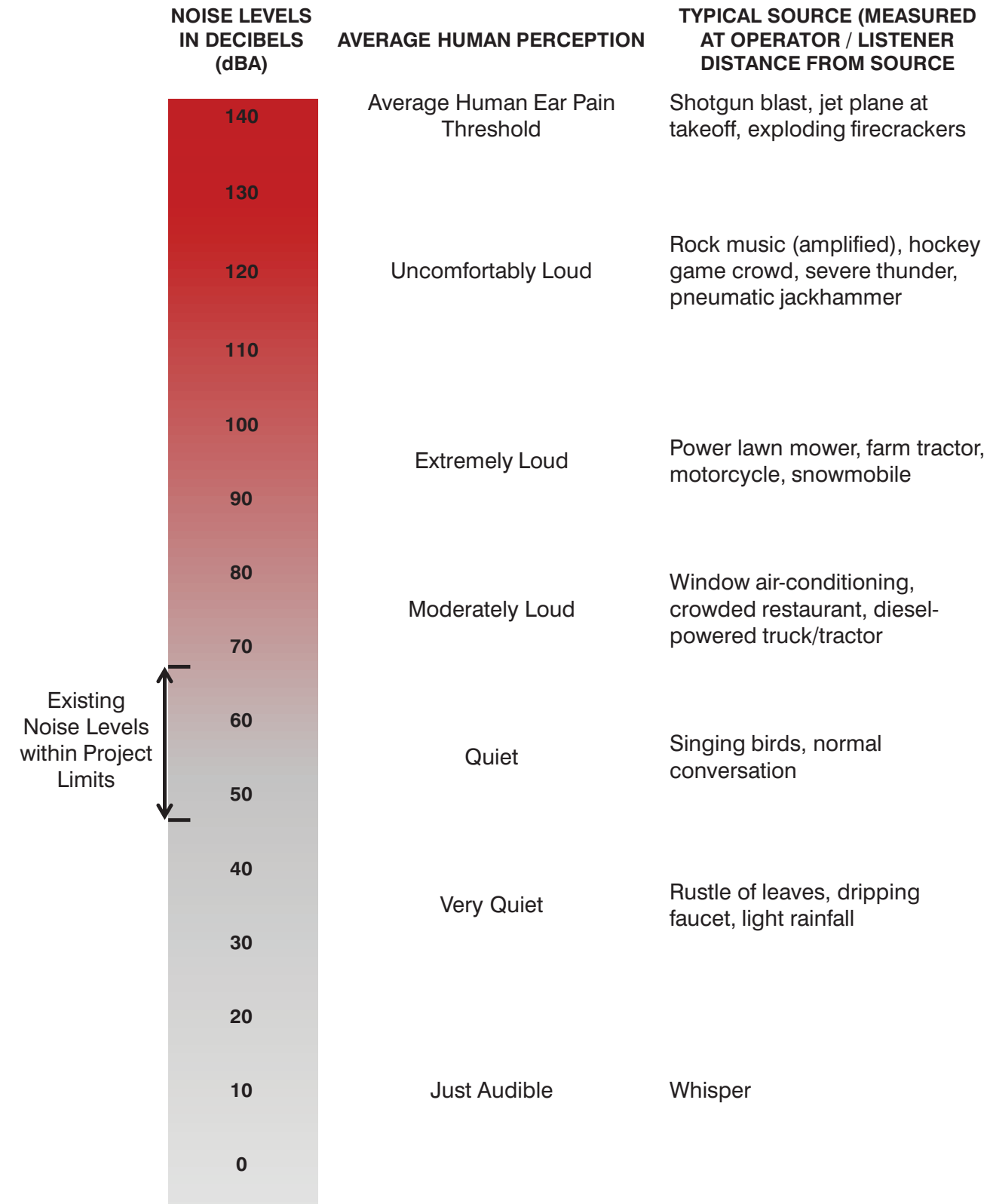
CONDITIONS ENVIRONNEMENTALES ACTUELLES

- Végétation :** La végétation aménagée et culturelle le long de l'autoroute, dans les limites du projet et des zones de rassemblement, est typique des terres urbaines et considérée comme ayant une faible importance écologique. Aucune aire naturelle désignée n'existe dans les limites du projet.
- Faune :** Aucune aire d'habitat important, aucune espèce en péril ni aucune espèce rare dans la province n'a été observée dans le secteur à l'étude.
- Circulation :** En direction est dans le secteur à l'étude, il y a quatre voies de l'avenue Carling à la rue Kent, trois voies de la rue Kent à la rue Metcalfe, et quatre voies de la rue Metcalfe à la rue Nicholas.
- En direction ouest dans le secteur à l'étude, il y a trois voies de la rue Metcalfe à la rue O'Connor, où une voie auxiliaire se joint à la voie principale pour former quatre voies jusqu'à l'avenue Carling.
- En 2010, le débit journalier moyen annuel totalisait 174 800 véhicules entre l'avenue Parkdale et la rue Rochester; 162 500 entre la rue Rochester et l'avenue Bronson; 158 900 entre l'avenue Bronson et la rue Kent; 162 800 entre les rues Kent et O'Connor; 161 900 entre les rues O'Connor et Metcalfe; et 158 800 entre les rues Metcalfe et Nicholas.
- Il y a neuf intersections de bretelles signalées dans le secteur à l'étude.
- Bruit :** Les niveaux actuels de bruit touchant les zones sensibles au bruit adjacentes à l'autoroute sont les suivants, selon une évaluation du bruit effectuée en 2014 :
  - de 50 à 66 dBA aux récepteurs sensibles au bruit, du côté sud de l'autoroute 417, entre les voies de l'O-Train et la rue Preston;
  - de 50 à 66 dBA aux récepteurs sensibles au bruit, du côté nord de l'autoroute 417, entre l'avenue Bronson et la rue Lyon; et
  - de 50 à 71 dBA aux récepteurs sensibles au bruit, du côté sud de l'autoroute 417, entre la rue Lyon et le canal Rideau.
- Archéologie :** Aucune preuve de ressource archéologique n'a été trouvée pendant la conception préliminaire, dans l'emprise actuelle de l'autoroute 417, qui a été gravement perturbée.



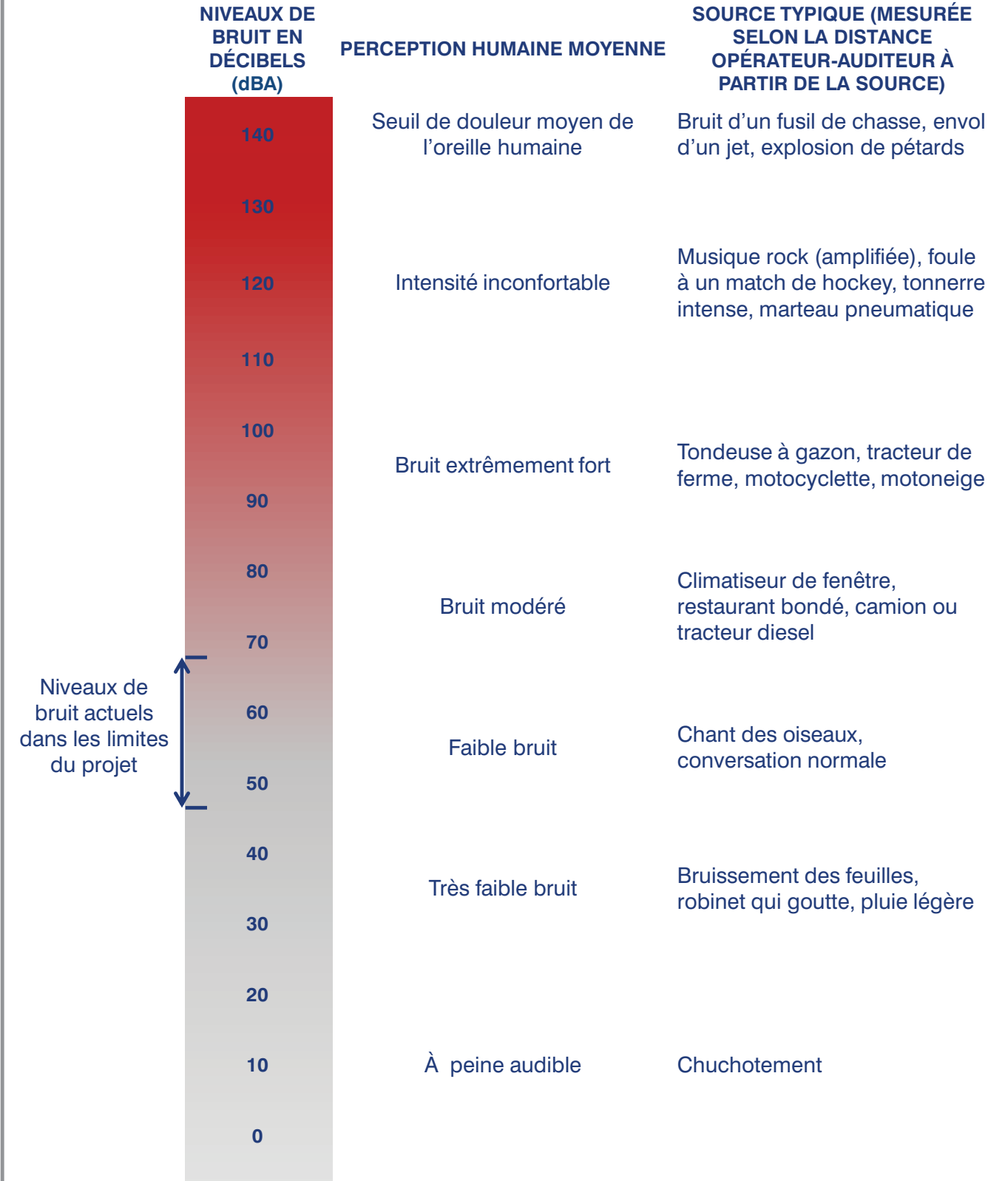


# Noise Levels of Familiar Sounds



Adapted from: "Noise Hazard and Control" Health and Welfare Canada Report 79-END29 March 1979

# Les niveaux de bruit de sons familiers



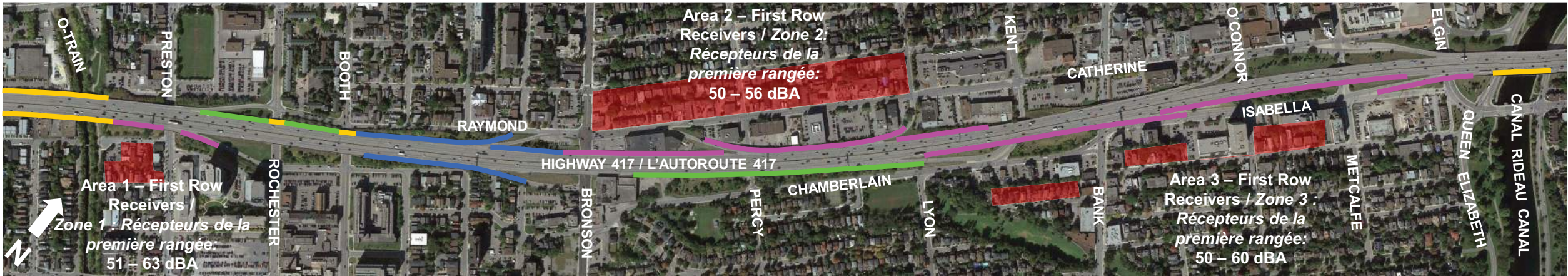
Adapté de : « Noise Hazard and Control », Santé et Bien-être social Canada, rapport 79-END, 29 mars 1979





Predicted Noise Level Reductions –  
First Row of Residential Receivers

Réductions prévues du niveau de bruit, première  
rangée de récepteurs résidentiels



Proposed Noise Barrier Retrofits / Réfections proposées de murs antibruit  
Existing Metal Noise Barriers / Actuels murs antibruit métalliques  
Existing Concrete Noise Barriers / Actuels murs antibruit en béton  
Existing Metal Snow Guard / Actuel garde-neige métallique  
General Location of Noise Sensitive Receptors for First Row of Receivers / Emplacements généraux des récepteurs sensibles au bruit de la première rangée

A Noise Assessment was conducted in 2014 to determine existing noise levels affecting Noise Sensitive Areas (i.e. residences with Outdoor Living Areas) located adjacent to the highway within the project limits, and to determine predicted noise levels after the installation of a new 5 m noise barrier and accounting for projected future traffic data to the year 2021.

Levels of noise reduction are measured for the first row of residential receivers in the three areas where noise barrier retrofits are proposed.

The effectiveness of noise barriers is reduced for residences that are located several blocks from the highway.

The average threshold of perception of a change in noise is 3 dBA.

NOISE WALL LOCATION	NOISE SENSITIVE RECEPTORS	NOISE LEVELS FOR 1 <sup>ST</sup> ROW OF RESIDENTIAL RECEPTORS		
		EXISTING	WITH NEW 5 M NOISE BARRIER	OVERALL AVERAGE REDUCTION*
Area 1: South side of Highway 417 between the O-Train tracks and Preston Street	<ul style="list-style-type: none"><li>Noise sensitive receptors representing first row of residential receivers: 5</li><li>Number of residences in first row of residential receivers: 14</li></ul>	55 – 66 dBA	51 – 63 dBA	7 dBA
Area 2: North side of Highway 417 between Bronson Avenue and Lyon Street	<ul style="list-style-type: none"><li>Noise sensitive receptors representing first row of residential receivers: 17</li><li>Number of residences in first row of residential receivers: 83</li></ul>	50 – 66 dBA	50 – 56 dBA	5 dBA
Area 3: South side of Highway 417 between Lyon Street and the Rideau Canal	<ul style="list-style-type: none"><li>Noise sensitive receptors representing first row of residential receivers: 11</li><li>Number of residences in first row of residential receivers: 57</li></ul>	50 – 71 dBA	50 – 60 dBA	6 dBA

\*Note: The overall average reduction for the first row of receivers includes weighting of the number of Noise Sensitive Areas represented.

Une évaluation du bruit a été menée en 2014 pour déterminer les niveaux de bruit actuels touchant les zones sensibles au bruit (c.-à-d. les résidences avec des aires d'habitation extérieures) adjacentes à l'autoroute, dans les limites du projet, et pour prédire les niveaux de bruit après l'installation d'un nouveau mur antibruit de 5 m, compte tenu des projections sur la circulation future jusqu'en 2021.

Les niveaux de réduction du bruit sont mesurés pour la première rangée de récepteurs résidentiels dans les trois zones pour lesquelles on propose des réfections aux murs antibruit.

L'efficacité des murs antibruit est réduite pour les résidences situées à plusieurs coins de rue de l'autoroute.

Le seuil moyen de perception d'un changement dans le bruit est de 3 dBA.

EMPLACEMENT DU MUR ANTIBRUIT	RÉCEPTEURS SENSIBLES AUX BRUITS	NIVEAUX DE BRUIT POUR LA PREMIÈRE RANGÉE DE RÉCEPTEURS RÉSIDENIELS		
		ACTUELS	AVEC LE NOUVEAU MUR ANTIBRUIT DE 5 M	RÉDUCTION GLOBALE MOYENNE*
Zone 1: de l'O-Train jusqu'à la rue Preston (côté sud de l'autoroute 417)	<ul style="list-style-type: none"><li>Récepteurs sensibles au bruit représentant la première rangée de récepteurs résidentiels : 5</li><li>Nombre de résidences de la première rangée de récepteurs résidentiels : 14</li></ul>	55 – 66 dBA	51 – 63 dBA	7 dBA
Zone 2: de l'avenue Bronson jusqu'à la rue Kent (côté nord de l'autoroute 417)	<ul style="list-style-type: none"><li>Récepteurs sensibles au bruit représentant la première rangée de récepteurs résidentiels : 17</li><li>Nombre de résidences de la première rangée de récepteurs résidentiels : 83</li></ul>	50 – 66 dBA	50 – 56 dBA	5 dBA
Zone 3: de la rue Lyon jusqu'au canal Rideau (côté sud de l'autoroute 417);	<ul style="list-style-type: none"><li>Récepteurs sensibles au bruit représentant la première rangée de récepteurs résidentiels : 11</li><li>Nombre de résidences de la première rangée de récepteurs résidentiels : 57</li></ul>	50 – 71 dBA	50 – 60 dBA	6 dBA

\*Note: La réduction globale moyenne pour la première rangée de récepteurs comprend la pondération du nombre de zones sensibles au bruit représentées.



HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU CANAL

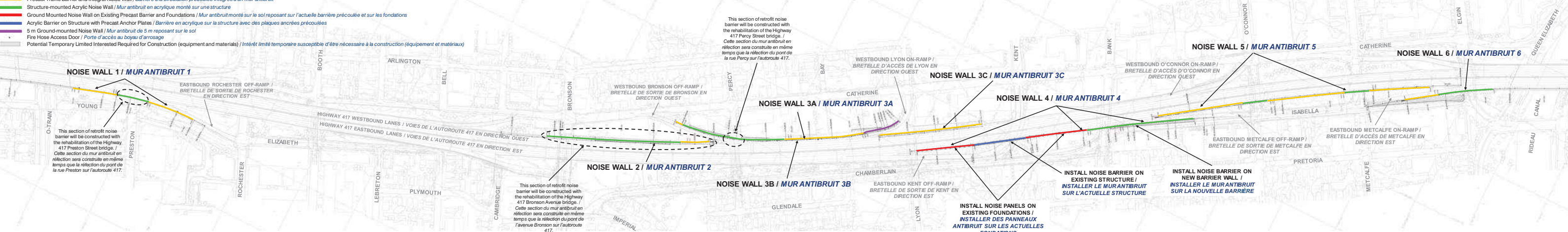
AMÉNAGEMENT DE MURS ANTIBRUIT SUR L'AUTOROUTE 417 ENTRE L'O-TRAIN ET LE CANAL RIDEAU

LEGEND / LÉGENDE

- Precast Traffic Barrier and Integral Noise Wall / Barrière à la circulation précoulée intégrée à un mur antibruit
- Structure-mounted Acrylic Noise Wall / Mur antibruit en acrylique monté sur une structure
- Ground Mounted Noise Wall on Existing Precast Barrier and Foundations / Mur antibruit monté sur le sol reposant sur l'actuelle barrière précoulée et sur les fondations
- Acrylic Barrier on Structure with Precast Anchor Plates / Barrière en acrylique sur la structure avec des plaques ancrées précoulées
- 5 m Ground-mounted Noise Wall / Mur antibruit de 5 m reposant sur le sol
- Fire Hose Access Door / Porte d'accès au boyau d'arrosage
- Potential Temporary Limited Interested Required for Construction (equipment and materials) / Intérêt limité temporaire susceptible d'être nécessaire à la construction (équipement et matériaux)

Recommended Plan

Plan recommandé



NOISE WALL 1 / MUR ANTIBRUIT 1

NOISE WALL 2 / MUR ANTIBRUIT 2

NOISE WALLS 3A/3B / MURS ANTIBRUIT 3A/3B

NOISE WALL 3C / MUR ANTIBRUIT 3C

NOISE WALL 4 / MUR ANTIBRUIT 4

NOISE WALL 5 / MUR ANTIBRUIT 5

NOISE WALL 6 / MUR ANTIBRUIT 6

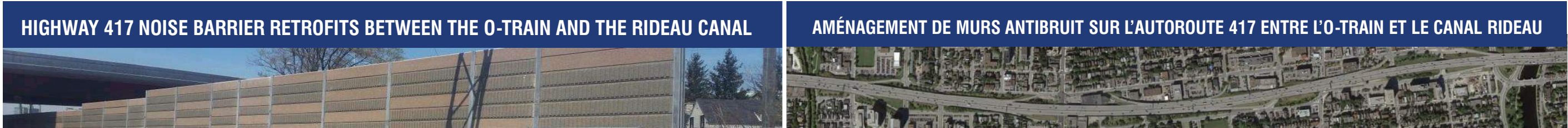


Ground-mounted concrete noise barriers  
Highway side noise barrier treatment (left) and back side treatment (right),  
as installed at Lees Avenue /  
Murs antibruit en béton montés sur le sol  
Traitement du mur antibruit du côté de l'autoroute (à gauche) et vu de  
l'arrière (à droite), installé à l'avenue Lees



Structure-mounted clear acrylic noise barriers /  
Murs antibruit en acrylique transparents montés sur la structure





## Construction and Traffic Staging

### NOISE WALL 1

NOISE WALL 1 STAGING			
WORK DESCRIPTION	TRAFFIC IMPACTS		
	TIME	HIGHWAY	RAMPS
<ul style="list-style-type: none"><li>Full depth removal of pavement on asphalt shoulder</li><li>Remove existing concrete barrier</li><li>Install new integral barrier and noise wall</li></ul>	Day	• N/A	• N/A
	Night	<ul style="list-style-type: none"><li>1 lane closed Eastbound</li><li>Duration: 8 pm to 6 am for 4 to 5 weeks</li></ul>	<ul style="list-style-type: none"><li>Eastbound Rochester off-ramp closed</li><li>Duration: 8 pm to 6 am for 4 to 5 weeks</li></ul>

### NOISE WALLS 2 / 3A

NOISE WALL 2/3A STAGING			
WORK DESCRIPTION	TRAFFIC IMPACTS		
	TIME	HIGHWAY	RAMPS
<ul style="list-style-type: none"><li>Remove asphalt shoulder and existing barrier for ground mounted barrier sections</li><li>Install new integral barrier and noise wall for ground mounted sections</li><li>Full depth removal of asphalt and roadway granulars for structure mounted portion of Noise Wall 2</li><li>Rehabilitate cantilever portion of Noise Wall 2</li><li>Remove and rehabilitate barrier wall of Noise Wall 2</li><li>Install acrylic noise barrier for Noise Wall 2</li><li>Install ground mounted noise barrier behind existing barrier wall and retaining wall of structural section of Noise Wall 3A</li></ul>	Day	<ul style="list-style-type: none"><li>Westbound reduced by 1 lane</li><li>Duration: 24 hour basis for 10 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>Westbound Lyon on-ramp closed</li><li>Westbound Bronson off-ramp open</li><li>Duration: 24 hour basis for 10 to 12 weeks</li></ul>
	Night	<ul style="list-style-type: none"><li>Westbound reduced by 1 lane</li><li>Duration: 24 hour basis for 10 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>Westbound Lyon on-ramp closed</li><li>Westbound Bronson off-ramp open</li><li>Duration: 24 hour basis for 10 to 12 weeks</li></ul>

### NOISE WALLS 3A / 3B

NOISE WALL 3A/3B STAGING			
WORK DESCRIPTION	TRAFFIC IMPACTS		
	TIME	HIGHWAY	RAMPS
<ul style="list-style-type: none"><li>Remove asphalt shoulder for ground mounted barrier sections</li><li>Install integral barrier and noise wall for ground mounted sections</li><li>Remove and rehabilitate barrier wall structural portion of Noise Wall 3B</li><li>Install acrylic noise barrier for Noise Wall 3B</li></ul>	Day	• N/A	<ul style="list-style-type: none"><li>Westbound Bronson off-ramp closed – Signed detour in place directing traffic to the Westbound Metcalfe off-ramp with Airport signage</li><li>Westbound Lyon on-ramp closed</li><li>Duration: 24 hour basis for 6 to 8 weeks</li></ul>
	Night	• N/A	<ul style="list-style-type: none"><li>Westbound Bronson off-ramp closed – Signed detour in place directing traffic to the Westbound Metcalfe off-ramp with Airport signage</li><li>Westbound Lyon on-ramp closed</li><li>Duration: 24 hour basis for 6 to 8 weeks</li></ul>

### NOISE WALL 3C

NOISE WALL 3C STAGING			
WORK DESCRIPTION	TRAFFIC IMPACTS		
	TIME	HIGHWAY	RAMPS
<ul style="list-style-type: none"><li>Remove asphalt shoulder and existing barrier</li><li>Install new integral barrier and noise wall</li></ul>	Day	• N/A	• N/A
	Night	<ul style="list-style-type: none"><li>Westbound reduced by 1 lane</li><li>Duration: 8 pm to 6 am for 3 to 4 weeks</li></ul>	<ul style="list-style-type: none"><li>Westbound O'Connor on-ramp / auxiliary lane closed</li><li>Duration: 8 pm to 6 am for 3 to 4 weeks</li></ul>

### NOISE WALL 4

NOISE WALL 4 STAGING			
WORK DESCRIPTION	TRAFFIC IMPACTS		
	TIME	HIGHWAY	RAMPS
<ul style="list-style-type: none"><li>Install noise wall panels from STA. 28+384.7 to STA. 28+482.6 and from STA. 28+572.2 to STA. 28+670</li><li>Remove and rehabilitate barrier walls on all structures and retaining walls</li><li>Erect noise barrier panels</li><li>Install precast barrier and integral noise wall and footings at all ground mounted locations</li><li>Install acrylic noise panels on Kent St. structure and retaining walls</li></ul>	Day	• All lanes open Eastbound	<ul style="list-style-type: none"><li>1 lane must exit and 1 either/or lane open at Eastbound Kent off-ramp</li><li>Eastbound Metcalfe off-ramp open</li></ul>
	Night	<ul style="list-style-type: none"><li>1 lane open Eastbound to STA. 28+600</li><li>Duration: 8 pm to 6 am for 2 to 3 weeks</li><li>2 lanes open Eastbound from STA. 28+600 easterly</li><li>Duration: 8 pm to 6 am for 8 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>2 lanes forced off at Eastbound Kent off-ramp</li><li>Eastbound Metcalfe off-ramp closed</li><li>Duration: 8 pm to 6 am for 2 to 3 weeks</li><li>Westbound Kent on-ramp closed for erection of panels across ramps</li><li>Duration: 8 pm to 6 am for 1 week</li></ul>

### AMÉNAGEMENT DE MURS ANTIBRUIT SUR L'AUTOROUTE 417 ENTRE L'O-TRAIN ET LE CANAL RIDEAU



### NOISE WALLS 4/5

NOISE WALL 4/5 STAGING				
WORK DESCRIPTION	TRAFFIC IMPACTS			
	TIME	HIGHWAY	RAMPS	CITY STREETS
<ul style="list-style-type: none"><li>Install noise wall panels from STA. 28+625 to STA. 28+678.8</li><li>Repair concrete fascia on retaining walls</li><li>Place anchors and concrete for noise barrier support posts on retaining walls</li><li>Erect noise barrier panels</li><li>Install precast barrier and integral noise wall and footings at all ground mounted locations</li></ul>	Day	• All lanes open Eastbound	<ul style="list-style-type: none"><li>1 lane must exit and 1 either/or lane open at Eastbound Kent off-ramp</li><li>Eastbound Metcalfe off-ramp open</li></ul>	<ul style="list-style-type: none"><li>2 lanes on Isabella with 3.5 m lane widths</li><li>Duration: 24 hour basis for 8 to 12 weeks</li></ul>
	Night	<ul style="list-style-type: none"><li>2 lanes open Eastbound</li><li>Duration: 8 to 12 weeks</li><li>9 pm to 6 am: single lane reduction</li><li>10 pm to 6 am: two lane reduction</li></ul>	<ul style="list-style-type: none"><li>2 lanes forced off at Eastbound Kent off-ramp</li><li>Eastbound Metcalfe off-ramp closed</li><li>Duration: 9 pm to 6 am for 8 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>Isabella reduced to 1 lane</li><li>Duration: 8 pm to 6 am for 8 to 12 weeks</li></ul>

### NOISE WALL 5

NOISE WALL 5 STAGING				
WORK DESCRIPTION	TRAFFIC IMPACTS			
	TIME	HIGHWAY	RAMPS	CITY STREETS
<ul style="list-style-type: none"><li>Install noise wall panels from STA. 50+003 to STA. 50+472</li><li>Repair concrete fascia on structures and retaining walls</li><li>Place anchors and concrete for noise barrier support posts on structures and retaining walls</li><li>Erect noise barrier panels</li><li>Install precast barrier and integral noise wall and footings at all ground mounted locations</li></ul>	Day	• All lanes open Eastbound	<ul style="list-style-type: none"><li>1 lane must exit and 1 either/or lane open at Eastbound Kent off-ramp</li><li>Eastbound Metcalfe off-ramp open</li></ul>	<ul style="list-style-type: none"><li>2 lanes on Isabella with 3.5 m lane widths</li><li>Duration: 24 hour basis for 8 to 12 weeks</li></ul>
	Night	<ul style="list-style-type: none"><li>2 lanes open Eastbound</li><li>Duration: 8 pm to 6 am for 8 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>2 lanes forced off at Eastbound Kent off-ramp</li><li>Eastbound Metcalfe off-ramp closed</li><li>Duration: 8 pm to 6 am for 8 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>Isabella reduced to 1 lane</li><li>Metcalfe north and south of Isabella closed for barrier wall removal, post and panel installation</li><li>Duration: 8 pm to 6 am for 8 to 12 weeks</li></ul>

### NOISE WALL 6

NOISE WALL 6 STAGING				
WORK DESCRIPTION	TRAFFIC IMPACTS			
	TIME	HIGHWAY	RAMPS	CITY STREETS
<ul style="list-style-type: none"><li>Install precast barrier and integral noise wall and footings at ground mounted wall locations</li><li>Rehabilitate structural and retaining wall overhang fascia, install base plate parts and acrylic noise panels</li></ul>	Day	• All lanes open Eastbound	<ul style="list-style-type: none"><li>Eastbound Metcalfe on-ramp reduced to one lane</li><li>Duration: 24 hour basis for 10 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>All lanes on Isabella open</li><li>Lane 2 revised to left turn only</li><li>Lane 3 access to 417 Eastbound</li><li>Duration: 24 hour basis for 10 to 12 weeks; Police presence for traffic control during PM peak</li></ul>
	Night	• All lanes open Eastbound	<ul style="list-style-type: none"><li>Eastbound Metcalfe on-ramp reduced to one lane</li><li>Duration: 24 hour basis for 10 to 12 weeks</li></ul>	<ul style="list-style-type: none"><li>All lanes on Isabella open</li><li>Lane 2 revised to left turn only</li><li>Lane 3 access to 417 Eastbound</li><li>Duration: 24 hour basis for 10 to 12 weeks</li><li>Lane reduction on Elgin for overhead work</li><li>Duration: 9 pm to 7 am for 1 week</li></ul>

### SUMMARY OF RAMP CLOSURES

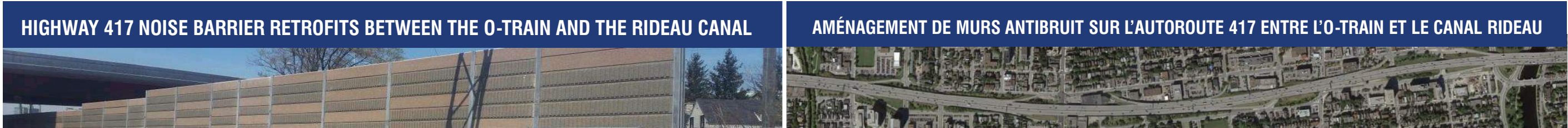
SUMMARY OF RAMP CLOSURES			
NOISE WALL	RAMP	CLOSURE TYPE	APPROXIMATE DURATION
1	• Eastbound Rochester off-ramp	• Nightly recurring from 8 pm to 6 am	• 4 to 5 weeks
2, 3A	• Westbound Lyon on-ramp	• Full-time closure (24 hour basis)	• 10 to 12 weeks
2, 3A / 3B	<ul style="list-style-type: none"><li>Westbound Bronson off-ramp</li><li>Westbound Lyon on-ramp</li></ul>	• Full-time closure (24 hour basis)	• 6 to 8 weeks
4	• Eastbound Kent off-ramp	• Full nighttime closure from 8 pm to 6 am (only for erection of overhead panels on structure)	• 1 week
3C	• Westbound O'Connor on-ramp	• Nightly recurring from 8 pm to 6 am	• 3 to 4 weeks
4 / 5	• Eastbound Metcalfe off-ramp	• Nightly recurring from 9 pm to 6 am	• 8 to 12 weeks
6	• Eastbound Metcalfe on-ramp	• Closure of 1 to 2 lanes (24 hour basis)	• 10 to 12 weeks

### SUMMARY OF PEDESTRIAN IMPACTS

PEDESTRIAN IMPACTS & MITIGATION			
NOISE WALL	LOCATION	IMPACTS & PROPOSED MITIGATION	APPROXIMATE DURATION
2	• Both sides of Bronson Avenue under Highway 417	• During the removal of the existing barrier and all overhead work, one of the sidewalks under the structure will remain open at all times.	• Nightly for 1 to 3 weeks
3B	• Both sides of Percy Street under Highway 417	• During the removal of the existing barrier and all overhead work, one of the sidewalks under the structure will remain open at all times.	• Nightly for 1 to 3 weeks
4	• East side of Kent Street under Highway 417	<ul style="list-style-type: none"><li>During the installation of the noise barrier panels over the West Chamberlain to Kent Street North lane, the east sidewalk will be closed.</li><li>Pedestrian traffic will be directed to Bank Street along the sidewalk on the south side of Chamberlain.</li></ul>	• Nightly for 1 week
4	• Both sides of Bank Street under Highway 417	• During the removal of the existing barrier and all overhead work, one of the sidewalks under the structure will remain open at all times.	• Nightly for 1 to 3 weeks
5	• Both sides of O'Connor Street under Highway 417	• During the removal of the existing barrier and all overhead work, one of the sidewalks under the structure will remain open at all times.	• Nightly for 1 to 3 weeks
5	• Both sides of Metcalfe Street under Highway 417	<ul style="list-style-type: none"><li>During the removal of the existing barrier and all overhead work, the west sidewalk will be closed.</li><li>Pedestrian traffic will be directed to O'Connor via the sidewalks on the south side of Catherine or Isabella.</li></ul>	• Nightly for 1 to 3 weeks
6	• Both sides of Elgin Street under Highway 417	<ul style="list-style-type: none"><li>During the removal of the existing barrier and all overhead work, at least one of the sidewalks will be maintained under the structure.</li><li>In the case of the closure of the west sidewalk, pedestrian traffic will be directed south along the extension of Elgin to Pretoria / Queen Elizabeth in order to cross to the east sidewalk under the structure.</li></ul>	• Nightly for 1 to 3 weeks

Note: Work may occur concurrently on individual noise wall locations and on Highway 417 Eastbound and Westbound.





## Construction et étapes de la circulation

### MUR ANTIBRUIT 1

MUR ANTIBRUIT 1 : ÉTAPES DE LA CONSTRUCTION			
DESCRIPTION DES TRAVAUX	RÉPERCUSSIONS SUR LA CIRCULATION		
	HEURE	AUTOROUTE	BRETELLES
<ul style="list-style-type: none"><li>Enlèvement sur toute la profondeur de l'asphalte de l'accotement</li><li>Enlèvement de l'actuelle barrière de béton</li><li>Installation d'une nouvelle barrière intégrée à un mur antibruit</li></ul>	Jour	<ul style="list-style-type: none"><li>S.O.</li></ul>	<ul style="list-style-type: none"><li>S.O.</li></ul>
	Nuit	<ul style="list-style-type: none"><li>Fermeture d'une voie en direction est</li><li>Durée : de 20 h à 6 h, durant 4 ou 5 semaines</li></ul>	<ul style="list-style-type: none"><li>Fermeture de la bretelle de sortie de Rochester en direction est</li><li>Durée : de 20 h à 6 h, durant 4 ou 5 semaines</li></ul>

### MURS ANTIBRUIT 2 / 3A

MURS ANTIBRUIT 2 / 3A : ÉTAPES DE LA CONSTRUCTION			
DESCRIPTION DES TRAVAUX	RÉPERCUSSIONS SUR LA CIRCULATION		
	HEURE	AUTOROUTE	BRETELLES
<ul style="list-style-type: none"><li>Enlèvement de l'accotement d'asphalte et de la barrière actuelle pour les sections montées sur le sol</li><li>Installation d'une nouvelle barrière intégrée à un mur antibruit pour les sections montées sur le sol</li><li>Enlèvement sur toute la profondeur des éléments granuleux de l'asphalte et de la chaussée pour la partie montée sur une structure du mur antibruit 2</li><li>Réfection de la partie en porte-à-faux du mur antibruit 2</li><li>Enlèvement et réfection de la barrière du mur antibruit 2</li><li>Installation d'une barrière en acrylique pour le mur antibruit 2</li><li>Installation d'un mur antibruit monté sur le sol derrière la barrière actuelle et le mur de soutènement de la section structurale du mur antibruit 3A</li></ul>	Jour	<ul style="list-style-type: none"><li>Direction ouest réduite d'une voie</li><li>Durée : 24 heures durant 10 à 12 semaines</li></ul>	<ul style="list-style-type: none"><li>Fermeture de la bretelle d'accès en direction ouest de Lyon</li><li>Ouverture de la bretelle de sortie de Bronson en direction ouest</li><li>Durée : 24 heures durant 10 à 12 semaines</li></ul>
	Nuit	<ul style="list-style-type: none"><li>Direction ouest réduite d'une voie</li><li>Durée : 24 heures durant 10 à 12 semaines</li></ul>	<ul style="list-style-type: none"><li>Fermeture de la bretelle d'accès en direction ouest de Lyon</li><li>Ouverture de la bretelle de sortie de Bronson en direction ouest</li><li>Durée : 24 heures durant 10 à 12 semaines</li></ul>

### MURS ANTIBRUIT 3A / 3B

MURS ANTIBRUIT 3A /3B : ÉTAPES DE LA CONSTRUCTION			
DESCRIPTION DES TRAVAUX	RÉPERCUSSIONS SUR LA CIRCULATION		
	HEURE	AUTOROUTE	BRETELLES
<ul style="list-style-type: none"><li>Enlèvement de l'accotement d'asphalte pour les sections de la barrière montées sur le sol</li><li>Installation d'une barrière intégrée à un mur antibruit pour les sections montées sur le sol</li><li>Enlèvement et réfection de la partie structurale de la barrière du mur antibruit 3B</li><li>Installation de la barrière antibruit en acrylique pour le mur antibruit 3B</li></ul>	Jour	<ul style="list-style-type: none"><li>S.O.</li></ul>	<ul style="list-style-type: none"><li>Fermeture de la bretelle de sortie de Bronson en direction ouest; détour avec panneaux de signalisation pour acheminer la circulation vers la bretelle de sortie de Metcalfe en direction ouest avec panneaux d'affichage vers l'aéroport</li><li>Fermeture de la bretelle d'accès de Lyon en direction ouest</li><li>Durée : 24 heures durant 6 à 8 semaines</li></ul>
	Nuit	<ul style="list-style-type: none"><li>S.O.</li></ul>	<ul style="list-style-type: none"><li>Fermeture de la bretelle de sortie de Bronson en direction ouest; détour avec panneaux de signalisation pour acheminer la circulation vers la bretelle de sortie de Metcalfe en direction ouest avec panneaux d'affichage vers l'aéroport</li><li>Fermeture de la bretelle d'accès de Lyon en direction ouest</li><li>Durée : 24 heures durant 6 à 8 semaines</li></ul>

### MUR ANTIBRUIT 3C

MUR ANTIBRUIT 3C : ÉTAPES DE LA CONSTRUCTION			
DESCRIPTION DES TRAVAUX	RÉPERCUSSIONS SUR LA CIRCULATION		
	HEURE	AUTOROUTE	BRETELLES
<ul style="list-style-type: none"><li>Enlèvement de l'accotement en asphalte et de la barrière actuelle</li><li>Installation d'une nouvelle barrière intégrée à un mur antibruit</li></ul>	Jour	<ul style="list-style-type: none"><li>S.O.</li></ul>	<ul style="list-style-type: none"><li>S.O.</li></ul>
	Nuit	<ul style="list-style-type: none"><li>Direction ouest réduite d'une voie</li><li>Durée : de 20 h à 6 h, durant trois ou quatre semaines</li></ul>	<ul style="list-style-type: none"><li>Fermeture de la bretelle d'accès d'O'Connor en direction ouest et de la voie auxiliaire</li><li>Durée : de 20 h à 6 h, durant 3 ou 4 semaines</li></ul>

### MUR ANTIBRUIT 4

MUR ANTIBRUIT 4 : ÉTAPES DE LA CONSTRUCTION			
DESCRIPTION DES TRAVAUX	RÉPERCUSSIONS SUR LA CIRCULATION		
	HEURE	AUTOROUTE	BRETELLES
<ul style="list-style-type: none"><li>Installation de panneaux muraux antibruit de la STA. 28+384.7 à la STA. 28+482.6 et de la STA. 28+572.2 à la STA. 28+670</li><li>Enlèvement et réfection des barrières sur toutes les structures et tous les murs de soutènement</li><li>Érection de panneaux de mur antibruit</li><li>Installation d'une barrière en béton précoulé intégrée à un mur antibruit, et de semelles à tous les emplacements montés sur le sol</li><li>Installation de panneaux antibruit en acrylique sur la structure et les murs de soutènement de la rue Kent</li></ul>	Jour	<ul style="list-style-type: none"><li>Toutes les voies seront ouvertes en direction est</li></ul>	<ul style="list-style-type: none"><li>Une voie de sortie obligatoire et une voie de sortie facultative ouverte à la bretelle de sortie de Kent en direction est</li><li>La bretelle de sortie de Metcalfe en direction est ouverte</li></ul>
	Nuit	<ul style="list-style-type: none"><li>Une voie ouverte en direction est vers la STA. 28+600</li><li>Durée : de 20 h à 6 h, durant 2 ou 3 semaines</li><li>Deux voies ouvertes en direction est à partir de la STA. 28+600 en direction est</li><li>Durée : de 20 h à 6 h, durant 8 à 12 semaines</li></ul>	<ul style="list-style-type: none"><li>Deux voies de sortie obligatoire à la bretelle de sortie de Kent en direction est</li><li>Fermeture de la bretelle de sortie de Metcalfe en direction est</li><li>Durée : de 20 h à 6 h, durant 2 ou 3 semaines</li><li>Bretelle d'accès de Kent en direction ouest fermée pour l'érection de panneaux en travers des bretelles</li><li>Durée : de 20 h à 6 h, durant 1 semaine</li></ul>

### MURS ANTIBRUIT 4/5

MURS ANTIBRUIT 4/5 : ÉTAPES DE LA CONSTRUCTION				
DESCRIPTION DES TRAVAUX	RÉPERCUSSIONS SUR LA CIRCULATION			
	HEURE	AUTOROUTE	BRETELLES	RUES DE LA VILLE
<ul style="list-style-type: none"><li>Installation de panneaux antibruit de la STA. 28+625 à la STA. 28+678.8</li><li>Réparation des bandeaux de béton sur les murs de soutènement</li><li>Placement des ancrs et du béton pour les poteaux de soutien des barrières sur les murs de soutènement</li><li>Érection de barrières à panneaux antibruit</li><li>Installation d'une barrière précoulée intégrée à un mur antibruit, et de semelles à tous les emplacements montés sur le sol</li></ul>	Jour	<ul style="list-style-type: none"><li>Toutes les voies seront ouvertes en direction est</li></ul>	<ul style="list-style-type: none"><li>Une voie de sortie obligatoire et une voie de sortie facultative ouverte à la bretelle de sortie de Kent en direction est</li><li>Ouverture de la bretelle de sortie de Metcalfe en direction est</li></ul>	<ul style="list-style-type: none"><li>Deux voies sur Isabella avec largeurs de voie de 3,5 m</li><li>Durée : 24 heures durant 8 à 12 semaines</li></ul>
	Nuit	<ul style="list-style-type: none"><li>Deux voies ouvertes en direction est</li><li>Durée: 8 à 12 semaines</li><li>De 21 h à 6 h : réduction d'une seule voie</li><li>De 22 h à 6 h : réduction de deux voies</li></ul>	<ul style="list-style-type: none"><li>Deux voies de sortie obligatoire à la bretelle de sortie de Kent en direction est</li><li>Fermeture de la bretelle de sortie de Metcalfe en direction est</li><li>Durée : de 21 h à 6 h, durant 8 à 12 semaines</li></ul>	<ul style="list-style-type: none"><li>Isabella réduite à une voie</li><li>Durée : de 20 h à 6 h, durant 8 à 12 semaines</li></ul>

### MUR ANTIBRUIT 5

MUR ANTIBRUIT 5 : ÉTAPES DE LA CONSTRUCTION				
DESCRIPTION DES TRAVAUX	RÉPERCUSSIONS SUR LA CIRCULATION			
	HEURE	AUTOROUTE	BRETELLES	RUES DE LA VILLE
<ul style="list-style-type: none"><li>Installation de panneaux antibruit de la STA. 50+003 à la STA. 50+472</li><li>Réparation des bandeaux de béton sur les structures et murs de soutènement</li><li>Placement des ancrs et du béton pour les poteaux de soutien du mur antibruit sur les structures et les murs de soutènement</li><li>Érection des panneaux du mur antibruit</li><li>Installation d'une barrière précoulée intégrée à un mur antibruit, et de semelles à tous les emplacements montés sur le sol</li></ul>	Jour	<ul style="list-style-type: none"><li>Toutes les voies seront ouvertes en direction est</li></ul>	<ul style="list-style-type: none"><li>Une voie de sortie obligatoire et une voie de sortie facultative sera ouverte à la bretelle de sortie de Kent en direction est</li><li>Ouverture de la bretelle de sortie de Metcalfe en direction est</li></ul>	<ul style="list-style-type: none"><li>Deux voies sur Isabella avec largeurs de voie de 3,5 m</li><li>Durée : 24 heures durant 8 à 12 semaines</li></ul>
	Nuit	<ul style="list-style-type: none"><li>Deux voies ouvertes en direction est</li><li>Durée : de 20 h à 6 h, durant 8 à 12 semaines</li></ul>	<ul style="list-style-type: none"><li>Deux voies de sortie obligatoire à la bretelle de sortie de Kent en direction est</li><li>Bretelle de sortie de Metcalfe en direction est fermée</li><li>Durée : de 20 h à 6 h, durant 8 à 12 semaines</li></ul>	<ul style="list-style-type: none"><li>Isabella réduite à une voie</li><li>Fermeture de Metcalfe au nord et au sud d'Isabella pour l'enlèvement du mur antibruit, l'installation de poteaux et de panneaux</li><li>Durée : de 20 h à 6 h, durant 8 à 12 semaines</li></ul>

### MUR ANTIBRUIT 6

MUR ANTIBRUIT 6 : ÉTAPES DE LA CONSTRUCTION				
DESCRIPTION DES TRAVAUX	RÉPERCUSSIONS SUR LA CIRCULATION			
	HEURE	AUTOROUTE	BRETELLES	RUES DE LA VILLE
<ul style="list-style-type: none"><li>Installation d'une barrière précoulée intégrée à un mur antibruit, et de semelles à tous les emplacements montés sur le sol</li><li>Réfection du bandeau suspendu de la structure et du mur de soutènement, installation des pièces de la plaque d'assise et de panneaux antibruit en acrylique</li></ul>	Jour	<ul style="list-style-type: none"><li>Toutes les voies seront ouvertes en direction est</li></ul>	<ul style="list-style-type: none"><li>Réduction à une voie de la bretelle d'accès de Metcalfe en direction est</li><li>Durée : 24 heures durant 10 à 12 semaines</li></ul>	<ul style="list-style-type: none"><li>Toutes les voies ouvertes sur Isabella</li><li>Révision de la voie 2 pour la réserver aux virages à gauche</li><li>Accès par la voie 3 à la 417 en direction est</li><li>Durée : 24 heures durant 10 à 12 semaines</li><li>Présence policière pour le contrôle de la circulation pendant la période de pointe de l'après-midi</li></ul>
	Nuit	<ul style="list-style-type: none"><li>Toutes les voies seront ouvertes en direction est</li></ul>	<ul style="list-style-type: none"><li>Réduction à une voie de la bretelle d'accès de Metcalfe en direction est</li><li>Durée : 24 heures durant 10 à 12 semaines</li></ul>	<ul style="list-style-type: none"><li>Toutes les voies ouvertes sur Isabella</li><li>Révision de la voie 2 pour la réserver aux virages à gauche</li><li>Accès par la voie 3 à la 417 en direction est</li><li>Durée : 24 heures durant 10 à 12 semaines</li><li>Réduction de voie sur Elgin pour les travaux en surplomb</li><li>Durée : de 21 h à 7 h, durant 1 semaine</li></ul>

### SOMMAIRE DES FERMETURES DE BRETELLES

SOMMAIRE DES FERMETURES DE BRETELLES			
MUR ANTIBRUIT	BRETELLES	TYPE DE FERMETURE	DURÉE APPROXIMATIVE
1	<ul style="list-style-type: none"><li>Bretelle de sortie de Rochester en direction est</li></ul>	<ul style="list-style-type: none"><li>De nuit, récurrente, de 20 h à 6 h</li></ul>	<ul style="list-style-type: none"><li>4 à 5 semaines</li></ul>
2, 3A	<ul style="list-style-type: none"><li>Bretelle d'accès de Lyon en direction ouest</li></ul>	<ul style="list-style-type: none"><li>Fermeture à plein temps (24 heures sur 24)</li></ul>	<ul style="list-style-type: none"><li>10 à 12 semaines</li></ul>
2, 3A / 3B	<ul style="list-style-type: none"><li>Bretelle de sortie de Bronson en direction ouest</li><li>Bretelle d'accès de Lyon en direction ouest</li></ul>	<ul style="list-style-type: none"><li>Fermeture à plein temps (24 heures sur 24)</li></ul>	<ul style="list-style-type: none"><li>6 à 8 semaines</li></ul>
4	<ul style="list-style-type: none"><li>Bretelle de sortie de Kent en direction est</li></ul>	<ul style="list-style-type: none"><li>Fermeture complète de nuit de 20 h à 6 h (seulement pour l'érection de panneaux suspendus sur la structure)</li></ul>	<ul style="list-style-type: none"><li>1 semaine</li></ul>
3C	<ul style="list-style-type: none"><li>Bretelle d'accès d'O'Connor en direction ouest</li></ul>	<ul style="list-style-type: none"><li>De nuit, récurrente, de 20 h à 6 h</li></ul>	<ul style="list-style-type: none"><li>3 à 4 semaines</li></ul>
4 / 5	<ul style="list-style-type: none"><li>Bretelle de sortie de Metcalfe en direction est</li></ul>	<ul style="list-style-type: none"><li>De nuit, récurrente, de 20 h à 6 h</li></ul>	<ul style="list-style-type: none"><li>8 à 12 semaines</li></ul>
6	<ul style="list-style-type: none"><li>Bretelle d'accès de Metcalfe en direction est</li></ul>	<ul style="list-style-type: none"><li>Fermeture d'une ou de deux voies (24 heures sur 24)</li></ul>	<ul style="list-style-type: none"><li>10 à 12 semaines</li></ul>

### SOMMAIRE DES EFFETS SUR LES PIÉTONS

SOMMAIRE DES EFFETS SUR LES PIÉTONS			
MUR ANTIBRUIT	LIEU	EFFETS ET MESURES D'ATTÉNUATION PROPOSÉES	DURÉE APPROXIMATIVE
2	<ul style="list-style-type: none"><li>Les deux côtés de l'avenue Bronson sous l'autoroute 417</li></ul>	<ul style="list-style-type: none"><li>Pendant l'enlèvement de la barrière actuelle et de tous les ouvrages surélevés, un des trottoirs sous la structure demeurera ouvert en permanence.</li></ul>	<ul style="list-style-type: none"><li>Chaque nuit durant 1 à 3 semaines</li></ul>
3B	<ul style="list-style-type: none"><li>Les deux côtés de la rue Percy sous l'autoroute 417</li></ul>	<ul style="list-style-type: none"><li>Pendant l'enlèvement de la barrière actuelle et de tous les ouvrages surélevés, un des trottoirs sous la structure demeurera ouvert en permanence.</li></ul>	<ul style="list-style-type: none"><li>Chaque nuit durant 1 à 3 semaines</li></ul>
4	<ul style="list-style-type: none"><li>Le côté est de la rue Kent sous l'autoroute 417</li></ul>	<ul style="list-style-type: none"><li>Pendant l'installation des panneaux du mur antibruit au-dessus de la voie menant de Chamberlain ouest à la rue Kent nord, le trottoir est sera fermé.</li><li>La circulation piétonnière sera acheminée vers la rue Bank le long du trottoir du côté sud de la rue Chamberlain.</li></ul>	<ul style="list-style-type: none"><li>Chaque nuit durant 1 semaine</li></ul>
4	<ul style="list-style-type: none"><li>Les deux côtés de la rue Bank sous l'autoroute 417</li></ul>	<ul style="list-style-type: none"><li>Pendant l'enlèvement de la barrière actuelle et de tous les ouvrages surélevés, un des trottoirs sous la structure demeurera ouvert en permanence.</li></ul>	<ul style="list-style-type: none"><li>Chaque nuit durant 1 à 3 semaines</li></ul>
5	<ul style="list-style-type: none"><li>Les deux côtés de la rue O'Connor sous l'autoroute 417</li></ul>	<ul style="list-style-type: none"><li>Pendant l'enlèvement de la barrière actuelle et de tous les ouvrages surélevés, un des trottoirs sous la structure demeurera ouvert en permanence.</li></ul>	<ul style="list-style-type: none"><li>Chaque nuit durant 1 à 3 semaines</li></ul>
5	<ul style="list-style-type: none"><li>Les deux côtés de la rue Metcalfe sous l'autoroute 417</li></ul>	<ul style="list-style-type: none"><li>Pendant l'enlèvement de la barrière actuelle et de tous les ouvrages surélevés, le trottoir ouest sera fermé.</li><li>La circulation piétonnière sera acheminée vers O'Connor par les trottoirs du côté sud de Catherine ou d'Isabella.</li></ul>	<ul style="list-style-type: none"><li>Chaque nuit durant 1 à 3 semaines</li></ul>
6	<ul style="list-style-type: none"><li>Les deux côtés de la rue Elgin sous l'autoroute 417</li></ul>	<ul style="list-style-type: none"><li>Pendant l'enlèvement de la barrière actuelle et de tous les ouvrages surélevés, au moins un des trottoirs sous la structure demeurera ouvert en permanence.</li><li>Dans le cas de la fermeture du trottoir ouest, la circulation piétonnière sera acheminée vers le sud le long du prolongement d'Elgin vers Pretoria / Queen Elizabeth pour traverser au trottoir est sous la structure.</li></ul>	<ul style="list-style-type: none"><li>Chaque nuit durant 1 à 3 semaines</li></ul>

Note : Des travaux pourront être effectués simultanément à des emplacements de murs antibruit individuels et sur l'autoroute 417, en directions est et ouest.





## Construction and Traffic Staging – Alternate Routes

## Construction et étapes de la circulation : autres trajets

**NOISE WALL 2 – BRONSON AVENUE ALTERNATE SIDEWALK CLOSURE**  
**MUR ANTIBRUIT 2 : AUTRE FERMETURE DE TROTTOIR SUR L'AVENUE BRONSON**



**NOISE WALL 3B – PERCY STREET ALTERNATE SIDEWALK CLOSURE**  
**MUR ANTIBRUIT 3B : AUTRE FERMETURE DE TROTTOIR SUR LA RUE PERCY**



**NOISE WALLS 3A / 3B - SIGNED DETOUR FOR WESTBOUND BRONSON OFF-RAMP CLOSURE**  
**MURS ANTIBRUIT 3A / 3B : DÉTOUR AVEC PANNEAUX DE SIGNALISATION POUR LA FERMETURE DE LA BRETELLE DE SORTIE DE BRONSON EN DIRECTION OUEST**



**NOISE WALL 4 – KENT STREET EAST SIDE SIDEWALK CLOSURE**  
**MUR ANTIBRUIT 4 : FERMETURE DU TROTTOIR DE LA RUE KENT, CÔTÉ EST**







## Construction and Traffic Staging – Alternate Routes

## Construction et étapes de la circulation : autres trajets

NOISE WALL 4 – BANK STREET ALTERNATE SIDEWALK CLOSURES  
MUR ANTIBRUIT 4 : AUTRE FERMETURES DE TROTTOIR SUR LA RUE BANK



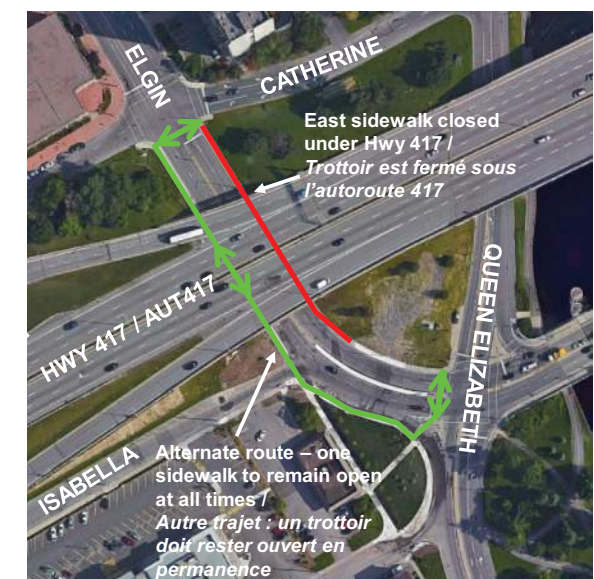
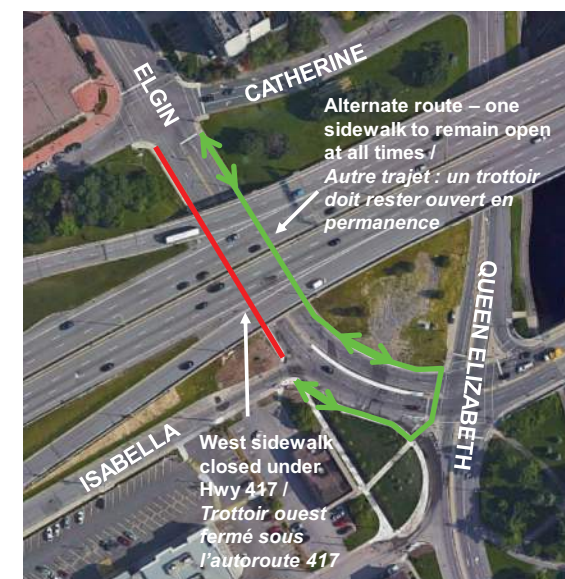
NOISE WALL 5 – O'CONNOR STREET ALTERNATE SIDEWALK CLOSURES  
MUR ANTIBRUIT 5 : AUTRE FERMETURES DE TROTTOIR SUR LA RUE O'CONNOR



NOISE WALL 5 – METCALFE STREET WEST SIDE SIDEWALK CLOSURE  
MUR ANTIBRUIT 5 : FERMETURE DU TROTTOIR DE LA RUE METCALFE, CÔTÉ OUEST



NOISE WALL 6 – ELGIN STREET ALTERNATE SIDEWALK CLOSURES  
MUR ANTIBRUIT 6 : AUTRE FERMETURES DE TROTTOIR SUR LA RUE ELGIN







Environmental Impacts & Mitigation

ISSUES/CONCERNS/POTENTIAL IMPACTS	MITIGATION/PROTECTION/MONITORING
<b>1.0 Vegetation</b>	
The installation of noise barriers along the north and south sides of the highway within the project limits will require minor vegetation removal.	<ul style="list-style-type: none"><li>• Vegetation clearing zones and vegetation retention zones will be clearly delineated in contract documentation and in the field;</li><li>• Vegetation clearing will not be permitted to take place from April 1<sup>st</sup> to August 15<sup>th</sup> to protect nesting migratory birds. In the event that tree removal must occur within the above-noted window, the Contractor should retain a Qualified Avian Biologist to conduct a nesting survey prior to clearing; and</li><li>• The use of appropriate vegetation clearing techniques will be employed and exposed surfaces will be re-stabilized and re-vegetated as soon as possible following construction.</li></ul>
<b>2.0 Wildlife</b>	
Potential disturbance to urban tolerant wildlife and migratory birds during construction. Species at Risk habitat is not present in the immediate study area.	<ul style="list-style-type: none"><li>• Any wildlife encountered during construction will not be knowingly harmed;</li><li>• Active nests will not be disturbed in accordance with the <i>Migratory Bird Convention Act</i>; and</li><li>• Should any Species at Risk be encountered at any time in the construction area, the Contract Administrator will be contacted immediately.</li></ul>
<b>3.0 Traffic Operations and Access</b>	
Ramp closures and lane reductions on Highway 417 and City streets are required for the installation of noise barriers and will result in disruptions to traffic operations and public access (pedestrians, cyclists, and motorists).	<ul style="list-style-type: none"><li>• The number and duration of closures has been kept to the minimum required to complete the work;</li><li>• A traffic management plan has been developed and will be implemented to minimize the impact of closures, in coordination with the City of Ottawa;</li><li>• Ongoing communication will be maintained with Emergency Services; and</li><li>• Temporary and advance signage in both official languages will provide notice of upcoming closures/disruptions and will identify alternate routes for pedestrians, cyclists, and motorists.</li></ul>
<b>4.0 Aesthetics</b>	
Potential impacts to the visual character of the corridor.	<ul style="list-style-type: none"><li>• Context Sensitive Design (CSD) solutions will be applied through noise barrier treatments, including concrete noise barrier walls in keeping with the same hues and tones as the new noise barrier installed at Lees Avenue, as well the use of translucent/clear acrylic noise barrier panels across highway overpass structures.</li></ul>
<b>5.0 Noise</b>	
Potential for elevated noise levels during construction.	<ul style="list-style-type: none"><li>• The Contractor will abide by the municipal noise control by-law for day-to-day operations and any exemptions to the municipal noise control by-law required for night work will be obtained prior to construction.</li></ul>
<b>6.0 Utilities</b>	
There are no major utility conflicts or relocations required as part of the project.	<ul style="list-style-type: none"><li>• Vibration monitors may be required to be installed along the City watermain on Isabella Street to ensure that vibrations related to heavy construction works are kept to within safe levels and do not result in damage to the watermain located immediately on site.</li></ul>
Depending on the final scope of the structural rehabilitation associated with Noise Wall 4, there may be potential for vibrations to occur during construction, resulting in potential impacts to the City watermain located on the north side of Isabella Street.	
<b>7.0 Archaeological &amp; Cultural Heritage</b>	
Potential to uncover archaeological resources during construction.	<ul style="list-style-type: none"><li>• No significant archaeological resources are known to exist within the study area, which has been significantly disturbed;</li><li>• If deeply buried archaeological resources are discovered during construction, the Ministry of Tourism, Culture and Sport shall be notified immediately. Construction will cease immediately and a licensed archaeologist will carry out a determination of the nature and significance of the archaeological resources.</li><li>• In the event that human remains are encountered during construction, the Cemeteries Regulation Unit of the Ministry of Consumer Service shall be notified. In situations where human remains are associated with archaeological resources, the Ministry of Tourism, Culture and Sport shall also be contacted to ensure that the site is not subject to unlicensed alterations which would be a contravention of the <i>Ontario Heritage Act</i>.</li></ul>
Potential to adversely affect cultural heritage resources.	
<b>8.0 Waste Management and Contamination</b>	
Waste, excess materials (including salt impacted soil) and emissions have the potential to contaminate the surrounding environment if not managed properly.	<ul style="list-style-type: none"><li>• Excess materials will be managed in accordance with OPSS 180.</li><li>• The Contractor will be responsible for controlling the emission of dust and other pollutants and preventing them from leaving the work site.</li></ul>



Répercussions environnementales et atténuation

ENJEUX, PRÉOCCUPATIONS ET IMPACTS POTENTIELS	ATTÉNUATION/PROTECTION/SURVEILLANCE
<b>1.0 Végétation</b>	
L'installation des murs antibruit le long des côtés nord et sud de l'autoroute, dans les limites du projet, nécessitera l'enlèvement d'une petite partie de la végétation.	<ul style="list-style-type: none"><li>Les zones à dégager et celles où on conservera la végétation seront clairement indiquées dans les documents contractuels et sur le site;</li><li>Il sera interdit de procéder à l'enlèvement de la végétation entre le 1 avril et le 15 août pour protéger les nids d'oiseaux migrateurs. Au cas où il faudrait enlever des arbres dans les lieux ci-dessus, l'entrepreneur devrait retenir les services d'un biologiste aviaire qualifié pour procéder à une inspection des nids avant le déboisement; et</li><li>Les techniques appropriées seront utilisées, et les surfaces exposées seront stabilisées et seront restaurées à l'état naturel dès que possible après la fin des travaux de construction.</li></ul>
<b>2.0 Faune</b>	
Il y a une possibilité de perturbation d'animaux sauvages adaptés à la ville et d'oiseaux migrateurs au cours de la construction. Il n'y a pas dans la zone à l'étude d'habitat d'espèces en péril.	<ul style="list-style-type: none"><li>Toute espèce faunique découverte lors des travaux sera traitée avec soin;</li><li>Les nids habités ne seront pas perturbés, conformément à la <i>Loi sur la convention concernant les oiseaux migrateurs</i>; et</li><li>À tout moment dans la zone où s'effectuent des travaux de construction, si des espèces en péril sont découvertes, l'administrateur du contrat en sera immédiatement informé.</li></ul>
<b>3.0 Circulation automobile et accès</b>	
Des fermetures de bretelles et des réductions de voies sur l'autoroute 417 et les rues de la ville s'imposeront pour l'installation de murs antibruit et entraîneront des perturbations de la circulation et de l'accès public (piétons, cyclistes et automobilistes).	<ul style="list-style-type: none"><li>Le nombre et la durée des fermetures pour permettre l'exécution des travaux ont été réduits au minimum;</li><li>Un plan de gestion de la circulation a été mis sur pied et sera mis en œuvre pour minimiser les répercussions des fermetures;</li><li>L'accès aux habitations privées et aux commerces le long de l'avenue Chamberlain sera maintenu;</li><li>Une communication permanente avec les services d'urgences sera assurée;</li><li>Une signalisation temporaire et une présignalisation dans les deux langues officielles informeront le public sur les fermetures et les perturbations à venir, et suggéreront des parcours de remplacement pour les piétons, les cyclistes et les automobilistes.</li></ul>
<b>4.0 Aménagement esthétique</b>	
Il y a des impacts possibles sur l'aspect visuel du corridor.	<ul style="list-style-type: none"><li>Des solutions basées sur la conception tenant compte du contexte (CTCC) s'appliqueront durant tous les traitements relatifs au mur antibruit, y compris des parois de béton pour préserver les mêmes teintes et couleurs que le nouveau mur antibruit installé sur l'avenue Lees, et l'usage de panneaux translucides ou transparents en acryliques pour les structures du passage supérieur de l'autoroute.</li></ul>
<b>5.0 Bruit</b>	
Possibilité de forts taux de bruit lors des travaux de construction.	<ul style="list-style-type: none"><li>L'entrepreneur respectera les règlements municipaux sur le bruit dans ses activités quotidiennes, toute dispense requise à ces règlements pour des travaux de nuit sera obtenue avant de procéder aux travaux.</li></ul>
<b>6.0 Services publics</b>	
Le projet ne nécessitera aucun conflit majeur ni déplacement important au sujet des services publics.	<ul style="list-style-type: none"><li>Il se peut que l'on doive installer des enregistreurs de vibrations le long de cette conduite d'eau maîtresse pour s'assurer que les vibrations produites par les travaux de construction lourde demeurent à des niveaux sécuritaires et n'endommagent pas la conduite d'eau maîtresse située sur place.</li></ul>
Selon l'envergure finale de la réfection structurale associée au mur antibruit 4, il se pourrait que des vibrations se produisent pendant la construction, avec des répercussions sur la conduite d'eau maîtresse de la ville située du côté nord de la rue Isabella.	
<b>7.0 Patrimoine archéologique et culturel</b>	
Possibilité de découverte de ressources archéologiques au cours des travaux de construction.	<ul style="list-style-type: none"><li>Aucune ressource archéologique d'intérêt n'a été signalée dans la zone à l'étude, qui a déjà été passablement perturbée.</li><li>Si, au cours des travaux de construction, on découvre des ressources archéologiques profondément enfouies, le ministère de la Culture, du Tourisme et du Sport sera averti immédiatement, et l'entrepreneur ou la personne ayant découvert la ressource archéologique devra cesser toute perturbation du site, et retenir les services d'un expert conseil en archéologie certifié pour déterminer la nature et l'importance de la découverte.</li><li>Si des restes humains sont découverts lors des travaux de construction, l'Unité de réglementation des cimetières du ministère des Services aux consommateurs devra être avertie. Si la découverte de restes humains est liée à celle de ressources archéologiques, le ministère de la Culture, du Tourisme et du Sport devra lui aussi être averti pour s'assurer que le site n'est pas soumis à des modifications non autorisées qui iraient à l'encontre de la <i>Loi sur le patrimoine de l'Ontario</i>.</li></ul>
Possibilité d'effets indésirables sur des ressources culturelles patrimoniales.	
<b>8.0 Gestion des déchets et contamination</b>	
Les déchets, les matériaux excédentaires (y compris les sols halomorphes) et les émissions qui pourraient contaminer le milieu s'ils ne sont pas gérés adéquatement.	<ul style="list-style-type: none"><li>Les matériaux excédentaires seront gérés conformément aux normes provinciales OPSS 180.</li><li>Il incombera à l'entrepreneur de contrôler les émissions de poussière et autres substances polluantes et de les confiner au site des travaux.</li></ul>



## Next Steps

Following this Public Information Centre, next steps will include:

- Reviewing and responding to comments received;
- Refining the detail design and mitigation plan;
- Finalizing the detail design and preparing the contract package;
- Preparing a Design and Construction Report for this study and submitting for a 30-day public review period;
- Securing property agreements with land owners adjacent to the highway for temporary usage to facilitate construction; and
- Construction.

Thank you for attending the Public Information Centre. We welcome your comments. Information is being collected in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

If you would like more information, please contact:

**Mr. David Lindensmith, P.Eng.**  
**MTO Senior Project Engineer**

Phone: 613-540-5130 / 1-800-267-0295 (toll-free)

Email: dave.lindensmith@ontario.ca

Fax: 613-540-5106

**Mr. Lincoln MacDonald, P.Eng.**  
**Consultant Project Manager (MMM)**

Phone: 613-736-7200 / 1-877-998-9912 (toll-free)

Email: macdonaldl@mmm.ca

Fax: 613-736-8710

## Prochaines étapes

Après la présente séance d'information publique, voici quelles seront les prochaines étapes :

- examiner les commentaires reçus et y répondre;
- perfectionner les conceptions détaillées et les plans d'atténuation;
- préparer un rapport de conception et de construction pour chaque projet et le soumettre à l'examen du public pendant 30 jours;
- mettre au point les conceptions détaillées et préparer les documents contractuels;
- conclusion d'accords avec les propriétaires fonciers adjacents à l'autoroute en vue d'un usage temporaire de leurs terrains pour faciliter la construction; et
- construction.

Merci d'avoir assisté à la séance d'information publique. Vos commentaires sont les bienvenus. Les renseignements sont recueillis conformément à la *Loi sur l'accès à l'information et la protection de la vie privée*. À l'exception des renseignements personnels, tous les commentaires feront partie du domaine public.

Pour de plus amples détails, veuillez communiquer avec :

Des renseignements sont disponibles en français auprès de:

**Meghan MacMillan**

**Urbaniste de la firme (MMM)**

Téléphone: 613-736-7200 poste 3561

Courriel : macmillanm@mmm.ca

Fax: 613-736-8710

[www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)



**HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN  
AND THE RIDEAU CANAL, DETAIL DESIGN (WP 4088-07-01)**

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**Design and Construction Report**

**APPENDIX C**  
**Agency Correspondence**



**From:** Tim Dickinson  
**Sent:** April-01-13 8:18 AM  
**To:** Anita Sott  
**Subject:** Fwd: Queensway Noise Barriers Retrofits  
**Attachments:** Notice of Study Commencement.pdf; ATT00001.htm

From: Holmes, Diane [<mailto:Diane.Holmes@ottawa.ca>]  
Sent: Thursday, March 28, 2013 3:07 PM  
To: '[dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)'; Manfred Goetz  
Cc: '[ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)'  
Subject: FW: Queensway Noise Barriers Retrofits

Thank you for the opportunity to comment. I am very pleased to see further discussion for the implementation of the approved retrofit barriers along the Queensway. These will fill gaps in the existing sound barriers and several are to be located in Somerset Ward at the following locations:

- Bronson Ave to Lyon St, North Side

CPR / SL&H Railway to Preston St (Facing Young St), South Side

The need for these noise barriers have been in discussion for decades, my files include letters between the residents, community associations and Province on this topic dating as far back as 1987. These barriers need to be installed as soon as possible to provide relief to residents abutting the Queensway.

It is positive that the discussions continue, hopefully bringing us closer to implementation. Please keep me updated on your decisions and the upcoming open house.

Regards,

Diane Holmes  
Councillor, Somerset Ward  
(613) 580-2484  
[Diane.Holmes@ottawa.ca](mailto:Diane.Holmes@ottawa.ca)<<mailto:Laura.Bergen@ottawa.ca>>  
[www.dianeholmes.ca](http://www.dianeholmes.ca)<<http://www.dianeholmes.ca>>

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Phone 3-1-1 or online at [serviceottawa.ca](http://serviceottawa.ca) for general inquiries including street and sidewalk maintenance, garbage pickup, recycling and by-law enforcement.

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**From:** McKenney, Catherine <Catherine.Mckenney@ottawa.ca>  
**Sent:** October-06-15 12:56 PM  
**To:** Anita Sott  
**Cc:** Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Steve Regel; Tim Dickinson;  
**Subject:** RE: Queensway Noise Barriers Retrofits

Thank you for this update. I look forward to hearing about the Public Information Centre.  
Sincerely ,  
Catherine

**Catherine McKenney.**  
Councillor - Somerset Ward  
613-580-2484 / catherinemckenney.ca / @cmckenney

---

**From:** Anita Sott [mailto:SottA@mmm.ca]  
**Sent:** Tuesday, October 06, 2015 9:28 AM  
**To:** McKenney, Catherine  
**Cc:** Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Steve Regel; Tim Dickinson  
**Subject:** RE: Queensway Noise Barriers Retrofits

Dear Ms. McKenney:

**RE: HIGHWAY 417 NOISE BARRIER RETROFITS, DETAIL DESIGN (WP 4088-07-01)**

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Former Councillor Diane Holmes submitted a comment to the Ministry of Transportation (MTO) in March 2013 regarding the Highway 417 Noise Barrier Retrofits project, which includes noise barrier retrofits on the south side of Highway 417 between the O-Train corridor and Preston Street; on the north side of the highway between Bronson Avenue and Kent Street; and on the south side of the highway from Lyon Street to the Rideau Canal. MMM Group Limited has been retained by the MTO to undertake the detail design and environmental assessment study for this project.

In response to the comment received from Ms. Holmes below, we would like to take this opportunity to advise you that the Highway 417 Noise Barrier Retrofits project is currently undergoing detail design. A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined. Please note that the timing of construction for this project is dependent on provincial priorities and the availability of funding. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174. Construction of the noise barrier retrofits may also be combined with other road improvements in the Highway 417 corridor to the extent possible, in order to minimize traffic impacts.

Please be advised that you will be notified directly of future consultation opportunities including the PIC and 30-day public review of the Design and Construction Report.

Sincerely,

**Anita Sott, MCIP, RPP**  
Planner  
Planning and Environmental Design  
On behalf of

**Steve Regel, P.Eng**  
Senior Project Manager  
Associate  
Highways Department

**MMM Group Limited**  
t: 613.736.7200 x3228| f: 613.736.8710

We invite you to explore our new MMM Group website. Visit [mmmgrouplimited.com](http://mmmgrouplimited.com) to learn who we are, what we do, and how we work. Reach out to an expert, view creative solutions to challenging projects, and learn about how we live our purpose – to ***Enrich the Quality of People's Lives.***

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*Please consider the environment before printing this e-mail and/or its attachments.*

**From:** McKenney, Catherine <Catherine.Mckenney@ottawa.ca>  
**Sent:** October-07-15 1:24 PM  
**To:** Anita Sott  
**Cc:** 'president@centretowncitizens.ca'; 'president@ottawadalhousie.ca';  
Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Steve Regel; Tim Dickinson;  
**Subject:** RE: Queensway Noise Barriers Retrofits

Hello Anita,

I am copying the two community associations in my ward that cover the span where this construction will take place, in order to ensure that they are aware:

- The Dalhousie Community Association (Michael Powell, President), which covers the area between the O-Train bridge on the west and LeBreton Street S (south of the 417) and Bay Street (north of the 417) on the east;
- The Centretown Citizens Community Association (Thomas McVeigh, President), which covers the area between Bronson Avenue and the Rideau Canal on the north side of the 417

Please ensure these associations are included in your consultations.

Thanks,  
Catherine

**Catherine McKenney.**

Councillor - Somerset Ward

613-580-2484 / catherinemckenney.ca / @cmckenney



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**From:** Tim Dickinson  
**Sent:** March-11-13 1:13 PM  
**To:** Anita Sott  
**Subject:** FW: WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design  
**Attachments:** Hwy417\_noise\_Grimwood.pdf

---

**From:** Grimwood, Robert [<mailto:Robert.Grimwood@ottawa.ca>]  
**Sent:** March-11-13 1:12 PM  
**To:** Tim Dickinson  
**Cc:**  
**Subject:** RE: WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design

Tim – please include me on the circulation list for this project, would appreciate seeing the drawings once they are ready. See attached. Thanks.

**Robert Grimwood, P.Eng.**

Senior Project Manager, Sustainable Transportation  
City of Ottawa  
(613) 580-2424 x28757  
[robert.grimwood@ottawa.ca](mailto:robert.grimwood@ottawa.ca)

---

**From:** Anita Sott [<mailto:SottA@mmm.ca>]  
**Sent:** March 08, 2013 11:38 AM  
**To:** Anita Sott  
**Cc:** [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca); [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca); Manfred Goetz; Tim Dickinson  
**Subject:** WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design

Dear Technical Advisory Committee Member,

The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for the Highway 417 Noise Barrier Retrofits (WP 4088-07-01). The purpose of this message is to inform you of the commencement of the project's detail design and provide you with an opportunity to offer comments and/or provide information in your capacity as a member of the Highway 417 Expansion Technical Advisory Committee.

Attached please find a letter from the Consultant Project Manager which provides a general overview of the project, as well as a comment form and details on how to provide any information, questions or comments you may have.

We look forward to your continued participation in this project.

Thank you,

**Anita Sott, M.A., M.P.I.**

Planner, Planning and Environmental Design

on behalf of

**Tim Dickinson, M.Pl., MCIP, RPP**

Associate, Planning and Environmental Design

**MMM Group Limited**

1145 Hunt Club Road, Suite 300

Ottawa, Ontario, Canada K1V 0Y3

t: 613-736-7200 x3546 | f: 613-736-8710 | c: 613-219-4760

[dickinsont@mmm.ca](mailto:dickinsont@mmm.ca) | [www.mmm.ca](http://www.mmm.ca)



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**HIGHWAY 417 NOISE BARRIER RETROFITS  
BETWEEN THE O-TRAIN AND THE RIDEAU CANAL  
(WP 4088-07-01)**

**COMMENT FORM**

Type of Project: Detail Design  
Environmental Assessment Type: Class EA for Provincial Transportation Facilities (Group "B" projects)  
Project Location: City of Ottawa – See enclosed Key Map

Agency Name & Division or Branch:	CITY OF OTTAWA - TRANSPORTATION PLANNING
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**COMMENTS:**

1. Does your organization wish to participate in this project?

☒ YES

☐ NO

2. If yes to the above, please provide the contact name, telephone #, address and e-mail for future correspondence.

Contact Name:	ROBERT GRIMWOOD
Telephone #:	613-580-2424 x28757
Address:	110 LAURIER AVE. WEST, 4th Floor OTTAWA, ONTARIO K1P 1J1
E-mail:	robert.grimwood@ottawa.ca

3. Preferred method of correspondence?

☒ Email

☐ Regular Mail

4. Please provide any information, questions, comments or concerns your agency may have at this time.

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For further information regarding this project, please visit [www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com) or contact the Consultant Environmental Planner, Mr. Tim Dickinson, M.P.I., MCIP, RPP, MMM Group Limited.

**PLEASE FAX OR EMAIL THIS FORM BACK BY MARCH 22, 2013**

FAX#: (613) 736-8710

EMAIL: dickinsont@mmm.ca

ATTN: Mr. Tim Dickinson, M.P.I., MCIP, RPP  
 MMM Group Limited  
 1145 Hunt Club Rd.  
 Ottawa, ON K1V 0Y3

---

**From:** Courtland, Steven <Steven.Courtland@ottawa.ca>  
**Sent:** March-11-13 10:16 AM  
**To:** Anita Sott  
**Subject:** RE: WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design

Hi Anita,  
It will not be necessary for me to be part of the TAC for this project.  
thanks,  
Steve

---

**From:** Anita Sott [<mailto:SottA@mmm.ca>]  
**Sent:** March 08, 2013 11:38 AM  
**To:** Anita Sott  
**Cc:** [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca); [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca); Manfred Goetz; Tim Dickinson  
**Subject:** WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design

Dear Technical Advisory Committee Member,

The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for the Highway 417 Noise Barrier Retrofits (WP 4088-07-01). The purpose of this message is to inform you of the commencement of the project's detail design and provide you with an opportunity to offer comments and/or provide information in your capacity as a member of the Highway 417 Expansion Technical Advisory Committee.

Attached please find a letter from the Consultant Project Manager which provides a general overview of the project, as well as a comment form and details on how to provide any information, questions or comments you may have.

We look forward to your continued participation in this project.

Thank you,

**Anita Sott, M.A., M.PI.**

Planner, Planning and Environmental Design

on behalf of

**Tim Dickinson, M.PI., MCIP, RPP**

Associate, Planning and Environmental Design

MMM Group Limited

1145 Hunt Club Road, Suite 300

Ottawa, Ontario, Canada K1V 0Y3

t: 613-736-7200 x3546 | f: 613-736-8710 | c: 613-219-4760

[dickinsont@mmm.ca](mailto:dickinsont@mmm.ca) | [www.mmm.ca](http://www.mmm.ca)



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**From:** Tim Dickinson  
**Sent:** March-18-13 7:27 AM  
**To:** Anita Sott  
**Subject:** Fwd: Highway 417 Noise Barrier Retrofits  
**Attachments:** image001.png; ATT00001.htm; 0A3219485146F633\_CRLB02F01MFD20130318071016.pdf; ATT00002.htm

**From:** "Tracey, Sean" <[Sean.Tracey@ottawa.ca](mailto:Sean.Tracey@ottawa.ca)>  
**Date:** 18 March, 2013 7:12:33 AM EDT  
**To:** "[dickinsont@mmm.ca](mailto:dickinsont@mmm.ca)" <[dickinsont@mmm.ca](mailto:dickinsont@mmm.ca)>  
**Subject:** Highway 417 Noise Barrier Retrofits

Please see attached form – we would not need to be notified of further developments on this project.

Sean

Sean A. Tracey, P.Eng., MIFireE  
Assistant Deputy Chief Community Standards/ Assistant Chef adjoint Normes communautaires  
City of Ottawa / Ville d'Ottawa  
1445 Carling Avenue  
Ottawa, ON  
K1Z 7L9  
Tel: 613-580-2424 ext 29420  
Fax: 613-580-2864  
[Sean.Tracey@ottawa.ca](mailto:Sean.Tracey@ottawa.ca)  
[www.ottawa.ca](http://www.ottawa.ca)



**HIGHWAY 417 NOISE BARRIER RETROFITS  
BETWEEN THE O-TRAIN AND THE RIDEAU CANAL  
(WP 4088-07-01)**

**COMMENT FORM**

Type of Project: Detail Design  
Environmental Assessment Type: Class EA for Provincial Transportation Facilities (Group "B" projects)  
Project Location: City of Ottawa – See enclosed Key Map

Agency Name & Division or Branch:	OTTAWA FIRE SERVICE
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**COMMENTS:**

1. Does your organization wish to participate in this project?

☐ YES

☒ NO

2. If yes to the above, please provide the contact name, telephone #, address and e-mail for future correspondence.

Contact Name:	
Telephone #:	
Address:	
E-mail:	

3. Preferred method of correspondence?

☐ Email

☐ Regular Mail

4. Please provide any information, questions, comments or concerns your agency may have at this time.

For further information regarding this project, please visit [www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com) or contact the Consultant Environmental Planner, Mr. Tim Dickinson, M.Pl., MCIP, RPP, MMM Group Limited.

**PLEASE FAX OR EMAIL THIS FORM BACK BY MARCH 22, 2013**

FAX#: (613) 736-8710

EMAIL: dickinson@mmm.ca

ATTN: Mr. Tim Dickinson, M.Pl., MCIP, RPP  
 MMM Group Limited  
 1145 Hunt Club Rd.  
 Ottawa, ON K1V 0Y3

**HIGHWAY 417 NOISE BARRIER RETROFITS  
BETWEEN THE O-TRAIN AND THE RIDEAU CANAL  
(WP 4088-07-01)**

**COMMENT FORM**

Type of Project: Detail Design  
Environmental Assessment Type: Class EA for Provincial Transportation Facilities (Group "B" projects)  
Project Location: City of Ottawa – See enclosed Key Map

Agency Name & Division or Branch:	Ministry of Tourism, Culture and Sport ("MTCS")
-----------------------------------	---

**COMMENTS:**

1. Does your organization wish to participate in this project?

☒ YES

☐ NO

2. If yes to the above, please provide the contact name, telephone #, address and e-mail for future correspondence.

Contact Name:	Amy Didrikson
Telephone #:	(416) 212-7420
Address:	401 Bay Street, Suite 1700 Toronto, ON M7A 0A7
E-mail:	amy.didrikson@ontario.ca

3. Preferred method of correspondence?

☒ Email

☐ Regular Mail

4. Please provide any information, questions, comments or concerns your agency may have at this time.

Please continue to circulate MTCS on all project notices and advise if the project area extends beyond areas previously subject to cultural heritage resource assessment.

MTCS's interest in this project relates to our mandate of conserving, protecting and preserving Ontario's cultural heritage resources, including archaeological resources, built heritage resources and cultural heritage landscapes.

For further information regarding this project, please visit [www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com) or contact the Consultant Environmental Planner, Mr. Tim Dickinson, M.P.I., MCIP, RPP, MMM Group Limited.

**PLEASE FAX OR EMAIL THIS FORM BACK BY MARCH 22, 2013**

FAX#: (613) 736-8710

EMAIL: dickinson@mmm.ca

ATTN: Mr. Tim Dickinson, M.P.I., MCIP, RPP  
 MMM Group Limited  
 1145 Hunt Club Rd.  
 Ottawa, ON K1V 0Y3

**From:** Tim Dickinson  
**Sent:** April-11-13 3:34 PM  
**To:** Anita Sott; Steve Regel  
**Subject:** FW: Enbridge Gas Response to Commencement of detail design, hwy 417 noise barrier retrofits b/w the O'train and the Rideau Canal (WP 4088-07-01)  
**Attachments:** 9553273\_Enbridge Gas Response.pdf

---

**From:** Suzanne Renaud [<mailto:Suzanne.Renaud@enbridge.com>]  
**Sent:** April-11-13 3:30 PM  
**To:** Manfred Goetz  
**Cc:** Tim Dickinson  
**Subject:** Enbridge Gas Response to Commencement of detail design, hwy 417 noise barrier retrofits b/w the O'train and the Rideau Canal (WP 4088-07-01)

Hello Manny,

We apologize for the delay in our response attached which includes a 3-page plot confirming we do not have natural gas plant on Hwy 417 itself however we do have gas in surroundings areas. Let me know if you need anything else.

**Suzanne J. Renaud** | Mark-Up Administrator/Permits Coordinator, Distribution Planning  
**Enbridge Gas Distribution Inc.** | 400 Coventry Road, Ottawa ON K1K 2C7, Canada  
Office ☎ 613.742.4534 | Fax 📠 613.748.6869 | Email ✉ [suzanne.renaud@enbridge.com](mailto:suzanne.renaud@enbridge.com)

Enbridge Gas Distribution Inc.  
400 Coventry Rd.  
Ottawa, ON, K1K 2C7  
Canada  
www.enbridge.com/gas

Suzanne J. Renaud  
Tel.: 613.742.4534  
Fax: 613.748.6869  
suzanne.renaud@enbridge.com  
[mark-ups@enbridge.com](mailto:mark-ups@enbridge.com)  
[utilitycirculations@enbridge.com](mailto:utilitycirculations@enbridge.com)



EGD File Number: 9553273

## **Re: Commencement of detail design, hwy 417 noise barrier retrofits b/w the O'train and the Rideau Canal (WP 4088-07-01)**

Please find attached a copy of your drawing(s) on which we have marked our existing/proposed underground plant and have made the following determinations:



### **GENERAL LOCATION**

- Please refer to the attached drawings for information on our existing or proposed gas plant. The information provided is for **GENERAL LOCATION ONLY**. You must re-submit your detailed drawings for sign off by Enbridge Gas Distribution.



### **NO-CONFLICT**

- We have **NO OBJECTION** to your proposed plant as indicated. Please refer to the attached drawings for information on our existing or proposed gas plant. GAS MAINS MUST BE FIELD LOCATED. Before digging, please call ONTARIO ONE CALL. 48 hours in advance at 1-800-400-2255 for free gas locates service.
- See “**Third Party Requirements**” booklet for definitions, requirements & contact information.
- Test Holes are required to determine actual depth where infrastructure crosses gas plant.



### **CONFLICT**

- We have an **OBJECTION** to your proposed plant as indicated. Please refer to the attached drawings for information on our existing or proposed gas plant. Review your proposal and make changes to your plant to satisfy these requirements.
- See “**Third Party Requirements**” booklet for definitions, requirements & contact information.
- If relocation of our plant is required, contact Manager of Special Projects:



Eastern Region: Farzeen Rizvi, 613-748-6768



### **NEB PERMIT REQUIRED**

- An application form needs to be filed when crossing or working within 30 m of the right-of-way of the NEB regulated natural gas pipeline.
- Find enclosed booklet containing information and permit application form.
- If you want to discuss NEB permit process contact the Enbridge Gas Distribution Land Dept.:  
Chuck Reaney: 416-753-6929



### **VITAL MAIN**

- You are working within 3 m of a Vital Main Pipeline. A representative of the company must be contacted three (3) days prior to commencement of work. A member of our field force must be present while excavation of the main takes place and prior to backfilling. Alternatively please contact the Enbridge Gas Distribution Damage Prevention Dept: 1(866) 922-3622.
- See “**Third Party Requirements**” booklet for definitions, requirements & contact information.

#### **For Enbridge Internal Use:**



### **PILING & SHORING - ENGINEERING COORDINATION REQUIRED**

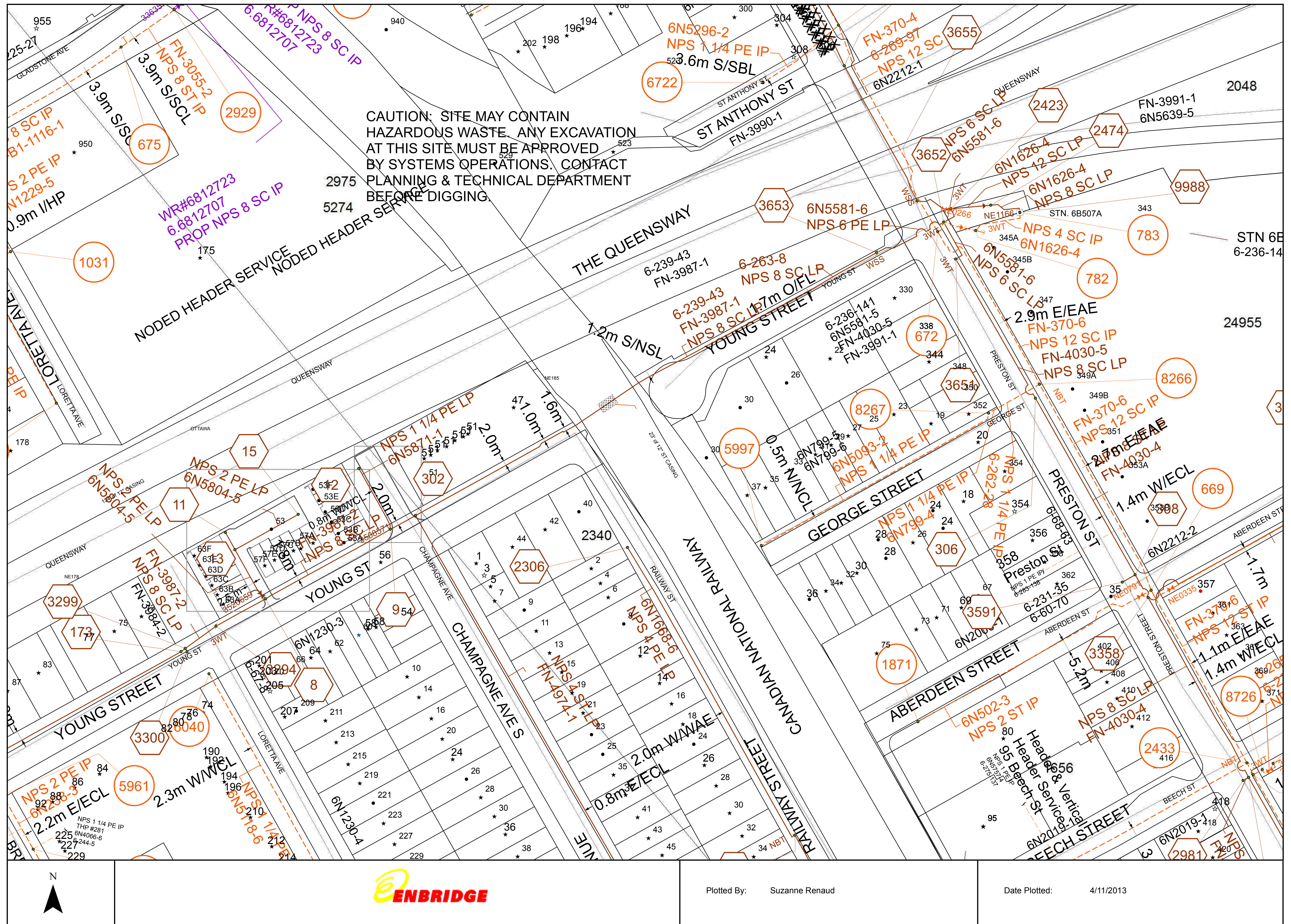


### **EXCAVATION, REPAVING OR GRADING - SPECIAL PROJECTS REVIEW REQUIRED**

Yours truly,

Suzanne J. Renaud





Note : Map is not to scale.











**Ministry of Aboriginal Affairs**

160 Bloor St. East, 9<sup>th</sup> Floor  
Toronto, ON M7A 2E6  
Tel: (416) 326-4740  
Fax: (416) 325-1066  
[www.aboriginalaffairs.gov.on.ca](http://www.aboriginalaffairs.gov.on.ca)

**Ministère des Affaires Autochtones**

160, rue Bloor Est, 9<sup>e</sup> étage  
Toronto ON M7A 2E6  
Tél. : (416) 326-4740  
Téléc. : (416) 325-1066  
[www.aboriginalaffairs.gov.on.ca](http://www.aboriginalaffairs.gov.on.ca)



**Ontario**

Reference: 76

April 11, 2013

Manny Goetz  
McCormick Rankin  
1145 Hunt Club Road, Suite 300  
Ottawa, Ontario  
K1V 0Y3

**Re: Commencement of Detail Design  
Highway 417 Noise Barrier Retrofits Between the O-Train and the Rideau  
Canal (WP 4088-07-01)**

Dear Manny Goetz:

Thank you for informing the Ministry of Aboriginal Affairs (MAA) of your project. Please note that MAA treats all letters, emails, general notices, etc. about a project as a request for information about which Aboriginal communities may have rights or interests in the project area.

For future Environmental Assessment (EA) inquiry correspondence to MAA, please take note of the following:

1. please send all future EA correspondence to the following email address:  
[MAA.EA.Review@ontario.ca](mailto:MAA.EA.Review@ontario.ca) ; or
2. if you prefer to send a hard copy rather than email, please address your correspondence as follows:  
Ministry of Aboriginal Affairs, Consultation Unit  
160 Bloor Street East, 4th floor  
Toronto, Ontario, Canada  
M7A 2E6.

As a member of the government review team, the Ministry of Aboriginal Affairs (MAA) identifies First Nation and Métis communities who may have the following interests in the area of your project:

- reserves;
- land claims or claims in litigation against Ontario;
- existing or asserted Aboriginal or treaty rights, such as harvesting rights; or
- an interest in the area of the project.

MAA is not the approval or regulatory authority for your project, and receives very limited information about projects in the early stages of their development. In circumstances where

a Crown-approved project may negatively impact a claimed Aboriginal or treaty right, the Crown may have a duty to consult the Aboriginal community advancing the claim. The Crown often delegates procedural aspects of its duty to consult to proponents. Please note that the information in this letter should not be relied on as advice about whether the Crown owes a duty to consult in respect of your project, or what consultation may be appropriate. Should you have any questions about your consultation obligations, please contact the appropriate ministry.

You should be aware that many First Nations and/or Métis communities either have or assert rights to hunt and fish in their traditional territories. For First Nations, these territories typically include lands and waters outside of their reserves.

In some instances, project work may impact aboriginal archaeological resources. If any Aboriginal archaeological resources could be impacted by your project, you should contact your regulating or approving Ministry to inquire about whether any additional Aboriginal communities should be contacted. Aboriginal communities with an interest in archaeological resources may include communities who are not presently located in the vicinity of the proposed project.

With respect to your project, and based on the brief materials you have provided, we can advise that the project appears to be located in an area where First Nations may have existing or asserted rights or claims in Ontario's land claims process or litigation, that could be impacted by your project. Contact information is below:

Algonquins Consultation Office 31 Riverside Drive Suite 101 Pembroke, ON K8A 8R6	Mr. Jim Hunton (613) 735-3759 (Fax) 735-6307
--	--

For your information, MAA notes that the following Métis community may be interested in your project given the proximity of their community or reserve lands to the area of the proposed project or because of your project's potential environmental impacts:

Ottawa Region Métis Council 528-740 Springland Drive Ottawa, ON K2J 2G2	Daniel Gilbeau, President (613) 859-4782 e-mail: <a href="mailto:gilbeaud@gmail.com">gilbeaud@gmail.com</a> website: <a href="http://www.ottawametis.ca">www.ottawametis.ca</a>
---	--

Please copy any correspondence to Ottawa Region Métis Council to the Métis Nation of Ontario. Contact information is below:

Métis Nation of Ontario Head Office 500 Old St. Patrick Street, Unit D Ottawa, Ontario, K1N 9G4	Métis Consultation Unit Fax: (613) 725-4225
---	--

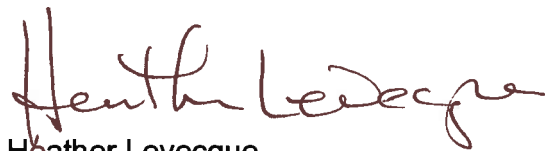
The Crown has a duty to consult Aboriginal peoples where it contemplates actions that may adversely impact on an existing or asserted Aboriginal or treaty right. Please seek the advice from your Legal Services Branch regarding whether the Crown has a duty to consult with respect to this project.

The information upon which the above comments are based is subject to change. First Nation or Métis communities can make claims at any time, and other developments can occur that could result in additional communities being affected by or interested in your undertaking.

Through Aboriginal Affairs and Northern Development (AANDC), the Government of Canada sometimes receives claims that Ontario does not receive, or with which Ontario does not become involved. AANDC's Consultation and Accommodation Unit (CAU) established a "single window" to respond to requests for baseline information held by AANDC on established or potential Aboriginal Treaty and rights. To request information from the Ontario Subject Matter Expert send an email to: [UCA-CAU@aadnc-aandc.gc.ca](mailto:UCA-CAU@aadnc-aandc.gc.ca)

Additional details about your project or changes to it that suggest impacts beyond what you have provided to date may necessitate further consideration of which Aboriginal communities may be affected by or interested in your undertaking. If you think that further consideration may be required, please bring your inquiry to whatever government body oversees the regulatory process for your project. MAA does not wish to be kept informed of the progress of the project; please be sure to remove MAA from the mailing list.

Yours truly,

A handwritten signature in dark ink, appearing to read "Heather Levecque". The signature is fluid and cursive, with the first name being more prominent.

Heather Levecque  
Manager, Consultation Unit  
Aboriginal Relations and Ministry Partnerships Division



**From:** Malkin, David <david.malkin@ncc-ccn.ca>  
**Sent:** April-02-14 4:01 PM  
**To:** Anita Sott; Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Manfred Goetz  
**Cc:** Tim Dickinson; Hutchison, Jason; Mulligan, Julie; Meek, Christopher  
**Subject:** RE: WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design  
**Attachments:** WP 4088-07-01 - Noise Barriers - Commencement - TAC.PDF

Anita, Dave, Kevin, Manny :

The NCC has a keen interest to continue to be involved in this file. I will be away on leave from April 18, 2014 until March 31, 2015 inclusively. Please ensure Julie, Jason and Christopher are included in correspondence during my absence.

Regards,

David Malkin, RPP, MCIP | urbaniste (OUQ), MICU  
 Senior Land Use Planner, Federal Land Use & Design Approvals  
 Capital Planning and Environmental Management Branch  
 National Capital Commission

Planificateur principal, utilisation du sol, Approbations fédérales d'utilisation du sol et de design  
 Direction de l'aménagement de la capitale et de la gestion de l'environnement  
 Commission de la capitale nationale

Email | Courriel: [david.malkin@ncc-ccn.ca](mailto:david.malkin@ncc-ccn.ca)  
 Tel | Tél. : 613-239-5678 ext. | poste 5103  
 202 - 40 rue Elgin Street, Ottawa Canada  
 K1P 1C7  
 \*\*\*\*\*

Initiate a Federal Approval application: [click here](#)  
 Amorcer une demande d'approbation fédérale : [cliquer ici](#)

Internet, English version: <http://www.ncc-ccn.gc.ca/>  
 Internet, version française : <http://www.ccn-ncc.gc.ca/>

---

**De :** Anita Sott [<mailto:SottA@mmm.ca>]

**Envoyé :** 8 mars 2013 11 h 38

**À :** Anita Sott

**Cc :** [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca); [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca); Manfred Goetz; Tim Dickinson

**Objet :** WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design

Dear Technical Advisory Committee Member,

The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for the Highway 417 Noise Barrier Retrofits (WP 4088-07-01). The purpose of this message is to inform you of the commencement of the project's detail design and provide you with an opportunity to offer comments and/or provide information in your capacity as a member of the Highway 417 Expansion Technical Advisory Committee.

Attached please find a letter from the Consultant Project Manager which provides a general overview of the project, as well as a comment form and details on how to provide any information, questions or comments you may have.



**HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN  
AND THE RIDEAU CANAL, DETAIL DESIGN (WP 4088-07-01)**

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**Design and Construction Report**

**APPENDIX D**  
**Public Correspondence**

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**From:**  
**Sent:** March-08-13 11:46 AM  
**To:** Anita Sott  
**Cc:** Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Manfred Goetz; Tim Dickinson  
**Subject:** Re: WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design

In my view the noise barriers should be in place along the Queensway in the downtown area. That should be a top priority.

---

**From:** Anita Sott [<mailto:SottA@mmm.ca>]  
**Sent:** Friday, March 08, 2013 11:29 AM  
**To:** Anita Sott <[SottA@mmm.ca](mailto:SottA@mmm.ca)>  
**Cc:** [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca) <[Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca)>; [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca) <[Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca)>; Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>; Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Subject:** WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design

Dear Sir/Madam,

The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for the Highway 417 Noise Barrier Retrofits (WP 4088-07-01). The purpose of this message is to inform you of the project and provide you with an opportunity to offer comments and/or provide information.

Attached please find a letter from the Consultant Project Manager, which provides a general overview of the project and details how you can get involved.

We look forward to your participation in this project.

Thank you,

**Anita Sott, M.A., M.PI.**

Planner, Planning and Environmental Design

on behalf of

**Tim Dickinson, M.PI., MCIP, RPP**

Associate, Planning and Environmental Design

**MMM Group Limited**

1145 Hunt Club Road, Suite 300

Ottawa, Ontario, Canada K1V 0Y3

t: 613-736-7200 x3546 | f: 613-736-8710 | c: 613-219-4760

[dickinsont@mmm.ca](mailto:dickinsont@mmm.ca) | [www.mmm.ca](http://www.mmm.ca)

**From:** Tim Dickinson  
**Sent:** March-18-13 7:27 AM  
**To:** Anita Sott  
**Subject:** Fwd: queensway noise barriers

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 18 March, 2013 6:42:09 AM EDT  
**To:** Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Cc:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>  
**Subject:** FW: queensway noise barriers

fyi

*Manney Goetz, P. Eng.  
McCormick Rankin  
A Member of the MCM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
K1V 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

**From:**  
**Sent:** Monday, March 11, 2013 11:28 AM  
**To:** Manfred Goetz  
**Subject:** Fwd: queensway noise barriers

----- Forwarded message -----

From:  
Date: Mon, Mar 11, 2013 at 11:27 AM  
Subject: queensway noise barriers  
To: [mgoetz@mrc.com](mailto:mgoetz@mrc.com)

The height and design of the barriers, including possibly raising the height or locating barriers where there currently aren't any, should take into account the increased number of residents and high rise buildings proposed in the new Carling-Preston CDP going forward to Ottawa Council later this month.

--



---

**From:** Tim Dickinson  
**Sent:** March-15-13 7:35 AM  
**To:** Anita Sott  
**Subject:** Fwd: Notice of Study Commencement for Highway 417 Noise Barriers

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 15 March, 2013 5:21:17 AM EDT  
**To:** Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Cc:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>  
**Subject:** FW: Notice of Study Commencement for Highway 417 Noise Barriers

FYI

*Manney Goetz, P. Eng.  
McCormick Rankin  
A Member of the MCM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
K1V 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

---

**From:**  
**Sent:** Wednesday, March 13, 2013 1:32 PM  
**To:** Manfred Goetz  
**Cc:** [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)  
**Subject:** Notice of Study Commencement for Highway 417 Noise Barriers

Dear Mr. Goetz

I am very pleased to see the Notice of Study Commencement for Highway 417 Noise Barriers (WP 4088-07-01), particularly the proposed southern section between Lyon and the Rideau Canal. I have been waiting 32 years to see the realization of this important capital project.

I assume that MOT will be requiring a standard concrete plank type wall in order to ensure economy of construction, consistency of local supply, and the ability to replace damaged components.

I would hope that the design will aim to significantly reduce current decibel output. While a noise barrier can achieve a 5 decibel noise level reduction when it is tall enough to break the line-of-sight from the roadway to the receiver location, after it breaks the line-of-sight it can achieve 1.5d decibels of additional noise level reduction for each metre of barrier height. My point here is that the number of transport vehicles using the 417 has steadily increased over the years and, unfortunately, the use of engine (Jake) breaks has drastically risen as well.

There is a need in the design to address this problem by increasing the height of the barrier well beyond the line-of-sight, possibly by two or three metres. Incorporating this additional height in the wall design would also compensate for the elevation of the Queensway. Since the highway is not at grade relative to the adjoining land, the noise created by the movement of traffic is amplified and broadcast over a greater distance. This must be addressed by increasing the height of the barrier wall well beyond the line-of-sight.

I look forward meeting you at the Public Information Centre. Can you give me an idea of when the PIC will be held?

**From:**  
**Sent:** March-14-13 2:16 PM  
**To:** Anita Sott  
**Cc:**  
**Subject:** RE: WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design

Dear Anita Sott,  
Thank you for sharing the notice regarding the retrofit of noise barriers associated with the Highway 417.

The Orchard Estates Community Association has no comment regarding the design or its review.

Regards,

Orchard Estates Community Association

---

**From:** Anita Sott [<mailto:SottA@mmm.ca>]  
**Sent:** March-08-13 11:30 AM  
**To:** Anita Sott  
**Cc:** [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca); [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca); Manfred Goetz; Tim Dickinson  
**Subject:** WP 4088-07-01 Highway 417 Noise Barrier Retrofits | Commencement of Detail Design

Dear Sir/Madam,

The Ministry of Transportation (MTO) has retained McCormick Rankin (MRC), a member of MMM Group Limited, to complete the detail design for the Highway 417 Noise Barrier Retrofits (WP 4088-07-01). The purpose of this message is to inform you of the project and provide you with an opportunity to offer comments and/or provide information.

Attached please find a letter from the Consultant Project Manager, which provides a general overview of the project and details how you can get involved.

We look forward to your participation in this project.

Thank you,

**Anita Sott, M.A., M.PI.**

Planner, Planning and Environmental Design

on behalf of

**Tim Dickinson, M.PI., MCIP, RPP**

Associate, Planning and Environmental Design

**MMM Group Limited**

1145 Hunt Club Road, Suite 300

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---

**From:** Tim Dickinson  
**Sent:** March-18-13 7:27 AM  
**To:** Anita Sott  
**Subject:** Fwd: 417 Noise Barriers

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 18 March, 2013 5:20:37 AM EDT  
**To:** Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Cc:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>  
**Subject:** FW: 417 Noise Barriers

fyi

*Manney Goetz, P. Eng.  
McCormick Rankin  
A Member of the MCM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
K1V 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

---

**From:**  
**Sent:** Saturday, March 16, 2013 2:53 PM  
**To:** Manfred Goetz; [davelindensmith@ontario.ca](mailto:davelindensmith@ontario.ca); [ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)  
**Subject:** 417 Noise Barriers

Dear Mr Goetz and Mr Lindensmith,

As a resident living less than 100m from Highway 417 in Ottawa I would like to encourage you to extend the current plan (WP 4088-07-01) to retrofit the noise barriers along the 417 to include the section of highway between Holland Ave and Preston St. The north side of the highway has already had it's noise barriers improved and as a result the noise on the south side is substantially



higher. A neighbour recently conducted a noise survey and had the highway noise recorded at over 60db. The current plan to retrofit sections of the existing noise barriers is inadequate. It makes more sense to do a thorough and complete retrofit now. Thank you for your consideration in this matter.

---

**From:** Tim Dickinson  
**Sent:** March-18-13 7:27 AM  
**To:** Anita Sott  
**Subject:** Fwd: Noise barrier Bronson Avenue to Lyon (WP 4088-07-01)

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 18 March, 2013 5:21:39 AM EDT  
**To:** Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Cc:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>  
**Subject:** FW: Noise barrier Bronson Avenue to Lyon (WP 4088-07-01)

fyi

*Manny Goetz, P. Eng.  
 McCormick Rankin  
 A Member of the MCM Group  
 300-1145 Hunt Club Road  
 Ottawa, Ontario  
 K1V 0Y3  
 P 613-736-7200 ext. 3225  
 F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

---

**From:**  
**Sent:** Saturday, March 16, 2013 4:31 PM  
**To:** Manfred Goetz; [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)  
**Subject:** Noise barrier Bronson Avenue to Lyon (WP 4088-07-01)

Hi Mr. Goetz and Mr. Lindensmith

I do not know if I have any concerns regarding this project. As I live on -- opposite side of Queensway to where the sound barrier will go up -- my first impression was that the sound barrier would be good for the other side so great. However, I have been told that there are two ways that sound barriers work. The first is that they can absorb sound so that it does not impact those on the other side of the barrier. The other way is to reflect sound back to where it came. If this is true, then I may have a concern as putting up a sound barrier that reflects the sound back could conceivably result in increased Queensway noise on my side of the Queensway.

Could you please tell me if there is any truth to the idea that the sound barrier could actually increase noise on my side of the street or direct this email to someone who could get back to me regarding this question?

Thanks

**From:**  
**Sent:** March-17-13 1:03 PM  
**To:** Anita Sott  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:

Subject: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

Message: I live at . We have noticed that since you have retrofitted the barriers on the other side of the queensway that there is more noise on this side. We encourage you to please extend the boundaries to include our area. Thanks

**From:**  
**Sent:** March-17-13 5:50 PM  
**To:** Anita Sott  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:

Subject: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

Message: Improved (higher, more absorbent, better deflection) noise barriers are required on the south side of the 417 between Preston and Parkdale.



---

**From:** Tim Dickinson  
**Sent:** March-20-13 7:18 AM  
**To:** Anita Sott  
**Subject:** Fwd: construction of new retrofit noise barriers on Young Street

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 20 March, 2013 6:00:56 AM EDT  
**To:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>, Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Cc:** "Lindensmith, Dave (MTO) ([Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca))"  
<[Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca)>, "'Ogilvie, Kevin' ([kevin.ogilvie@ontario.ca](mailto:kevin.ogilvie@ontario.ca))"  
<[kevin.ogilvie@ontario.ca](mailto:kevin.ogilvie@ontario.ca)>  
**Subject:** FW: construction of new retrofit noise barriers on Young Street

fyi

*Manney Goetz, P. Eng.  
McCormick Rankin  
A Member of the MCM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
K1N 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

---

**From:**  
**Sent:** Monday, March 18, 2013 2:25 PM  
**To:** Manfred Goetz  
**Cc:** [ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)  
**Subject:** construction of new retrofit noise barriers on Young Street

I have been living at (south/west of Preston Street) for the past 27 years and was totally devastated when our MPP Yasir Naqvi informed me that our part of Young Street was not included in the retrofits. I feel everyone who borders the queensway especially in the downtown core should have the same consideration as everyone else who have already been retrofitted. I urge you to please reconsider and add our section up to at least the Westgate

Shopping Centre in the retrofits. We need all the help we can get to help us with the noise and pollution we put up with on a daily basis.

If after our protests we are still not included then I want to receive a detailed report as to why not.

love to hear some feedback.

regards

---

**From:** Tim Dickinson  
**Sent:** March-19-13 12:41 PM  
**To:** Anita Sott  
**Subject:** Fwd: Queensway Noise Barriers Retrofits

Begin forwarded message:

**From:** "Lindensmith, Dave (MTO)" <[Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca)>  
**Date:** 19 March, 2013 12:20:40 PM EDT  
**To:** "Blaney, Chris (MTO)" <[Chris.Blaney@ontario.ca](mailto:Chris.Blaney@ontario.ca)>, "Ogilvie, Kevin (MTO)" <[Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca)>  
**Cc:** "[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)" <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>, "[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)" <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>, "Gibbs, Kevin (MTO)" <[Kevin.Gibbs@ontario.ca](mailto:Kevin.Gibbs@ontario.ca)>  
**Subject:** Fw: Queensway Noise Barriers Retrofits

Chris / Kevin...could you pls provide any messaging / background / results of previous noise. studies that mrc could use in formulating a draft reply.

Thanks...dave

---

**From:**  
**Sent:** Tuesday, March 19, 2013 11:47 AM  
**To:** Lindensmith, Dave (MTO)  
**Subject:** Fw: Queensway Noise Barriers Retrofits

Good morning Mr. Lindensmith  
Could you please see Minister Naqvi's notes below and let me know if you can accommodate a wider review area along the Queensway to include out neighbourhood, being that part to the north side of the Queensway between the canal and the Nicholas exchange?  
Thanks for you assistance.

Cheers

----- Forwarded Message -----

**From:** Naqvi\_Yasir-MPP-CO <[ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)>

**To:**

**Sent:** Friday, March 15, 2013 6:18:10 PM

**Subject:** RE: Queensway Noise Barriers Retrofits

Dear

Thank you for your email. I would like to stress that the areas currently being examined are approximate, and will be refined as the review progresses. I recommend that you to provide your comments below to the Ministry as part of this review, and encourage them to extend the boundaries to include your area. I will also be speaking to my colleague, the Honourable Glen Murray, Minister of Transportation, to ask his consideration of this request.

Sincerely,

Yasir

**Yasir Naqvi, MPP**

Ottawa Centre

**From:**

**Sent:** Sunday, March 10, 2013 2:01 PM

**To:** Naqvi\_Yasir-MPP-CO

**Subject:** Re: Queensway Noise Barriers Retrofits

Dear Minister Naqvi

Thank you for reply. While this is encouraging for other neighbourhoods nearby, it exposes exactly the issue that I have been trying to raise for the past two years. One project currently underway looks east towards the "Split" but does not include our area; the one you noted now looks west, but also does not include our area - we get left in the middle with no review. I tried to explain this situation to the MTO engineer in Kingston at the regional office but that seems to have gone nowhere. In the meantime, we have no coverage in much of our backyard, to inadequate where there is anything at all.

I also asked our City councillor to come and have a walk-about at the site to get a better appreciation, especially when we were in the process of replacing our own privacy fence, but that just didn't come to pass. Perhaps when you are home one of these weeks you could stop in and we can show you exactly what these issues look like on the ground.

Cheers

**From:** Naqvi\_Yasir-MPP-CO <[ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)>

**To:** Naqvi\_Yasir-MPP-CO <[ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)>

**Sent:** Sunday, March 10, 2013 1:41:18 PM

**Subject:** Queensway Noise Barriers Retrofits

Dear Neighbours,

The Ministry of Transportation has issued a Notice of Study Commencement for the noise barrier retrofits along the Queensway between the O-Train and the Rideau Canal . I have attached a copy of the notice here for your information.

The project includes the construction of new retrofit noise barriers at the following locations:

- O-Train to Preston Street (south side of Highway 417, facing Young Street );
- Bronson Avenue to Lyon Street (north side of Highway 417); and

- Lyon Street to the Rideau Canal (south side of Highway 417).

Note that these locations are approximate, and will be refined as the process continues.

The Ministry is now accepting comments and feedback regarding this study. This could include suggestions regarding the timeline, locations or the plans for construction. I encourage you to get involved and ensure your feedback is heard.

Please do not hesitate to contact me if you have any questions.

Sincerely,  
Yasir

**Yasir Naqvi, MPP** | Ottawa Centre |

Community Office | 109 Catherine Street , Ottawa , ON K2P 0P4  
[t] 613-722-6414 | [f] 613-722-6703 | [e] [ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)

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**From:**  
**Sent:** March-19-13 6:46 PM  
**To:** Anita Sott  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:

Subject: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

Message: Please note that just south of the 417 on Fairmont Avenue is exceptionally noisy, especially on higher levels of houses, which are above the levels of the current low sound barrier on the south side of the Queensway. It is interesting to note that for some reason, the north side has a far higher, more efficient barrier. I understand that the Fairmont section of the sound barrier is not currently scheduled to be upgraded. However, I would argue that there is a great need for this to be done.

Thank you for your consideration.

**From:**  
**Sent:** March-19-13 10:52 AM  
**To:** Anita Sott  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:

Subject: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

Message: As a new resident in recently constructed homes on \_\_\_\_\_, I would like to obtain additional information about this project and would like to be determine the process that will have to be initiated to expand this Noise Barrier Retrofit Project. I would suggest that the barrier should be expanded now to include the requirements of recent infill housing projects that have recently been completed on/or near the corner of Young and Champagne and are backing on Highway 417. Certainly this new need should be considered now to correct noise concerns that will require abatement in the future.

---

**From:** Tim Dickinson  
**Sent:** March-20-13 12:00 PM  
**To:** Anita Sott  
**Subject:** Fwd: FW:

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 20 March, 2013 11:51:00 AM EDT  
**To:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>, Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Subject:** FW:

FYI

*Manfred Goetz, P. Eng.  
 McCormick Rankin  
 A Member of the MCM Group  
 300-1145 Hunt Club Road  
 Ottawa, Ontario  
 K1N 0Y3  
 P 613-736-7200 ext. 3225  
 F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

**From:**  
**Sent:** Wednesday, March 20, 2013 11:49 AM  
**To:** Manfred Goetz; [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca); [ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)  
**Subject:**

Hi,

I am responding in regards to the 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal, Detail Design (WP 4088-07-01). It has been brought to my attention that the Study Commencement for this project has a westerly limit of of the O-Train rail line. This study should include the impact these barriers have on the residences further west along Young Street. I am a home owner residing where the current barriers end. We have seen the new barriers erected on the opposite of the 417 and have anticipated the

same to be done on our side. It has been challenging as the 417 traffic noise now bounces off the north side barriers making it much louder than it was before.

The noise and debris prevent windows to be opened for any extended period of time. Cleaning our windows is a weekly chore. Our west facing windows, which are the windows to our child's bedroom, are the noisiest. Our home is situated at the top of the hill, and with the current barriers so low, we are also affected by vehicular light pollution. This is exacerbated by the fact that the current barrier ends 3 metres . Extending this barrier would greatly reduce noise and light pollution to our neighbourhood. Additionally, extending the barrier to replace the existing chain link fence would give our growing family a sense of security and peace of mind.

I strongly urge you to reconsider the limits of the Noise Barrier Study to include the residents up to Reid Ave and moving even further west to Reid Park.

Sincerely,

--

**From:** Tim Dickinson  
**Sent:** March-25-13 7:06 AM  
**To:** Anita Sott  
**Subject:** Fwd: 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal, Detail Design WP 4088-07-01

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 25 March, 2013 5:24:28 AM EDT  
**To:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>, Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Subject:** FW: 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal, Detail Design WP 4088-07-01

FYI

*Manney Goetz, P. Eng.  
McCormick Rankin  
A Member of the MCM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
K1V 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

---

**From:**  
**Sent:** Friday, March 22, 2013 2:05 PM  
**To:** Manfred Goetz; [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca); [ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)  
**Subject:** 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal, Detail Design WP 4088-07-01

March 22, 2013

Hello,

I am writing with regards to the 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal, Detail Design WP 4088-07-01.



I am a resident at \_\_\_\_\_ the house closest to the Queensway  
\_\_\_\_\_. I am writing to urge you to extend the study to include the barriers west of the O-Train rail line. I find it very disappointing that the study should omit a section of the Queensway that is in need of upgrading, since it so greatly affects our neighbourhood.

As you know, new (higher) barriers have already been erected on the opposite side of the Queensway, and because of this, all of the 417 traffic noise now bounces off those barriers and comes at us with full force. In fact, the noise levels have actually INCREASED since those barriers were installed. We had expected that our side of the Queensway would be fitted with the new barriers in tandem with the North side, but so far that has not been the case.

The noise and dust are a real problem for us, since it stops us from keeping our doors and windows open in the summertime. It is impossible to keep our windows, shutters, steps and planters clean for any length of time. The traffic, punctuated by speeding vehicles and sirens and horns, generates enough noise at night that we have to use a white noise machine in our child's bedroom so that he may sleep more peacefully. And since our home is situated at the top of the hill, the headlights and taillights of cars on the Queensway actually shine through our second floor windows at night.

For these reasons, my family strongly urges you to reconsider the limits of the Noise Barrier Study to include our neighbourhood, and even beyond Reid Park, where currently the only thing between us and the Queensway is a standard chain link fence.

Thank you for your attention to this matter,

---

**From:** Tim Dickinson  
**Sent:** March-25-13 7:06 AM  
**To:** Anita Sott  
**Subject:** Fwd: Noise barrier Bronson Avenue to Lyon (WP 4088-07-01)

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 25 March, 2013 5:32:20 AM EDT  
**To:**  
**Cc:** "Lindensmith, Dave (MTO) ([Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca))"  
<[Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca)>, Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>, Steve Regel  
<[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>  
**Subject:** RE: Noise barrier Bronson Avenue to Lyon (WP 4088-07-01)

We received your e-mail and are in the process of formulating a response.

Thanks

*Manny Goetz, P. Eng.  
McCormick Rankin  
A Member of the MCM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
K1V 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

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**From:**  
**Sent:** Saturday, March 23, 2013 10:00 PM  
**To:** Manfred Goetz; [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)  
**Subject:** RE: Noise barrier Bronson Avenue to Lyon (WP 4088-07-01)

One of you may have already received the attached email, but may not yet have had time to respond. However, as I did not receive an acknowledgement I thought I should send another e-mail to make sure the email was actually sent to the correct spot.

---

**From:** Tim Dickinson  
**Sent:** March-25-13 7:07 AM  
**To:** Anita Sott  
**Subject:** Fwd: Noise Barriers

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 25 March, 2013 5:34:31 AM EDT  
**To:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>, Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Subject:** FW: Noise Barriers

FYI

*Manney Goetz, P. Eng.  
McCormick Rankin  
A Member of the MMM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
K1N 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

---

**From:**  
**Sent:** Sunday, March 24, 2013 10:52 AM  
**To:** Manfred Goetz; [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)  
**Cc:** [yvaqvi.mpp.co@liberal.ola.org](mailto:yvaqvi.mpp.co@liberal.ola.org)  
**Subject:** Noise Barriers

Sir,

My sister who lives on \_\_\_\_\_ was informed by her MPP that noise/sound barriers were being put in place along certain sections of the 417. Why are these areas any more special than the rest of the Queensway? I have lived in my house \_\_\_\_\_ for 34 years, and bought it with the assumption that sound barriers

were being installed. What a laugh that was! Tin does not rank among the best sound barriers and they were never high enough to compete with the transport trucks. Instead of the barriers being erected at road level, they should have been erected at the level of the houses in the area and therefore giving everyone the same level of noise reduction. In the last 10 years the noise has increased 100 fold and the air pollution has gotten much worse. Not only is there the fire, police and ambulance sirens, transport truck air brakes and very noisy motorcycles there is also the children from the Fisher Park school standing on the cross-over walkway wanting the transport drivers to honk their horns. I sure that your first thought is why does she not sell. Well, I have tried that several time without much luck and unless I want to give it away and lose the equity built up in my home, I am stuck.

I am sending this email to you in the hope that perhaps you might be able to send this on to the powers that be in the Ministry of Transportation or to the group that are handling these sound barriers and let them know that all houses backing onto the 417 are putting up with a great deal of noise and pollution.

Thanking you for any assistance you may provide.

Sincerely,

**From:**  
**Sent:** March-28-13 7:48 PM  
**To:** Anita Sott  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:

Subject: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

Message: my neighbours and myself are so happy that the long awaited barriers are FINALLY coming, after all these years. life for us will be much improved, as the rise in Queensway traffic has been most noticeable and most of us need to wear earplugs to sleep during summer months when windows are open. I highly recommend the concrete barriers. I have seen them in many other cities and they seem to hold up the best, AND they can be artistically adorned with embossed designs and such. but mainly, I just want the barriers AS SOON AS POSSIBLE! we have been waiting so patiently for so long. thank you,



**From:** Tim Dickinson  
**Sent:** April-04-13 11:34 AM  
**To:** Anita Sott  
**Subject:** Fwd: Queensway Noise Barriers Retrofits  
**Attachments:** MTO Queensway Sound Barriers Letter.pdf; ATT00001.htm

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 4 April, 2013 10:58:24 AM EDT  
**To:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>, Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Subject:** FW: Queensway Noise Barriers Retrofits

fyi

Manny Goetz, P.Eng.  
McCormick Rankin  
A Member of the MMM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
K1V 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)

-----Original Message-----

From:  
Sent: Thursday, April 04, 2013 10:56 AM  
To: Manfred Goetz  
Cc: [ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org); [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca);

Subject: Queensway Noise Barriers Retrofits

Dear Mr. Goetz,

Attached is a letter from the Glebe Community Association's Traffic Committee supporting the Queensway Noise Barriers project.

Kind regards,



April 4, 2013

Mr. Manny Goetz, P.Eng.  
Consultant Project Manager  
McCormick Rankin  
1145 Hunt Club Road, Suite 300  
Ottawa, ON K1V 0Y3

By e-mail to: [mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)

**Subject: Commencement of detail design highway 417 noise barrier retrofits between the o-train and the Rideau Canal (WP 4088-07-01)**

Dear Mr. Goetz:

This letter is in response to the Ministry of Transportation's notification that detail design work is scheduled to begin shortly for the construction of retrofit noise barriers to be built on the section of the Queensway bordering the Glebe from Lyon Street to the Rideau Canal. The Glebe Community Association (GCA) wishes to express our strong support for the construction of these barriers. We have actively advocated for noise barriers for Glebe residents living near the Queensway since 2005. As it has been nearly eight years since it was determined that this stretch of the Queensway qualifies for such noise mitigation, we trust that this commencement of detail design work means that these barriers will soon be constructed, hopefully in 2014.

Given the Community Association's keen interest in this project, we will appreciate you keeping the GCA informed of developments and community consultations associated with this project.

The Glebe Community Association is one of the oldest and largest community associations in Canada. We represent a 10,000 household-strong community in the heart of the nation's capital. The GCA has a very active Traffic Committee, which is committed to supporting the Ministry of Transportation and its consulting engineers on this project through community feedback and consultation. I would kindly ask that you include the Chair of the GCA Traffic Committee, \_\_\_\_\_, in all public correspondence issued in connection with this project.

Many thanks for the welcome news of the commencement of this long overdue noise mitigation project.

Kind regards,

President  
Glebe Community Association  
[gca@glebeca.ca](mailto:gca@glebeca.ca)

cc: Yasir Naqvi, MPP  
Dave Lindensmith, Ministry of Transportation ([dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca))

---

**From:** Tim Dickinson  
**Sent:** April-10-13 6:05 AM  
**To:** Anita Sott  
**Subject:** Fwd: HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU CANAL

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 10 April, 2013 7:01:16 AM ADT  
**To:** "Lindensmith, Dave (MTO) ([Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca))"  
<[Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca)>, "'Ogilvie, Kevin' ([kevin.ogilvie@ontario.ca](mailto:kevin.ogilvie@ontario.ca))"  
<[kevin.ogilvie@ontario.ca](mailto:kevin.ogilvie@ontario.ca)>  
**Cc:** Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>, Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>  
**Subject:** FW: HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU CANAL

Tim,

There is an existing corrugated metal wall in this location.

Thanks

*Manney Goetz, P. Eng.  
McCormick Rankin  
A Member of the MCM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
KW 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

**From:**  
**Sent:** Monday, April 08, 2013 9:41 PM  
**To:** Manfred Goetz  
**Subject:** HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU CANAL

I notice that these plans retrofit noise barriers on the north side of the 417 between Bronson and Lyon, but not the south side. Is there any consideration for a retrofit on the south side of the 417 between those two points? There is a stub of new barrier being built with the Bronson offramp reconstruction.

**From:** Tim Dickinson  
**Sent:** April-15-13 10:27 AM  
**To:** Anita Sott  
**Subject:** FW: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal:  
**Attachments:** The Ministry of Transport recently issued a Notice of Study for the noise barrier retrofit along the Queensway between the O.pdf

---

**From:**  
**Sent:** April 10, 2013 8:24 PM  
**To:** Lindensmith, Dave (MTO); [mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal:

Good evening,

I am writing in reference to the Notice of Study Commencement for the Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal. Please find attached a PDF of my correspondence.

Thank you,



The Ministry of Transport recently issued a Notice of Study for the noise barrier retrofit along the Queensway between the O-Train and the Rideau Canal. While Young street is not currently included in the locations identified, I am hopeful it might be in the months to come.

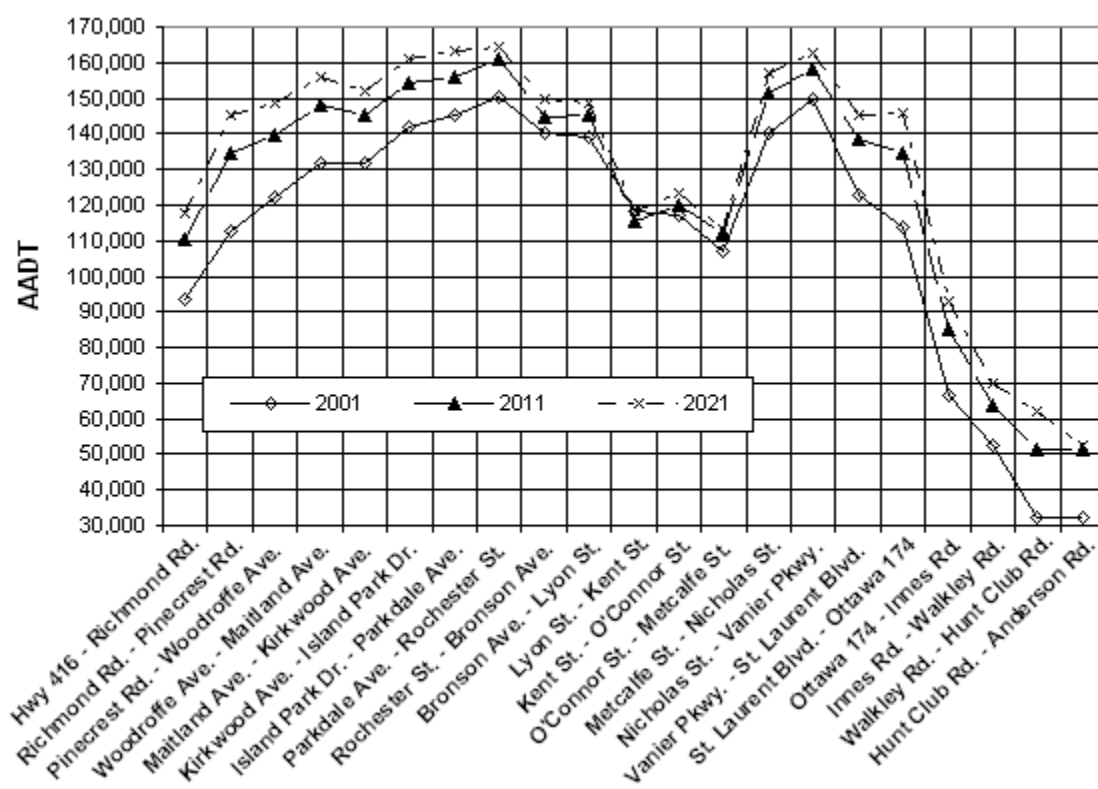
I am writing in hopes of encouraging the Ministry of Transport to extend the boundaries to include my area for the following reasons:

#### Proximity to the 417 Queensway

Traffic volumes on Highway 417 are high and are increasing annually. The Ministry of Transport records show that the section of the Queensway 417 between Parkdale Ave. and Rochester St. experiences high volumes of annual average daily traffic (AADT) (173 200 in 2009<sup>1</sup>).

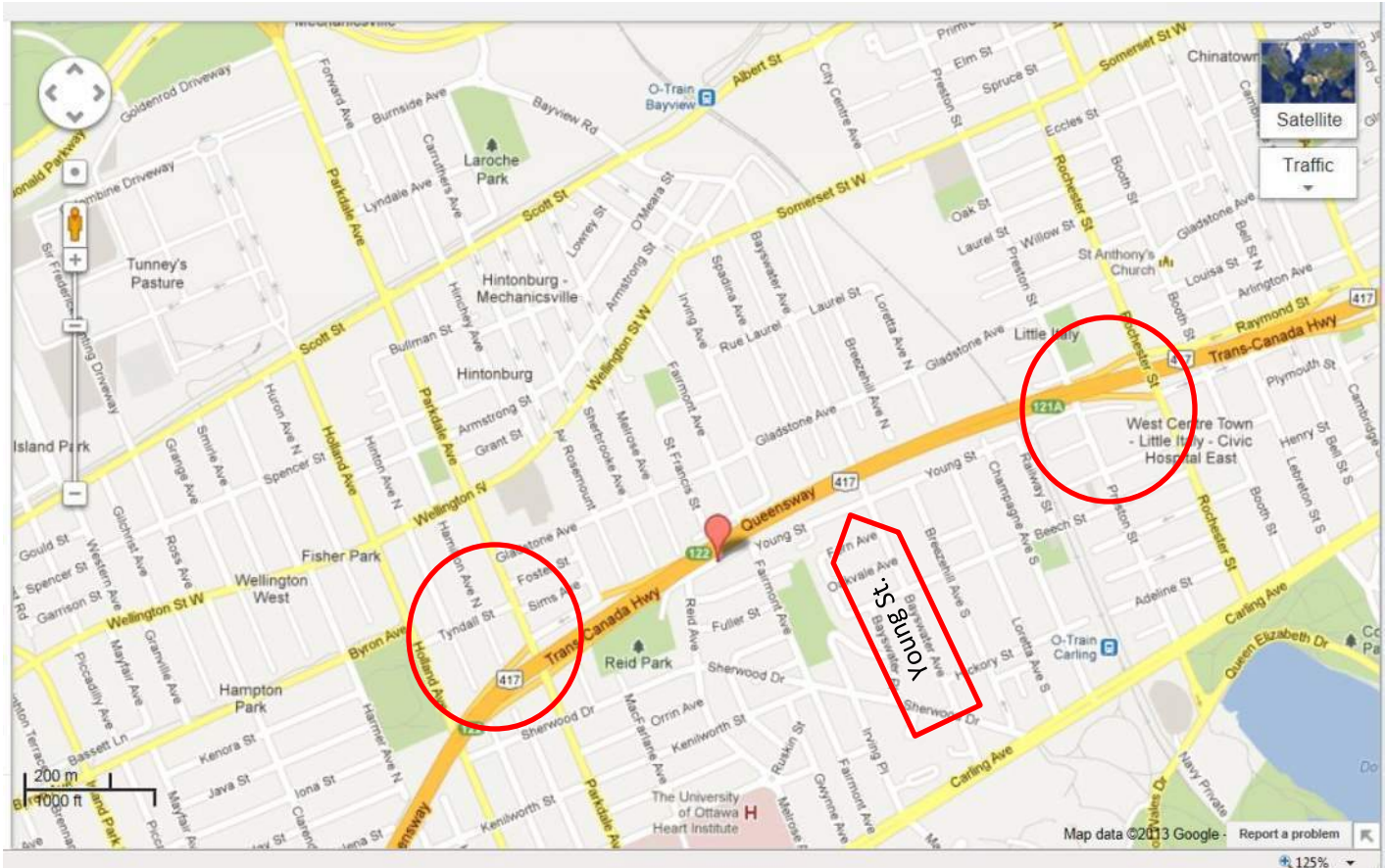
Ottawa's expected population growth outside the Greenbelt is 604,000 by 2021, an increase of 321,000 people from 2001 levels<sup>2</sup>. Continuing expansion of communities to the west, south and east sides of Ottawa will result in additional traffic demands in the Queensway corridor. It is projected that traffic volume will increase 1.8% annually over the next eight years (to 2021) between the Parkdale Ave. /Rochester St area.

#### *Highway 417 Mainline Current and Projected AADT*



<sup>1</sup> <http://www.ragsb.mto.gov.on.ca/techpubs/TrafficVolumes.nsf/tvweb?OpenForm&Seq=1>

<sup>2</sup> <http://www.mto.gov.on.ca/english/engineering/417ea/index.shtml>



Source - <http://www.mto.gov.on.ca/english/engineering/417ea/traffic/volumes.shtml>

Young Street faces North onto the 417 Queensway and is situated within the section of the highway between Parkdale Ave. and Rochester St.

### Noise Pollution Caused by Traffic

Recently (August 1, 2012) the Gradient Microclimate Engineering Inc, completed a study to assess traffic noise for \_\_\_\_\_ in Ottawa, a site for which a condominium was proposed. The results of the study were published and are available to the general public online<sup>3</sup>.

The images below show \_\_\_\_\_ proximity to the 417 Queensway. The third photo shows that \_\_\_\_\_ is approximately 28m from the 417 Queensway.

Similarly Young street is approximately 20m from the 417 Queensway.

The result of the Gradient Microclimate Engineering Inc. study indicates that noise levels due to roadway traffic at range between 64 and 81 dBA during the daytime period (07:00-23:00) and between 56 and 72 dBA during the nighttime period (23:00-07:00).

The City of Ottawa's Environmental Noise Control Guideline (ENCG) specifies that noise levels between 45 dBA and 40 dBA are tolerable for living and sleeping<sup>4</sup>.

Given the similar/nearly exact proximity of the two locations and Young street) to the Queensway (Young street being approximately 8 m closer to the Queensway) the results of the Gradient Microclimate Engineering Inc study are significant in explaining the noise levels experienced on Young street.

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<sup>4</sup> <http://ottawa.ca/sites/ottawa.ca/files/migrated/files/con029033.pdf>



## Traffic Induced Vibrations

Vibrations caused by traffic have a negative impact on a house by contributing to the process of deterioration<sup>5</sup>. In some cases, when a building is subjected to vibration for many years, fatigue damage may occur if the induced stresses in the building are high enough. In addition to the damage caused directly by vibrations, indirect damage may result from differential movements caused by soil settlement due to densification.<sup>6</sup>

**TABLE 1**  
**Suggested Effect of Traffic Vibrations on Masonry Buildings**  
(Adapted from Bata<sup>5</sup>)

Category	Average Acceleration, g (mm/s <sup>2</sup> )	Location of Building	Traffic Density (veh/day)	Danger of Crack Origin
a	<0.005	on secondary road	—	none
b	0.005-0.010	>10m from main road	<2000	none in next few decades
c	0.005-0.010	near main road	>2000	probable in next few decades
d	0.010-0.020	near main road	>2000	probable in next 1 or 2 decades
e	>0.020	near main road	>2000	certain within next few years

Source - <http://archive.nrc-cnrc.gc.ca/obj/irc/doc/pubs/nrcc21092/nrcc21092.pdf>

The vibrations also lend themselves to an unfavorable urban living environment. Vibrations may be unacceptable to occupants because of annoying physical sensations produced in the human body, interference with activities such as sleep and conversation, rattling of window panes and loose objects, and fear of damage to the building and its contents.

Experience shows that people living in houses are likely to complain if vibration levels are only slightly above the perception threshold, the major concern being fear of damage to the building or its contents.<sup>7</sup>

The most severe vibrations associated with road traffic result from heavy vehicles with stiff suspensions moving rapidly along roads with irregular surfaces<sup>8</sup>. Road traffic produces vibrations with frequencies predominantly in the range from 5 to 25 Hz (oscillations per second)<sup>9</sup>. Heavy trucks, particularly with Jake brakes engaged, can produce predominant frequencies in the range of 50 to 200 Hz<sup>10</sup>.

<sup>5</sup> [http://archive.nrc-cnrc.gc.ca/obj/irc/doc/ctu-n39\\_eng.pdf](http://archive.nrc-cnrc.gc.ca/obj/irc/doc/ctu-n39_eng.pdf)

<sup>6</sup> Ibid

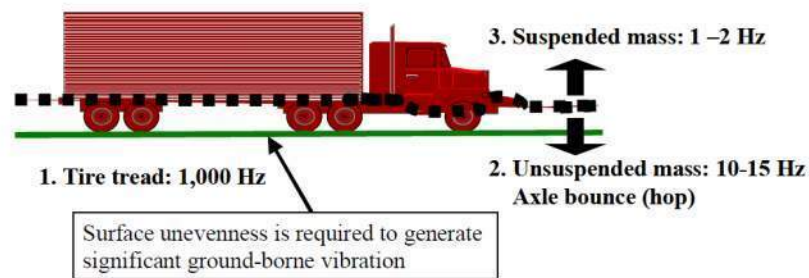
<sup>7</sup> Ibid

<sup>8</sup> <http://www.environment.nsw.gov.au/resources/noise/vibrationguide0643.pdf>

<sup>9</sup> [http://archive.nrc-cnrc.gc.ca/obj/irc/doc/ctu-n39\\_eng.pdf](http://archive.nrc-cnrc.gc.ca/obj/irc/doc/ctu-n39_eng.pdf)

<sup>10</sup> <http://www.tac-atc.ca/english/resourcecentre/readingroom/conference/conf2006/docs/s016/hajek.pdf>

*Impact forces linked to the fundamental frequency of trucks.* At highway speeds, a typical 5-axle tractor semi-trailer has the fundamental frequency of the suspended mass of about 1 or 2 Hz. Thus, the suspended mass (the part of the truck supported by the suspension system) heaves up and down about 1 or 2 times per second as the truck moves at highway speeds. When the truck heaves down, its static weight on the pavement increases due to the dynamic motion component.



**Figure 3. Source of vibration caused by a truck**

Source - <http://www.tac-atc.ca/english/resourcecentre/readingroom/conference/conf2006/docs/s016/hajek.pdf>

The 417 Queensway in Ottawa has above-average heavy truck traffic due to dumps and quarries on both east and west ends of the city. Also, as the primary route in Ontario and as part of the transcontinental federal-provincial highway, truck traffic is elevated on the 417.

The section nearest Young can be seen firsthand at <http://traffic.ottawa.ca/map/intersections>. A retrofitted noise barrier could stand not only to reduce noise pollution, but also unfavorable and disruptive vibrations.

Thank you for your consideration,

---

**From:** Tim Dickinson  
**Sent:** April-26-13 11:25 AM  
**To:** Anita Sott  
**Subject:** Fwd: Highway 417 Noise Barrier Retrofits

Begin forwarded message:

**From:** Manfred Goetz <[MGoetz@mrc.ca](mailto:MGoetz@mrc.ca)>  
**Date:** 26 April, 2013 10:21:54 AM GMT+01:00  
**To:** Steve Regel <[SRegel@mrc.ca](mailto:SRegel@mrc.ca)>, Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Subject:** FW: Highway 417 Noise Barrier Retrofits

fyi

*Manny Goetz, P. Eng.  
McCormick Rankin  
A Member of the MMM Group  
300-1145 Hunt Club Road  
Ottawa, Ontario  
K1V 0Y3  
P 613-736-7200 ext. 3225  
F 613-736-8710  
[mgoetz@mrc.ca](mailto:mgoetz@mrc.ca)*

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**From:**  
**Sent:** Thursday, April 25, 2013 3:59 PM  
**To:** Manfred Goetz; [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca)  
**Subject:** Fwd: Highway 417 Noise Barrier Retrofits

In regards to your request for comments, please find attached an email that I previously submitted to Glen Murray, Minister of Transportation.

Cheers

Begin forwarded message:

**From:**  
**Subject:** Highway 417 Noise Barrier Retrofits  
**Date:** 24 April, 2013 8:45:05 PM EDT  
**To:** [gmurray.mpp@liberal.ola.org](mailto:gmurray.mpp@liberal.ola.org)

**Cc:** [ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)

**Bcc:**

The Honourable Glen Murray, Minister of Transportation,

Please be advised that I am a resident of \_\_\_\_\_ in Ottawa. I have recently purchased a home in an area now know as Little Italy. My home is amongst approximately 28 new residential homes that have recently been built in

It has come to my attention that the Province will soon be undertaking a Sound Barrier Project to mitigate the noise that is a result of high vehicular traffic on the Highway 417. Current designs (WP 4088-07-01) seem to indicate barriers are to be constructed in areas that are relatively unpopulated relative to the area that I, as well as my neighbours, reside. I would hope that the recent creation of the high number of new housing units, as well as the resultant increase in the quantity of property owners, will justify a new design that will mitigate the high noise levels. I trust the Minister recognizes the need for Intensification of development of housing in Ottawa. It is indeed apparent that initial planning for barrier retrofits had not anticipated the high number of new housing that has recently been constructed in the area West of the O-Train. This particular area is currently being protected by a low barrier that obviously not been intended to provide protection for residential housing.

In addition, I would like to make your Ministry aware of a potential Health and Safety issue that was noticed after taking possession of our property In December 2012. On more than one occasion, snow and Ice from fast moving trucks and/or normal SnowPlowing Operations, has narrowly missed my wife, and our vehicles when the height of the existing barrier has proven insufficient for the purpose of limiting the distribution of snow and ice from fast moving vehicles.

I hope that these concerns can quickly be addressed and that any future repairs to the Queensway can now include plans to remedy these obvious sound and safety deficiencies in our neighborhood.

Thank you for your consideration.

---

**From:** Tim Dickinson  
**Sent:** November-20-13 10:05 AM  
**To:** Anita Sott  
**Subject:** Fwd: Clarification/questions re: Queensway Expansion East project

Begin forwarded message:

**From:** "Gibbs, Kevin (MTO)" <[Kevin.Gibbs@ontario.ca](mailto:Kevin.Gibbs@ontario.ca)>  
**Date:** November 20, 2013 at 10:03:13 AM EST  
**To:** Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Cc:** "Ogilvie, Kevin (MTO)" <[Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca)>  
**Subject:** FW: Clarification/questions re: Queensway Expansion East project

FYI

---

**From:**  
**Sent:** 20 novembre 2013 09:55  
**To:** Gibbs, Kevin (MTO); [regels@mmm.ca](mailto:regels@mmm.ca); [mgoetz@mrc.ca](mailto:mgoetz@mrc.ca); Lindensmith, Dave (MTO)  
**Cc:**  
**Subject:** Clarification/questions re: Queensway Expansion East project

Greetings,

A neighbour recently forwarded me some information about the Oct. announcement of study for the Queensway Expansion East project.

Firstly, I would appreciate being added to your distribution list to ensure I receive future updates.



Secondly, I would appreciate confirmation/clarification that actual noise barriers (not just "snow fences") will be established along the North side of the highway to protect Centretown residents from the air and noise pollution coming off the highway.

The information presented [here](#) suggests :

>Construction of new noise walls on the south barrier walls and replacement of the existing snow fence on the north barrier walls west of the bridge;

However, information [here](#) indicates the potential for proper noise barriers:

≥ Construction of new retrofit noise barriers at the following locations:

- O-Train to Preston Street (south side of Highway 417);
- Bronson Avenue to Lyon Street (north side of Highway 417);
- Lyon Street to the Rideau Canal (south side of Highway 417);

Finally, the [information](#) regarding the Local realignment of Chamberlain Avenue to the north, to be opposite the eastbound off-ramp mentioned does not provide a clear picture of what the revised route may look like.

I look forward to any clarification/confirmation you may be able to provide, and very much hope that there will be proper noise barriers on the N. side. Currently – while Glebe residents are protected by high barrier walls, Centretown residents can watch the cars go by with no greater height than the guardrail.

Many thanks in advance,

---

**From:**  
**Sent:** January-27-14 12:02 PM  
**To:** Anita Sott  
**Subject:** timing of noise barrier

Hi Anita,  
Would you know the proposed timing of the noise barrier installation, from Lyon to Bronson, north side of 417? I didn't see this information in the link. Thank you,

---

**From:**  
**Sent:** Friday, January 24, 2014 3:11 PM  
**To:** 'Anita Sott'  
**Subject:** RE: WP 4089-07-01 | Highway 417 Bronson Avenue Interchange - Public Information Centre Comment

I learned about the noise barriers shortly after submitting the comment, but thank you for the information and link.

---

**From:** Anita Sott [<mailto:SottA@mmm.ca>]  
**Sent:** Friday, January 24, 2014 3:02 PM  
**To:**  
**Cc:** Gibbs, Kevin (MTO) ([Kevin.Gibbs@ontario.ca](mailto:Kevin.Gibbs@ontario.ca)); [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca); Steve Regel; Tim Dickinson  
**Subject:** FW: WP 4089-07-01 | Highway 417 Bronson Avenue Interchange - Public Information Centre Comment

Dear

**RE: PUBLIC INFORMATION CENTRE (PIC) – HIGHWAY 417 BRONSON AVENUE INTERCHANGE OPERATIONAL IMPROVEMENTS (WP 4089-07-01), DETAIL DESIGN**

---

On behalf of the Ministry of Transportation, we thank you for submitting a comment sheet at the PIC regarding the Highway 417 Bronson Avenue Interchange Operational Improvements project. Specifically, you requested that noise barriers be installed on the north side of Highway 417 around Bronson Avenue.

Please be advised that as part of the Highway 417 Noise Barrier Retrofits project (WP 4088-07-01), noise barriers are proposed to be installed on the north side of Highway 417 from Bronson Avenue to the Lyon Street westbound off-ramp. More information about this project is available on the project website at <http://queenswayexpansioneast.com/highway-417-noise-barrier-retrofits-between-the-o-train-and-the-rideau-canal/>.

Please note that your contact information will be updated in the project contact list and will be notified of the upcoming 30-day public review period of the Design and Construction Report. Should you require additional information, please feel free to contact the undersigned.

Thank you for your participation in this project.

Sincerely,

**Anita Sott, M.A., M.P.I.**

Planner, Planning and Environmental Design

On behalf of

**Steve Regel, P.Eng**  
Senior Project Manager  
Associate  
Highways Department

**MMM Group**  
t: 613.736.7200 x3228| f: 613.736.8710



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**From:**  
**Sent:** May-14-14 11:29 AM  
**To:** Anita Sott  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:

Subject: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

Message: The metal barrier along hwy 417 between Parkdale and Island Park Dr. is in desperate need of being replaced. The rusted metal barrier has holes everywhere and looks like it is in danger of falling over in some places.

When will this dangerous eye-sore going to be replaced. Small animals can actually get through onto the 417 hwy. through some of the rusted out holes in the barrier. (I actually say a dead squirrel on the hwy just before the Island Park Dr. exit ramp. last week.

Hope to hear that there will be some barrier replacements soon before a Hwy vehicle accident happens because of the dangerous deterioration of the metal wall between Parkdale and Island Park Drive.

Sincerely,

---

**From:** Tim Dickinson  
**Sent:** June-09-14 11:06 AM  
**To:** Anita Sott  
**Subject:** FW: Queensway Mid-town Bridges Comment / Inquiry

---

**From:** Gibbs, Kevin (MTO) [mailto:Kevin.Gibbs@ontario.ca]  
**Sent:** June-09-14 10:39 AM  
**To:** Steve Regel; Tim Dickinson  
**Cc:** Ogilvie, Kevin (MTO)  
**Subject:** FW: Queensway Mid-town Bridges Comment / Inquiry

Steve,

Can you please add to our contact list for the Retro Fit Noise Barrier job....

Thank you

Kevin

---

**From:** Rowat, Don (MTO)  
**Sent:** 9 juin 2014 10:33  
**To:** Gibbs, Kevin (MTO)  
**Cc:** Ogilvie, Kevin (MTO)  
**Subject:** FW: Queensway Mid-town Bridges Comment / Inquiry

Kevin G,

Please see below. Can you please have your Service Provider add to your contact list?

Thanks,

Don

---

**From:**  
**Sent:** June 6, 2014 5:27 PM  
**To:** Rowat, Don (MTO)  
**Cc:**  
**Subject:** Re: Queensway Mid-town Bridges Comment / Inquiry

Dear Mr. Rowat,

I appreciate your response - the detail and thought involved was terrific. I would like you to ask the Project Team for the noise barrier retrofit detail design project to add my name to their contact list and thank you for the suggestion.



Regards,

On 5 June 2014 15:21, Rowat, Don (MTO) <[Don.Rowat@ontario.ca](mailto:Don.Rowat@ontario.ca)> wrote:

Hello

Thank you for your inquiry regarding this project. The current study is not considering noise abatement except to maintain existing barriers within the adjacent sections to the bridges under study.

The Ministry may consider noise mitigation on a retrofit basis, for subdivisions constructed prior to 1977. Candidate sites under this program are identified and construction occurs on a priority basis. Subsequent to 1977, the issue of noise relating to proposed new developments is dealt with by the municipality through the development approvals process.

The Ministry currently has a noise barrier retrofit detail design project underway on the south side of Highway 417 from Lyon St easterly to the Rideau Canal. It is a separate project from this one however I can have that Project Team add your name to the contact list so you are notified of the Public Information Centre in the future.

The timing of construction for the retrofit noise barriers has not yet been confirmed and is subject to funding and approvals. The Ministry will endeavor to combine it with adjacent construction in order to minimize traffic disruption.

With respect to the noise abatement of the barriers along the freeway, they are most effective for the first rows of residential receivers. For houses that are located several blocks from the freeway, their effectiveness is reduced.

If you have any questions, please let me know.

Thanks,

**Don M. Rowat, P.Eng**

Senior Project Engineer

Ontario Ministry of Transportation

Planning and Design - Eastern Region

1355 John Counter Blvd., Kingston, Ontario K7L 5A3

Tel: [613-545-4723](tel:613-545-4723) Fax: [613-540-5106](tel:613-540-5106)

[don.rowat@ontario.ca](mailto:don.rowat@ontario.ca)

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**From:**

**Sent:** June 2, 2014 10:02 AM

**To:** Rowat, Don (MTO)

**Subject:** Queensway Mid-town Bridges Comment / Inquiry

Hello,

I live 3 blocks from the Queensway. Sound is, as you can imagine, a 24 hour a day issue for anyone living near this road. I have a friend who has just moved into a home on one block from the Queensway where sound barriers have been installed and the noise at her home is significantly less than it is at my home where there are no sound barriers.

The number of residents living in the area where I live has increased substantially from the time when the Queensway was first constructed. Certainly, the zoning along the original train line used for construction of the Queensway might have justified leaving the highway exposed in the area since there were, at that time, commercial industrial installations located there. However, the use of the land has changed. Just in the last three years, several new apartment buildings and a large old age home are now in place on the south and north sides and facing the Queensway between the O'Connor and Metcalfe exits and the density of residents is increasing all the time with the location being so close to attractions like the Rideau Canal, Colonel By Drive and Queen Elizabeth Driveway and downtown.

What is the policy of the Queensway Mid-town Bridges project on installation of sound barriers as far as Elgin Street on both sides of the highway?

I look forward to your response.

---

**From:**  
**Sent:** June-18-14 10:36 AM  
**To:** Michel Bisson  
**Subject:** Question concernant les murs antibruit et l'élargissement du Queensway

Bonjour M. Bisson,

J'aimerais en savoir davantage sur une composante du projet de mise en place de murs antibruit dans le cadre de l'élargissement du Queensway Est.

Votre plan semble prévoir la mise en place de murs antibruit sur toute la longueur du côté sud du Queensway entre Lyon et le canal Rideau. Je viens de déménager dans une maison , et j'aimerais bien savoir s'il est prévu d'installer des murs antibruit dans ce secteur, puisqu'en ce moment il n'y en a pas le long de la rue Isabella et des bretelles d'entrée et de sortie du Queensway.

Serait-il possible de préciser où les murs antibruit seront installés?

Merci bien.

**From:**  
**Sent:** July-21-14 12:27 PM  
**To:** Anita Sott  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:  
Subject: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal  
Message: Hello,

I live backing onto the noise barrier (in Ottawa) South side between                      and                      .

Could you please advise as to when the south side will be receiving concrete noise barriers.

If this is not the correct email could you please forward my inquiry to the appropriate person.

Thanks,

---

**From:**  
**Sent:** August-20-14 6:58 AM  
**To:** Michel Bisson  
**Subject:** RE: Question concernant les murs antibruit et l'élargissement du Queensway

Bonjour,

Je me demandais si vous aviez eu une réponse à mes questions ?

Merci et bonne journée,



---

**From:**  
**Sent:** April-14-15 3:15 PM  
**To:** Anita Sott  
**Cc:** Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Steve Regel; Tim Dickinson; ynaqvi.mpp.co@liberal.ola.org  
**Subject:** Re: WP 4089-07-01 | Highway 417 Bronson Avenue Interchange - Public Informat

Ms Sott,

Many thanks for this update. It is unfortunate that the noise barriers could not have been combined with this summer's bridge replacement work at Kent Street since it's the same part of the Queensway and is the only project that I'm aware of planned by MTO for the foreseeable future (10+ years) which will require lane reductions on the section of the Queensway where the south-side retrofit noise barriers are to be installed (since the Bank Street and O'Connor overpasses are only slated for rehabilitation and not replacement).

Sincerest regards,

Glebe Community Association, Traffic Committee

Anita Sott <SottA@mmm.ca> writes:

**RE: HIGHWAY 417 BRONSON AVENUE INTERCHANGE IMPROVEMENTS (WP 4089-07-01) & HIGHWAY 417 NOISE BARRIER RETROFITS (WP 4088-07-01)**

On behalf of the Ministry of Transportation (MTO), thank you for submitting a comment regarding the Highway 417 Bronson Avenue Interchange improvements, Highway 417 Noise Barrier Retrofits, and Highway 417 Kent Street Overpass projects. Specifically, you requested an update on the projects and approximate timing of any future information sessions or construction of the project works.

The Highway 417 Bronson Avenue Interchange improvements project is currently undergoing detail design. A Public Information Centre (PIC) for this project was held on April 3, 2013. No other PICs are planned for this project. The City of Ottawa is developing plans for the construction of an underground combined sewer tunnel which will use the former Ottawa Board of Education building site / MTO property as a staging / launching area for the tunnel boring machine following the demolition of the existing building. Consequently, the construction of the proposed works for this project cannot begin until the tunnel project is completed, likely in 2017.

The Highway 417 Noise Barrier Retrofits project (WP 4088-07-01) is currently undergoing detail design. A Public Information Centre (PIC) for this project will be taking place, however a specific date for the event has not been determined at this time. Construction timing for the noise barrier retrofits is dependent on provincial priorities and the availability of funding. As various lane and ramp closures will be required to complete the installation of noise barriers, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to OR 174. The construction of noise barrier retrofits may also be

combined with other road improvements in the Highway 417 corridor to the extent possible, in order to minimize traffic impacts.

The detail design and environmental assessment study for the Highway 417 Kent Street Overpass Replacement project (WP 4033-08-01) was completed in December 2014, and the Design and Construction Report (DCR) was made available for a 30-day public review between November 24, 2014 and December 23, 2014. The contract for this project is being tendered by MTO and construction is scheduled to begin in Spring 2015. The rapid bridge replacement is scheduled to take place over one weekend between July and September 2015.

Please be advised that you will be notified directly of any upcoming consultation opportunities for the above-noted projects, including the PIC for the Highway 417 Noise Barrier Retrofits and the 30-day public review of the DCRs for the Highway 417 Noise Barrier Retrofits and Bronson Avenue Interchange Improvements projects. Should you have any further questions, please feel free to contact the undersigned.

Thank you for your participation.

Sincerely,

**Anita Sott, M.A., M.Pl.**

Planner

Planning and Environmental Design

On behalf of

**Steve Regel, P.Eng.**

Senior Project Manager

Associate

Highways Department

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Comment 33

**From:**

**Sent:** January-22-15 4:18 PM

**To:** Anita Sott

**Cc:** Kevin.Gibbs@ontario.ca; Tim Dickinson; Steve Regel; Kevin.Ogilvie@ontario.ca

**Subject:** Re: WP 4089-07-01 | Highway 417 Bronson Avenue Interchange - Public Informat

Ms. Sott,

I'm resending this message from November 23rd. Very grateful for an update and approximate timing regarding the Retrofit Sound Barriers project as it's been nearly two years since the commencement notice for this went out.

Also grateful if you can confirm whether the the Kent Street overpass replacement will be taking place this year.

Many thanks,

writes:

Grateful for any update or approximate timing (e.g. for the next information session, if not the construction itself) you can provide regarding this project (Highway 417 Bronson Avenue Interchange Operational Improvements) **and also the Queensway (south side) Retrofit Sound Barriers (Lyon to the Rideau Canal) project.**

I have a Traffic Committee meeting tomorrow (Monday) evening and so would welcome any news that anyone copied on this e-mail could provide sometime on Monday.

Many thanks,

Chair, Glebe Community Association Traffic Committee

Comment 32

writes:

Many thanks for this positive update regarding the reconfiguration of Imperial and its connection to Chamberlain. The Community is highly appreciative of this change. When do you expect the 30 day public review period will begin and will there be a second open house as was originally planned? And big question - when is this work likely to happen - 2015?

Best regards,

Anita Sott <[SottA@mmm.ca](mailto:SottA@mmm.ca)> writes:

**RE: PUBLIC INFORMATION CENTRE (PIC) – HIGHWAY 417 BRONSON AVENUE INTERCHANGE OPERATIONAL IMPROVEMENTS (WP 4089-07-01), DETAIL DESIGN**

On behalf of the Ministry of Transportation (MTO), thank you for submitting a comment sheet at the PIC regarding the Highway 417 Bronson Avenue Interchange Operational Improvements project. Specifically, you suggested having Chamberlain / Imperial yield into the 2-lane eastbound off-ramp extension, making Bronson / Imperial a right-angled intersection, and providing a traffic light at Bronson / Imperial.

In response to concerns received at the Public Information Centre (PIC) on Wednesday, April 3, 2013, the Ministry is recommending that the Chamberlain Avenue extension be reduced to 2 lanes from the 3-lane design originally presented at the PIC. The realigned portion of Chamberlain / Imperial adjacent to Drummond's gas station will meet the 2-lane Chamberlain extension at a tighter radius and require motorists to yield and merge onto Chamberlain to proceed east.

Further modifications to the intersection of Bronson and Chamberlain / Imperial, including the synchronization of traffic lights, are outside the scope of the current interchange improvements project. However, we have forwarded your comments to the City of Ottawa for consideration. Any additional comments or concerns regarding this intersection should be directed to Mr. Philippe Landry, Manager – Traffic Management & Operational Support, at the City of Ottawa at [philippe.landry@ottawa.ca](mailto:philippe.landry@ottawa.ca).

Please note that you will be notified directly of the upcoming 30-day public review period of the Design and Construction Report. Should you require additional information, please feel free to contact the undersigned.

Thank you for your participation in this project.

Sincerely,

**Anita Sott, M.A., M.Pl.**

Planner, Planning and Environmental Design

On behalf of

**Steve Regel, P.Eng**

Senior Project Manager

Associate

Highways Department

**MMM Group**

t: 613.736.7200 x3228| f: 613.736.8710

**From:**  
**Sent:** March-15-15 6:42 PM  
**To:** Anita Sott  
**Subject:** Highway 417 Kent Street Overpass Replacement

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:

Subject: Highway 417 Kent Street Overpass Replacement

Message: Please subscribe me to notifications relating to the advancement of the Kent St overpass, the noise barrier retrofit, and the other overpass reconstructions/replacements.  
thank you



-----Original Message-----

From:

Sent: June-28-15 7:45 PM

To: Steve Regel

Subject: Queensway noise barrier retrofit project between O-train and Rideau canal

Sir,

I understand that you are the project manager for the subject work, which I am keenly interested in, because we are very much inconvenienced by the Queensway noise, in the Lyon St - Rideau canal segment, where we live, and have eagerly awaited the relief that you are about to bring.

Your site indicates that updates and notices of public meetings will be posted prior to the beginning of the actual work. I have been unable to find any of those, but that might be attributable to my relative lack of internet skills. Would you be so kind as to forwarding any of this information that you may have already sent, but which I have not seen, to this e-mail address, and also including me in your distribution list, if that is possible?

Many thanks,

**From:** Lindensmith, Dave (MTO) <Dave.Lindensmith@ontario.ca>  
**Sent:** July-09-15 9:56 AM  
**To:**  
**Cc:** Steve Regel; Anita Sott; Lindensmith, Dave (MTO)  
**Subject:** RE: Contact Form Message.

Dear

Thank you for your interest in this study.

By copy of this e-mail I will request our study consultant to add your name to the mailing list for this study.

Regards,

David Lindensmith, P. Eng.  
Sr. Project Engineer  
MTO Planning & Design Section  
Eastern Region

**From:**  
**Sent:** July-08-15 3:58 PM  
**To:** Lindensmith, Dave (MTO)  
**Subject:** Fwd: Contact Form Message.

Dear Mr. Lindensmith:

Could you please add my name on the mailing list for the Public Information Center in the Fall 2015?

Thanks.

----- Forwarded message -----

**From:** Yasir Naqvi, MPP (Constituency Office) <[ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)>  
**Date:** Wed, Jul 8, 2015 at 3:52 PM  
**Subject:** RE: Contact Form Message.  
**To:**

Hello

Thank you for your email. I have inquired with staff and they have confirmed that MTO is currently undertaking the detailed design for retrofit noise barriers along the south side of Highway 417 in the vicinity of O'Connor Street. It is anticipated that a Public Information Center for this project will be held in Autumn 2015.

If the constituent would like to be added to the mailing list for the Detailed Design Study, they may contact David Lindensmith, Senior Project Engineer, at [613-540-5130](tel:613-540-5130) or [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca).

I hope this information is helpful. Please let me know if you have further questions.

**Jessica Dawson** | Constituency Assistant |

Office of Yasir Naqvi, MPP Ottawa Centre

[t] [613-722-6414](tel:613-722-6414) | [f] [613-722-6703](tel:613-722-6703) | [e] [ynaqvi.mpp.co@liberal.ola.org](mailto:ynaqvi.mpp.co@liberal.ola.org)

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-----Original Message-----

From:

Sent: Wednesday, July 08, 2015 3:44 PM

To: Naqvi, Yasir MPP CO

Subject: Contact Form Message.

First Name:

Message: I would like to know what is the status for the highway 417 noise barrier between O'Connor and Elgin/Queen Elizabeth Driveway? I always wonder how come this section of the highway was neglected in terms of noise issue especially there is an old age resident and several condos situated at the north and south of the highway. I was told the noise barrier on highway belongs to the provincial government. I would appreciate

that you could let me know if there is a plan eventually having the noise barrier be erected on highway 417 between O'Connor and Elgin/Queen Elizabeth Driveway.

Thank you for your message.

**From:** Lindensmith, Dave (MTO) <Dave.Lindensmith@ontario.ca>  
**Sent:** July-09-15 1:55 PM  
**To:**  
**Cc:** Steve Regel; Anita Sott  
**Subject:** RE: Detailed design study for the noise barrier retrofit on the south side of the 417 around Bank Street in Ottawa

Dear

Thank you for your e-mail and your interest in this project.

By copy of this response I will request that our design consultant add your name to the mailing list for this study.

Regards,

David Lindensmith, P. Eng.  
Sr. Project Engineer  
MTO Planning & Design Section  
Eastern Region

---

**From:**  
**Sent:** July-08-15 4:32 PM  
**To:** Lindensmith, Dave (MTO)  
**Cc:**  
**Subject:** Detailed design study for the noise barrier retrofit on the south side of the 417 around Bank Street in Ottawa

Dear Mr. Lindensmith,

Could you please add my e-mail address to your distribution list for information regarding the detailed design study for the noise barrier retrofit on the south side of the 417 around Bank Street in Ottawa? I live in that neighbourhood close to the Queensway and am very interested in the progress of this project.



**From:**  
**Sent:** August-05-15 3:00 PM  
**To:** Anita Sott  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:

Subject: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

Message: Good afternoon. Can you please, please, please reassure me that noise barriers will be installed on the North side of the Queensway? We just moved to a condo on \_\_\_\_\_, between Kent and Lyon. The noise from the Queensway is deafening. Thank you!

**From:**  
**Sent:** September-18-15 6:30 PM  
**To:** Anita Sott  
**Subject:** Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

You have received a message regarding Queensway Expansion East. Message details are as follows:

Name:  
Subject: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal  
Message: When is this expected to start?

**From:**  
**Sent:** September-30-15 9:31 PM  
**To:** Dave.Lindensmith@ontario.ca  
**Cc:** Tim Dickinson; Kevin.Ogilvie@ontario.ca; Steve Regel; Anita Sott  
**Subject:** Queensway Sound Barriers

Mr. Lindensmith,

I'm writing again on my favourite recurring topic - when will the retrofit sound barriers approved in 2005 for the downtown and Glebe sections of the Queensway be built? Our MPP, Yasir Naqvi's office reported to a resident this summer that the deferred open house, where the design plans for these barriers will be presented, will take place this autumn. I'm hoping you can confirm this with an approximate date as there is still no update appearing on the Queensway Expansion East project website.

Many thanks and kind regards,

**From:**  
**Sent:** October-06-15 9:40 AM  
**To:** Anita Sott  
**Cc:** Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Steve Regel; Tim Dickinson  
**Subject:** Re: RE: 417 Noise Barriers

It took you over 2 years to reply to my email! Only to tell me that you aren't going to do anything for any of the residents along 417 corridor. Pathetic doesn't even begin to describe the level of service you are "providing". I look forward to hearing why it took you almost 3 years to reply to my inquiry and why residents on one side of the 417 receive new sound barriers while those on the south side do not. Can I expect a response in 2017 or would you like to wait until 2018?

---

**From:** "Anita Sott" <SottA@mmm.ca>  
**Date:** Tue, Oct 6, 2015 at 9:29 AM  
**Subject:** RE: 417 Noise Barriers

---

**RE: HIGHWAY 417 NOISE BARRIER RETROFITS, DETAIL DESIGN (WP 4088-07-01)**

---

On behalf of the Ministry of Transportation (MTO), thank you for submitting a comment (March 2013) regarding the Highway 417 Noise Barrier Retrofits project. Specifically, you requested that the limits of the current project be extended to include the area on the south side of Highway 417 between Holland Avenue and Preston Street.

The existing noise barrier on the north side of Highway 417 between Melrose Avenue and Loretta Avenue consists of Durisol precast noise barriers which are designed to absorb noise rather than reflect it. As a result, these noise barriers are much more effective in minimizing reflected noise than the metal barriers they replaced.

Please be advised that the current noise barrier retrofits detail design project includes the construction of a noise barrier on the south side of Highway 417 between Preston Street and the O-Train corridor. However, the area on the south side of Highway 417 between Preston and Holland is outside of the limits of the current project.

Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in

the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the noise barriers on the south side of Highway 417 west of the O-Train corridor has not been determined at this time.

Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.

Please also be advised that the noise barriers constructed as part of the current detail design project and future noise barrier replacements within the Highway 417 corridor will generally consist of 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection.

A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined. You have been added to the project contact list and will be notified directly of future consultation opportunities including the PIC and 30-day public review of the Design and Construction Report.

Thank you for your participation in this project.

Sincerely,

**Anita Sott, MCIP, RPP**  
Planner

Planning and Environmental Design

On behalf of

**Steve Regel, P.Eng**  
Senior Project Manager

Associate

Highways Department

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**From:**  
**Sent:** October-06-15 10:46 AM  
**To:** Anita Sott; David Chernushenko; Naqvi\_Yasir-MPP-CO  
**Cc:** Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Steve Regel; Tim Dickinson  
**Subject:** Re: Queensway Noise Barriers Retrofits

Good morning Ms. Sott

As Dave Lindensmith might recount you, we are now back to the same place we were over three years ago when we asked for the review area to be extended in advance of the 417 work starting. These are not barriers in need of replacement as you note - it is a residential area left uncovered years ago and never corrected. Both our City councillor and MPP were involved at that time and promised to support our request. We have seen retrofits underway elsewhere in the region and across the Province since that time, and many, like those around Brockville, not directly impacted by construction noise and dust.

Since the City and the Province have decided not to enforce no-engine-breaking areas nor to enforce current noise limits on motorcycles, speeding cars and trucks, we continue to face a barrage of noise 24/7. Further, we will be confronting more construction noise as the LRT climbs out of the tunnel and heads off to the south and east.

As well, the area is also no longer well protected by a security fence so our neighbourhood is also now subject to increased vandalism and vagrancy, with associated increases in petty thefts and now even attempted break-ins.

Could you please let us know what route we should now follow to get the priority of our request advanced?

Cheers

---

**From:** Anita Sott <SottA@mmm.ca>  
**To:**  
**Cc:** "Dave.Lindensmith@ontario.ca" <Dave.Lindensmith@ontario.ca>; "Kevin.Ogilvie@ontario.ca" <Kevin.Ogilvie@ontario.ca>; Steve Regel <RegelS@mmm.ca>; Tim Dickinson <DickinsonT@mmm.ca>  
**Sent:** Tuesday, October 6, 2015 9:29 AM  
**Subject:** RE: Queensway Noise Barriers Retrofits

Dea

**RE: HIGHWAY 417 NOISE BARRIER RETROFITS, DETAIL DESIGN (WP 4088-07-01)**

---

On behalf of the Ministry of Transportation (MTO), thank you for submitting a comment (March 2013) regarding the Highway 417 Noise Barrier Retrofits project. Specifically, you requested that the limits of the current project be extended to include the north side of the Queensway between the Rideau Canal and the Nicholas Street interchange.

Please be advised that the area on the north side of the Queensway between the Rideau Canal and the Nicholas Street interchange is outside of the limits of the current Noise Barrier Retrofits detail design project. Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if the noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of the north side Highway 417 noise barriers between the Rideau Canal and the Nicholas Street interchange has not been determined at this time.

Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing of the work to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.

A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined. You have been added to the project contact list and will be notified directly of future consultation opportunities including the PIC and 30-day public review of the Design and Construction Report.

Thank you for your participation in this project.

Sincerely,

**Anita Sott, MCIP, RPP**

Planner

Planning and Environmental Design

On behalf of

**Steve Regel, P.Eng**

Senior Project Manager

Associate

Highways Department

**MMM Group Limited**

t: 613.736.7200 x3228 | f: 613.736.8710

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---

**From:**  
**Sent:** October-06-15 10:32 AM  
**To:** Anita Sott  
**Subject:** Re: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

Anita,

Thanks very much for the update, although I am, obviously, disappointed. Still, I understand that these projects take time and money. Hopefully, the city will get to Fairmont Avenue eventually.

Regards,

On Tue, Oct 6, 2015 at 9:30 AM, Anita Sott <[SottA@mmm.ca](mailto:SottA@mmm.ca)> wrote:

Dear

**RE: HIGHWAY 417 NOISE BARRIER RETROFITS, DETAIL DESIGN (WP 4088-07-01)**

---

On behalf of the Ministry of Transportation (MTO), thank you for submitting a comment (March 2013) regarding the Highway 417 Noise Barrier Retrofits project. Specifically, you suggested that there is a need to upgrade the noise barriers on the south side of Highway 417 around Fairmont Avenue.

Please be advised that the current Noise Barrier Retrofits detail design project includes the construction of a noise barrier on the south side of Highway 417 between Preston Street and the O-Train corridor. However, the remaining area on the south side of Highway 417 west of the O-Train corridor is outside the limits of the current detail design project.

Under the Ministry's Queensway improvement plan, all existing metal noise barriers throughout the Highway 417 corridor are planned for replacement over time. Replacements will be completed if existing noise barriers are impacted by construction works in the highway corridor or if they are at the end of their service life, and will be designed to meet the Ministry's current design standard at the time of construction. As such, a date for the replacement of noise barriers on south side of Highway 417 at Fairmont Avenue has not been determined at this time. Please note that the timing of construction for the current noise barrier retrofits detail design project, and future noise barrier replacements, is dependent on provincial priorities and the availability of funding. Construction will be combined with other road improvements in the corridor to the extent possible in order to minimize traffic impacts. As lane and ramp closures will be required for construction of the noise barrier retrofits, the Ministry will plan construction timing to avoid disruptions to the ongoing construction of the Highway 417 expansion from the Nicholas Street interchange to Ottawa Road 174.

The existing noise barrier on the north side of Highway 417 between Melrose Avenue and Loretta Avenue consists of Durisol precast noise barriers which are designed to absorb noise rather than reflect it. The noise barriers constructed as part of the current detail design project and future noise barrier replacements within the Highway 417 corridor will generally consist of 5 metre high concrete barriers with absorptive wall faces for improved noise attenuation to adjacent residential areas. This type of noise barrier results in minimal noise reflection. The noise attenuation achieved through the installation of noise barriers is highest for the first row of residential receivers. The effectiveness of noise barriers is

reduced for residences that are located several blocks from the highway. Furthermore, the level of noise attenuation achieved is measured from the outdoor living area (OLA) at a residence considered to be a Noise Sensitive Area, and not from upper floors.

A Public Information Centre (PIC) for this project is being scheduled to take place in Fall 2015, although a date has yet to be determined. Please be advised that you have been added to the project contact list will be notified directly of future consultation opportunities including the PIC and 30-day public review of the Design and Construction Report.

Thank you for your participation in this project.

Sincerely,

**Anita Sott, MCIP, RPP**  
Planner

Planning and Environmental Design

On behalf of

**Steve Regel, P.Eng**  
Senior Project Manager

Associate

Highways Department

**MMM Group Limited**

t: [613.736.7200](tel:613.736.7200) x3228 | f: [613.736.8710](tel:613.736.8710)

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---

**From:**  
**Sent:** October-06-15 1:10 PM  
**To:** Anita Sott  
**Cc:** Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Steve Regel; Tim Dickinson  
**Subject:** Re: Highway 417 Noise Barrier Retrofits between the O-Train and the Rideau Canal

Thank you, Ms. Sott.

As we live on between Kent and Lyon, perhaps the barriers identified in your second bullet point (North Side between Bronson and Kent) will do the job.

I will endeavor to attend the consultation session.

Have a great day.

On Oct 6, 2015, at 9:35 AM, Anita Sott <[SottA@mmm.ca](mailto:SottA@mmm.ca)> wrote:

Dear

**RE: HIGHWAY 417 NOISE BARRIER RETROFITS, DETAIL DESIGN (WP 4088-07-01)**

On behalf of the Ministry of Transportation (MTO), thank you for submitting a comment regarding the Highway 417 Noise Barrier Retrofits project. Specifically, you inquired about the installation of noise barriers on the north side of the Queensway.

Please be advised that the noise barrier retrofits proposed as part of the current detail design project were approved as part of the Preliminary Design and Environmental Assessment (EA) Study for the Highway 417 Expansion from Highway 416 easterly to Anderson Road, which was documented in a Transportation Environmental Study Report (TESR) and received environmental clearance in 2008. The purpose of the current detail design project is to prepare the approved plan for implementation. As part of the current detail design project, a Noise Assessment was conducted in May 2014 and recommended that noise barrier retrofits be constructed in the following locations:

- On the south side of Highway 417 between the O-Train corridor and Preston Street;
- On the north side of Highway 417 between Bronson Avenue and Kent Street; and
- On the south side of Highway 417 between Lyon Street and the Rideau Canal.

In 2005, the Ministry conducted the Highway 417 Centretown Noise Study which did not recommend noise barrier retrofits on the north side of the highway covering the entire area between Bronson Avenue and the Rideau River for the following reasons:

- Installation of noise barriers would not achieve a reduction in noise levels of 5 dBA, which represents the minimum requirement for noise barrier retrofits to be considered under the MTO Retrofit Noise Barrier Program;
- A potential loss of highway visibility would result for businesses in the area;
- A tunnel-like effect would be created in the central area of Ottawa; and
- Noise barrier retrofits in this area represent significant cost of construction for modest benefits.



Please also be advised that the noise attenuation received through the installation of noise barriers is highest for the first row of receivers. The effectiveness of noise barriers is reduced for residences that are located several blocks from the highway.

A Public Information Centre (PIC) is being scheduled to be held in Fall 2015, although a date has not yet been determined. You have been added to the project contact list and will be notified directly of future consultation opportunities including the PIC and 30-day public review of the Design and Construction Report.

Thank you for your participation in this project.

Sincerely,

**Anita Sott, MCIP, RPP**

Planner

Planning and Environmental Design

On behalf of

**Steve Regel, P.Eng**

Senior Project Manager

Associate

Highways Department

**MMM Group Limited**

t: 613.736.7200 x3228 | f: 613.736.8710

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---

**From:** Lindensmith, Dave (MTO) <Dave.Lindensmith@ontario.ca>  
**Sent:** November-17-15 5:11 PM  
**To:**  
**Cc:** Tim Dickinson; Anita Sott; Ogilvie, Kevin (MTO); Steve Regel  
**Subject:** RE: Queensway Sound Barriers

Hi

We have been working through some construction staging issues and need to have these sorted out prior to the PIC.

MTO avoids scheduling PICs in December as this is a very busy time of year for most people and the participation in the PIC and input received suffers as a result. The PIC will be scheduled for late January 2016. This will allow us to place the ads in the newspapers and contact those on the study mailing list in early January, once the Christmas rush has passed.

I apologize for any inconvenience caused.

Regards,

Dave

---

**From:** \_\_\_\_\_  
**Sent:** November-14-15 5:54 PM  
**To:** [RegelS@mmm.ca](mailto:RegelS@mmm.ca)  
**Cc:** Ogilvie, Kevin (MTO); Lindensmith, Dave (MTO); [DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca); [SottA@mmm.ca](mailto:SottA@mmm.ca)  
**Subject:** Re: Queensway Sound Barriers

Comment 50

Steve,

Are you still planning this meeting for November 24th? We would like to give the community at least one week's notice about this open house.

Thanks,

Glebe Community Association

Steve Regel <[RegelS@mmm.ca](mailto:RegelS@mmm.ca)> writes:

Please disregard below, the date we looking at is November 24, 2015

**Steve Regel, P.Eng**

Senior Project Manager

Associate

Highways Department

**MMM Group Limited**

1145 Hunt Club Road, Suite 300

Ottawa, ON Canada K1V 0Y3

t: 613.736.7200 x3228 | f: 613.736.8710

**From:** Steve Regel

**Sent:** November-02-15 7:31 AM

**To:**

**Cc:** Tim Dickinson; [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca); [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca)

**Subject:** RE: Queensway Sound Barriers

We are looking at a PIC on November 30, 2015

**Steve Regel, P.Eng**

Senior Project Manager

Associate

Highways Department

**MMM Group Limited**

1145 Hunt Club Road, Suite 300

Ottawa, ON Canada K1V 0Y3

t: 613.736.7200 x3228 | f: 613.736.8710

**From:** [redacted]  
**Sent:** October-30-15 5:42 PM  
**To:** Anita Sott  
**Cc:** Steve Regel; Tim Dickinson; [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca); [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca)  
**Subject:** Re: Queensway Sound Barriers

Has a date been set for the Fall 2015 PIC on this project? We have a community Traffic Committee meeting coming up shortly and I would like to advise them of the meeting date. I presume there will be at least 30 days notice of this meeting?

Many thanks,

Glebe Community Association

Comment 47

Many thanks for this update - I look forward to receiving the notification.

Anita Sott <[SottA@mmm.ca](mailto:SottA@mmm.ca)> writes:

Dear

Please be advised that a Public Information Centre (PIC) for this project is being scheduled to take place this Fall 2015, although a date has yet to be determined. You will be notified directly of the date, time and location of the PIC. The website will also be updated with the PIC information

Thank you,

**Anita Sott, MCIP, RPP**

Planner  
 Planning and Environmental Design

On behalf of

**Steve Regel, P.Eng**

Senior Project Manager  
 Associate  
 Highways Department

**MMM Group Limited**

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**From:**  
**Sent:** October-29-15 2:08 PM  
**To:** Anita Sott  
**Cc:** Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Steve Regel; Tim Dickinson  
**Subject:** RE: Notice of Study Commencement for Highway 417 Noise Barriers

Dear Ms Sott

Thank you for your update. I look forward to the Public Information Centre scheduled during Fall 2015. I was informed by the onsite contractor responsible for the MTO bridge replacement project at Kent and the Queensway (417) that they had installed the footings for the noise barrier retrofit. While this is positive news I am concerned that footings were not installed along the 417 egress at Kent street. The continuity of noise barriers along the egress towards ground level at Chamberlain is critical in order to minimize the sound of traffic during deceleration as it achieves the 50 km per hour limit at Kent and Catherine. I hope to hear more about this during the PIC.

Sincerely



**From:**  
**Sent:** November-17-15 9:24 PM  
**To:** Dave.Lindensmith@ontario.ca  
**Cc:** Tim Dickinson; Anita Sott; Kevin.Ogilvie@ontario.ca; Steve Regel  
**Subject:** Re: Queensway Sound Barriers

Many thanks for the update - please do advise of the date once it is known.

Have any timing decisions been made yet on the actual construction of the barriers?

Thank you,

"Lindensmith, Dave (MTO)" <[Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca)> writes:

We have been working through some construction staging issues and need to have these sorted out prior to the PIC.

MTO avoids scheduling PICs in December as this is a very busy time of year for most people and the participation in the PIC and input received suffers as a result. The PIC will be scheduled for late January 2016. This will allow us to place the ads in the newspapers and contact those on the study mailing list in early January, once the Christmas rush has passed.

I apologize for any inconvenience caused.

|

|

Regards,

|

Dave

|

**From:**  
**Sent:** December-01-15 9:43 PM  
**To:** Anita Sott  
**Cc:** Dave.Lindensmith@ontario.ca; Kevin.Ogilvie@ontario.ca; Steve Regel; Tim Dickinson; ynaqvi.mpp.co@liberal.ola.org;  
**Subject:** Re: HIGHWAY 417 NOISE BARRIER RETROFITS, DETAIL DESIGN (WP 4088-07-01)  
**Attachments:** Reid Park Aerial.JPG; IMG\_5255.JPG; IMG\_5264.JPG; IMG\_5254.JPG

Hi Anita,

Thank you for the very concise email regarding the 417 Noise Barrier Retrofit Detail Design WP 4088-07-01 and its potential extension further west to Reid Park. I can appreciate the design challenges associated with the area and respect the opinions of Professional Engineers involved with this project. I am happy to hear that the metal barriers are slated to be upgraded to the standard you spoke of and the height mentioned.

I believe this area requires further review due to the Reid Park Redevelopment Project slated to be finalized very soon. The City of Ottawa has been holding public consultations since 2013 regarding this project. It has finally come to point where the ongoing conversations have resulted in a plan to move forward. Part of the project involves reclaiming an old municipal yard and extending the boundaries of Reid Park. This former municipal yard backs on to the 417 beyond Reid ave where there are no barriers and merely a 5' high chain link fence. Concern revolves around the pleasantness of the park with only chain link bordering its limits at the 417. Safety is also a stated issue, not only for park users but users of the 417. Consider an errant ball on to the highway.

City of Ottawa and the public attending these consultations believe something is required in order to have a space that reflects what a public park is for its users. If not the standard MTO highway noise barriers then something equal to the task of mitigating noise and keeping the public safe. Alternative solutions should be looked in to, and at such proximity to the 417 MTO must have provisions set out to review.

If you are unfamiliar with the area I am speaking of I have attached some photos including an aerial view and where the old metal barriers end and the chain link begins.

Thank you again for your time.

Regards,

**From:**  
**Sent:** January-06-16 9:20 AM  
**To:** Anita Sott  
**Subject:** Unsubscribe

Please unsubscribe me.

**From:** [Anita Sott](#)  
**Sent:** Wednesday, January 6, 2016 9:03 AM  
**To:** [Anita Sott](#)  
**Cc:** [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca) ; [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca) ; [Lincoln MacDonald](#) ; [Tim Dickinson](#)  
**Subject:** WP 4088-07-01 - Highway 417 Noise Barrier Retrofits - Notice of Public Information Centre

Dear Sir / Madam,

The purpose of this message is to invite you to attend a Public Information Centre (PIC) for the above-noted Ministry of Transportation (MTO) Detail Design and Environmental Assessment Study. The PIC is scheduled as follows:

<b>Date:</b>	<b>Wednesday, January 20, 2016</b>
<b>Location:</b>	<b>St. Anthony's Hall (Hall A) 523 St. Anthony Street Ottawa, ON</b>
<b>Time:</b>	<b>4:00 PM to 8:00 PM</b>

Attached, please find a letter from the Consultant Project Manager which provides a general overview of the project and provides details on the PIC and study process.

Thank you,



**Anita Sott**, MCIP, RPP  
 Planner  
 Planning & Environmental Design

On behalf of

**Lincoln S. MacDonald**, P.Eng.  
 Manager, Highways  
 Associate Partner  
 Transportation

**MMM Group Limited**  
 1145 Hunt Club Road, Suite 300  
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 T +1 613-736-7200 #3298  
 F +1 613-736-8710

---

**From:**  
**Sent:** January-06-16 11:38 AM  
**To:** Anita Sott  
**Subject:** RE: WP 4088-07-01 - Highway 417 Noise Barrier Retrofits - Notice of Public Information Centre

Please remove me from your email distribution. I no longer have any time for these issues.

Try possible email addresses:

Thanks

---

From: [SottA@mmm.ca](mailto:SottA@mmm.ca)  
 To: [SottA@mmm.ca](mailto:SottA@mmm.ca)  
 CC: [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca); [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca); [MacDonaldL@mmm.ca](mailto:MacDonaldL@mmm.ca); [DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)  
 Subject: WP 4088-07-01 - Highway 417 Noise Barrier Retrofits - Notice of Public Information Centre  
 Date: Wed, 6 Jan 2016 14:03:24 +0000

Dear Sir / Madam,

The purpose of this message is to invite you to attend a Public Information Centre (PIC) for the above-noted Ministry of Transportation (MTO) Detail Design and Environmental Assessment Study. The PIC is scheduled as follows:

<b>Date:</b>	<b>Wednesday, January 20, 2016</b>
<b>Location:</b>	<b>St. Anthony's Hall (Hall A) 523 St. Anthony Street Ottawa, ON</b>
<b>Time:</b>	<b>4:00 PM to 8:00 PM</b>

Attached, please find a letter from the Consultant Project Manager which provides a general overview of the project and provides details on the PIC and study process.

Thank you,



**Anita Sott**, MCIP, RPP  
 Planner  
 Planning & Environmental Design

---

**From:**  
**Sent:** January-06-16 3:54 PM  
**To:** Anita Sott; Dave.Lindensmith@ontario.ca  
**Cc:** Kevin.Ogilvie@ontario.ca; Lincoln MacDonald; Tim Dickinson; Yasir Naqvi MPP (Constituency Office); David Chernushenko  
**Subject:** Re: WP 4088-07-01 - Highway 417 Noise Barrier Retrofits - Notice of Public Information Centre  
**Attachments:** Noise Barriers - PIC Email Letter - Public.pdf

Good afternoon Anita

Thank you for the notice. I intend to attend on behalf of our Condominium Corporation. Dave Lindensmith will be familiar with our issues on the NORTH side of the Queensway between Lyon and the canal.

Cheers

---

**From:** Anita Sott <[SottA@mmm.ca](mailto:SottA@mmm.ca)>  
**To:** Anita Sott <[SottA@mmm.ca](mailto:SottA@mmm.ca)>  
**Cc:** "Dave.Lindensmith@ontario.ca" <[Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca)>; "Kevin.Ogilvie@ontario.ca" <[Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca)>; Lincoln MacDonald <[MacDonaldL@mmm.ca](mailto:MacDonaldL@mmm.ca)>; Tim Dickinson <[DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)>  
**Sent:** Wednesday, January 6, 2016 9:03 AM  
**Subject:** WP 4088-07-01 - Highway 417 Noise Barrier Retrofits - Notice of Public Information Centre

Dear Sir / Madam,

The purpose of this message is to invite you to attend a Public Information Centre (PIC) for the above-noted Ministry of Transportation (MTO) Detail Design and Environmental Assessment Study. The PIC is scheduled as follows:

<b>Date:</b>	<b>Wednesday, January 20, 2016</b>
<b>Location:</b>	<b>St. Anthony's Hall (Hall A) 523 St. Anthony Street Ottawa, ON</b>
<b>Time:</b>	<b>4:00 PM to 8:00 PM</b>

Attached, please find a letter from the Consultant Project Manager which provides a general overview of the project and provides details on the PIC and study process.



**From:**  
**Sent:** January-06-16 5:15 PM  
**To:** Anita Sott  
**Subject:** Re: WP 4088-07-01 - Highway 417 Noise Barrier Retrofits - Notice of Public Information Centre

Thanks but I'll be away fom Ottawa that night.

**From:** [Anita Sott](#)  
**Sent:** Wednesday, January 6, 2016 9:03 AM  
**To:** [Anita Sott](#)  
**Cc:** [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca) ; [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca) ; [Lincoln MacDonald](#) ; [Tim Dickinson](#)  
**Subject:** WP 4088-07-01 - Highway 417 Noise Barrier Retrofits - Notice of Public Information Centre

Dear Sir / Madam,

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<b>Time:</b>	<b>4:00 PM to 8:00 PM</b>

Attached, please find a letter from the Consultant Project Manager which provides a general overview of the project and provides details on the PIC and study process.

Thank you,



**Anita Sott**, MCIP, RPP  
 Planner  
 Planning & Environmental Design

On behalf of

**Lincoln S. MacDonald**, P.Eng.  
 Manager, Highways  
 Associate Partner  
 Transportation

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[macdonaldl@mmm.ca](mailto:macdonaldl@mmm.ca)

[www.mmmgrouplimited.com](http://www.mmmgrouplimited.com) | [www.wspgroup.ca](http://www.wspgroup.ca)

**From:**  
**Sent:** January-11-16 3:11 PM  
**To:** Anita Sott  
**Subject:** RE: WP 4088-07-01 - Highway 417 Noise Barrier Retrofits - Notice of Public Information Centre

There are two ways I know that will reduce noise from the traffic, the road could be put underground or a land bridge can be build over it ( the land bridge would have trees or grass growing above it. What about those two ideas?

How well do trees block noise, and if you decided to build a wall would it be possible to plant trees to hide the walls on both sides?

---

From: [SottA@mmm.ca](mailto:SottA@mmm.ca)  
 To: [SottA@mmm.ca](mailto:SottA@mmm.ca)  
 CC: [Dave.Lindensmith@ontario.ca](mailto:Dave.Lindensmith@ontario.ca); [Kevin.Ogilvie@ontario.ca](mailto:Kevin.Ogilvie@ontario.ca); [MacDonaldL@mmm.ca](mailto:MacDonaldL@mmm.ca); [DickinsonT@mmm.ca](mailto:DickinsonT@mmm.ca)  
 Subject: WP 4088-07-01 - Highway 417 Noise Barrier Retrofits - Notice of Public Information Centre  
 Date: Wed, 6 Jan 2016 14:03:24 +0000

Dear Sir / Madam,

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<b>Date:</b>	<b>Wednesday, January 20, 2016</b>
<b>Location:</b>	<b>St. Anthony's Hall (Hall A) 523 St. Anthony Street Ottawa, ON</b>
<b>Time:</b>	<b>4:00 PM to 8:00 PM</b>

Attached, please find a letter from the Consultant Project Manager which provides a general overview of the project and provides details on the PIC and study process.

Thank you,



**Anita Sott**, MCIP, RPP  
 Planner  
 Planning & Environmental Design

On behalf of  
**Lincoln S. MacDonald**, P.Eng.  
 Manager, Highways

---

**From:** Lincoln MacDonald  
**Sent:** January-20-16 1:02 PM  
**To:** Anita Sott  
**Subject:** FW: PIC tonight

FYI , response preparation, etc.



**Lincoln S. MacDonald, P. Eng.**  
Manager, Transportation - Highways

T +1 613-736-7200 #3298  
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---

**From:**  
**Sent:** January-20-16 12:20 PM  
**To:** [dave.lindensmith@ontario.ca](mailto:dave.lindensmith@ontario.ca); Lincoln MacDonald  
**Cc:**  
**Subject:** PIC tonight  
**Importance:** High

Hello David and Lincoln,

Unfortunately, there's a probable chance I cannot attend tonight's PIC regarding Hwy 417 noise retrofit plan so I'd appreciate your feedback on a few questions.

Firstly, I saw in the notice mailed a couple of weeks ago that the project involves noise barrier retrofits from:

- Bronson Ave to Lyon St (north side of Hwy 417)

I suppose my first question is, according to the original recommended plan there would be erection of new noise barriers on the south side of Hwy 417 between Bronson and Percy, is that still projected to happen (in addition to above)? I assume this was omitted from the notice and since the OCDSB building was demolished it is expected the erection of new barriers will replace the noise attenuation offered by the former building, is this accurate? Secondly, is

the noise barrier expected to be erected on the south side of Hwy 417 from Bronson to be made of concrete, tan, random pattern, etc... as detailed in an image as part of the original recommended plan? Will the existing steel noise barrier, currently running half way between Bronson & Percy to Lyon, be retrofitted with the new concrete barrier technology? Also, apart from largely ineffective noise attenuation properties of shrub planting and ground cover, are there any more effective solutions to buffer the increased noise levels expected to arise from increased traffic volume on Chamberlain immediately east of Bronson and south of Chamberlain?

Finally, has anything related to the eastbound 417 Bronson Ave exit ramp extension and Chamberlain Ave reconfiguration deviated from the original recommended plan? Particularly, I'm concerned with the trajectory of Chamberlain, landscaped area between the new Chamberlain route and residences that back onto Glebe Memorial Park, and left/right access from Bronson to Chamberlain/Imperial?

Thanks so much for your time.

# COMMENT SHEET / FEUILLE DE COMMENTAIRES

HIGHWAY 417 NOISE BARRIER RETROFITS  
BETWEEN THE O-TRAIN AND THE RIDEAU CANAL,  
DETAIL DESIGN

AMÉNAGEMENT DE MURS ANTIBRUIT SUR  
L'AUTOROUTE 417 ENTRE L'O-TRAIN ET LE  
CANAL RIDEAU, CONCEPTION DÉTAILLÉE

(WP 4088-07-01)

## PUBLIC INFORMATION CENTRE / SÉANCE D'INFORMATION PUBLIQUE

WEDNESDAY, JANUARY 20, 2016	LE MERCREDI 20 JANVIER 2016
ST. ANTHONY'S HALL, HALL A 523 ST. ANTHONY STREET OTTAWA, ON 4:00 PM TO 8:00 PM	SALLE DE RÉCEPTION ST. ANTHONY (SALLE A) 523, RUE ST. ANTHONY OTTAWA (ONTARIO) DE 16 H À 20 H

PLEASE NOTE: Either drop your completed comment sheet in the box provided or send it by January 27, 2016 to:

NOTEZ S.V.P.: Déposez votre feuille de commentaires remplie dans la boîte prévue à cet effet ou envoyez-là, au plus tard le 27 janvier 2016 à :

Mr. Lincoln MacDonald, P.Eng.  
Consultant Project Manager  
MMM Group Limited  
1145 Hunt Club Rd., Suite 300  
Ottawa, ON K1V 0Y3

Fax / Téléc: (613) 736-8710  
E-mail / Courriel: sotta@mmm.ca  
(on behalf of / au nom de Lincoln MacDonald)  
Project Website / le site Web du projet:  
[www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)

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### COMMENTS / COMMENTAIRES:

My concern is in areas not scheduled for noise barrier retrofit as part of the work in this plan.  
There has been a building removed (old school board/pops, bottling plant) at Bronson south of 417.  
During the Kent street replacement, vegetation was removed to provide for a staging area.  
As a result noise has increased on south side.  
The barrier on the north side just reflects Queensway traffic noise to the south.  
A noise barrier on the south side on the Bronson ex) from Bronson to Lyon would keep queensway traffic noise contained.  
I understand this may be done during the Bronson bridge replacement. (a representative informed me)  
I would encourage that this noise barrier (Bronson - Lyon south side) be installed sooner rather than later.

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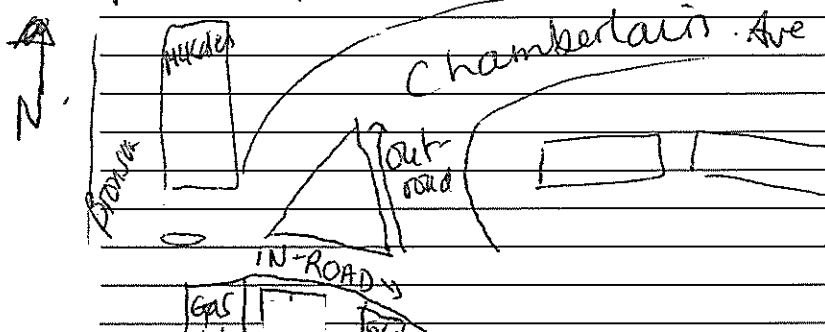
☒ E-mail / Courriel

☐ Regular Mail / Poste régulière

### COMMENTS / COMMENTAIRES:

Please could you send me any information  
regarding the Bronson/Imperial intersection proposed  
new configuration?

Specifics if possible on: Imperial Ave changes, the  
closure of the current in-road in front of Imperial  
and the new 2-way entry-exit at the current exit  
from Imperial Avenue onto Chamberlain



Thanks!



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### COMMENTS / COMMENTAIRES:

I'm glad to hear that the retrofit noise barrier is being extended on the north side of the 417 in Centretown. It is a densely populated area with 25K residents, 80% renters, & 20% homeowners, whose quality of life would be vastly improved if the noise barrier were extended. The barriers were discussed during Mayor Nagfi's first mandate. It would be wonderful if it could be realized during his second. Why does the GTA get a most of the infrastructure money? Ottawa is Ontario's second largest municipality with a stable tax base. We should have priority, too. \* renters are taxed indirectly through

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### COMMENTS / COMMENTAIRES:

LOOKS GOOD, JUST BUILD

THEM! WE'VE BEEN

WAITING OVER 30 YEARS

FOR THESE



# COMMENT SHEET / FEUILLE DE COMMENTAIRES

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### COMMENTS / COMMENTAIRES:

Is there any plans for major work to be  
staged at <sup>east</sup> ~~west~~ side of Kent/417 underpass  
(south side of 417)?

Thank you.

# COMMENT SHEET / FEUILLE DE COMMENTAIRES

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### COMMENTS / COMMENTAIRES:

Excellent opportunity to discuss our concerns  
with the relevant project manager.

I cannot begin to tell you how relieved we  
are that the noise barriers will be installed shortly -  
while others that will have a significant impact  
on our mental and physical health are planned for.

Thank you! Kevin was superb

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### COMMENTS / COMMENTAIRES:

Very pleased that the barriers will be installed on the South side, right to Bault especially!!

Would appreciate an extension of the barrier from the Queensway on the exit ramp to Kent Street. The traffic is in decelerative mode, which is signif. cant as the trucks exit at the ramp and continue under the new Kent St. bridge. Barriers along this section of the ramp would be of great help.

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### COMMENTS / COMMENTAIRES:

- improved noise barrier needed.
- hope this is coordinated with bridge replacement work.
- interesting that the area between O-Train & Preston is considered Green Space & wild life corridor in their planning.
- will look for notice of sidewalk closures on Preston - very busy sidewalks at all hours a year round.



## COMMENT SHEET / FEUILLE DE COMMENTAIRES

## HIGHWAY 417 NOISE BARRIER RETROFITS BETWEEN THE O-TRAIN AND THE RIDEAU CANAL, DETAIL DESIGN

## AMÉNAGEMENT DE MURS ANTIBRUIT SUR L'AUTOROUTE 417 ENTRE L'O-TRAIN ET LE CANAL RIDEAU, CONCEPTION DÉTAILLÉE

(WP 4088-07-01)

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## COMMENTS / COMMENTAIRES

- Thank you for finally installing barriers on N. side from Branson to Lynn.
- Concerned about noise impact of the work itself, esp overnight.
- Concerned that acrylic barriers on south side east of Elgin will reflect sound northward & amplify impact to residents off Elgin and Queen Elizabeth Drive.
- Concerned about night time ~~noise~~ pedestrian detour from Metcalfe to O'Connor — esp. if work is concurrent with any 2016 work for the O'Connor Bypass. — Beaver Barr. ... residents often go to Loblaw's at night, residents with mobility difficulties will have their walking distance increased 4x. Loblaw's is open until 11pm. Consider: — deferring nighttime sidewalk closure until 11pm
- temporary crosswalks to detour ped traffic onto East sidewalk under Metcalfe (S. side @ Catherine N. side @ Beakella)

---

**From:** Lincoln MacDonald  
**Sent:** January-29-16 3:26 PM  
**To:** Anita Sott  
**Subject:** FW: Highway 417 Noise Barrier Retrofits...(WP 4088-07-01)  
**Attachments:** MTO Env Guide Noise Final 2006.pdf

Anita,

Response from MTO to

Lincoln

Lincoln S. MacDonald, P. Eng.  
Manager, Transportation - Highways

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-----Original Message-----

From: Lindensmith, Dave (MTO) [<mailto:Dave.Lindensmith@ontario.ca>]  
Sent: January-29-16 3:16 PM  
To:  
Cc: Lincoln MacDonald; Ogilvie, Kevin (MTO); Lindensmith, Dave (MTO)  
Subject: RE: Highway 417 Noise Barrier Retrofits...(WP 4088-07-01)

Dear

Thank you for your e-mail.

Hwy 417 is under the jurisdiction of the Province of Ontario, while Ottawa Road 174 is under the jurisdiction of the City of Ottawa.

The noise barrier project on Highway 417 is being undertaken by the Ministry of Transportation of Ontario, according to provincial policy guidelines. I have attached a copy of the MTO noise policy for your reference.

Please note that the City of Ottawa will have their own noise attenuation policies. As a result, you need to touch base directly with the City of Ottawa for any questions relating to their noise policy or to noise attenuation for city projects on 174.

The display boards from the Ministry's recent Public Information Centre for our project ([http://queenswayexpansioneast.com/wp-content/uploads/2016/01/FINAL-Hwy-417-Noise-Barriers-PIC-Boards\\_Jan-20-2016.pdf](http://queenswayexpansioneast.com/wp-content/uploads/2016/01/FINAL-Hwy-417-Noise-Barriers-PIC-Boards_Jan-20-2016.pdf)) show typical noise barrier installations (see photo inset on the "Recommended Plan" board.)

In MTO applications, noise barriers are typically 5 m in height and constructed using sound absorptive concrete panels. In specific locations, the noise barrier will be made up of clear acrylic panels. Both these are shown on the PIC "Recommended Plan" board.

Regards,

David Lindensmith, P. Eng.  
Sr. Project Engineer  
MTO Planning & Design Section  
Eastern Region

-----Original Message-----

From:  
Sent: January-29-16 2:35 PM  
To: [macdonaldl@mmm.ca](mailto:macdonaldl@mmm.ca)  
Cc: Lindensmith, Dave (MTO)  
Subject: Highway 417 Noise Barrier Retrofits...(WP 4088-07-01)

Dear Mr MacDonald, earlier this week I telephoned your office and left you a voice mail pertaining to the above referenced project.

Not having received a reply I thought an email outlining my questions would assist you in answering my information needs.

Generally, I am interested in the concept of Noise Barriers I thought by accessing the Project on the city's website would provide me something. To no avail as no updates were posted on the above project or other related topics.

Could you refer me to both policy information pertaining to installation policy, and the actual technical description of these 'barriers'?

I live in a neighbourhood to be affected by the extension of Highway 174's LRT and am interested in the issue of noise barriers. In the Notice of Public Information Centre I saw a potential opportunity to see both the overall policy, and its actual implementation in this project.

Thank you for your assistance and cooperation. I suggest you may wish to review the information link missing from the referred to website ([www.queenswayexpansioneast.com](http://www.queenswayexpansioneast.com)).

Sincerely